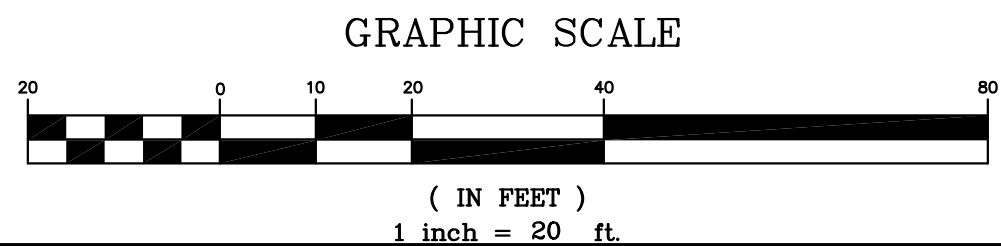
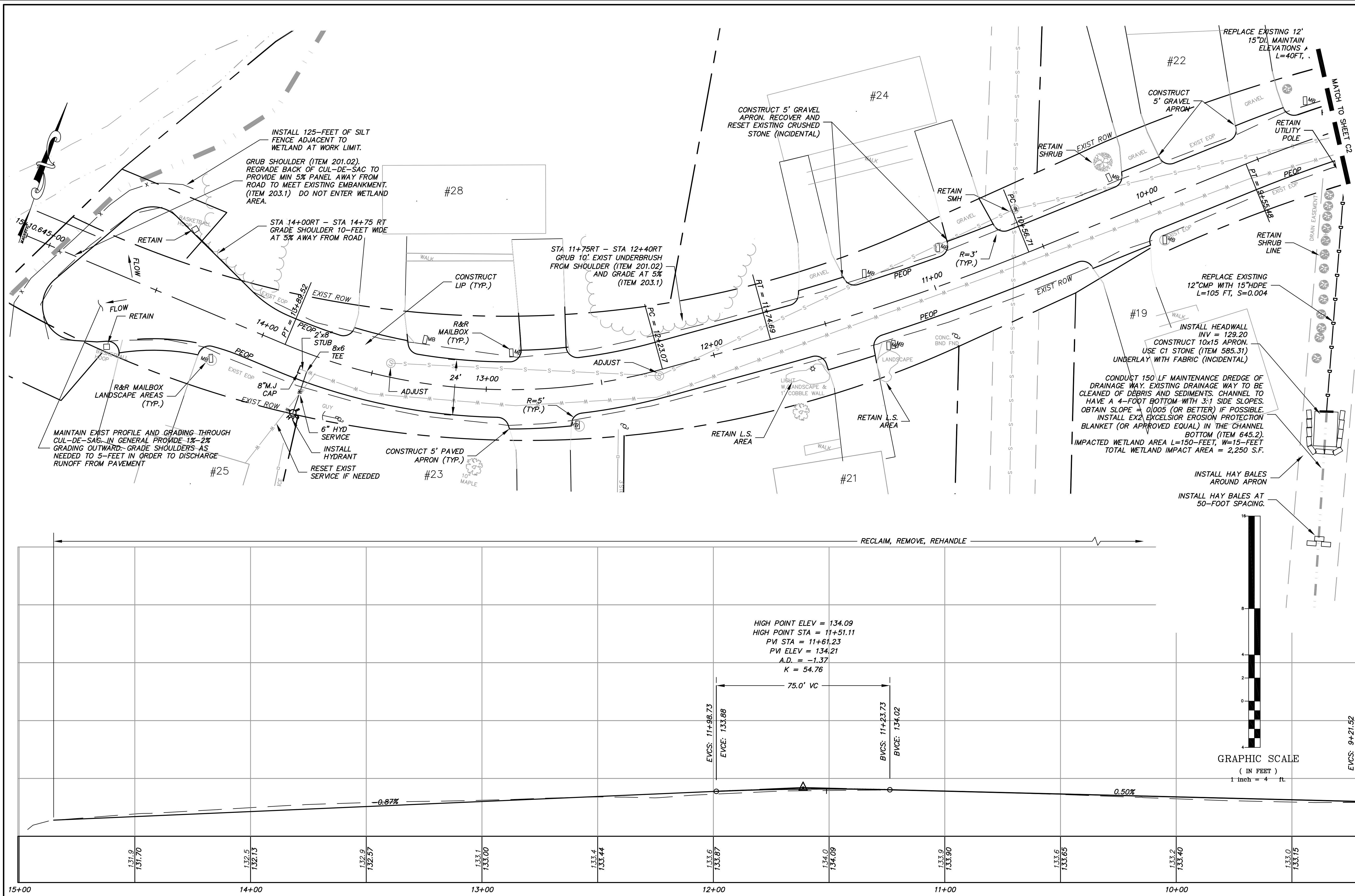


**OWNER:**

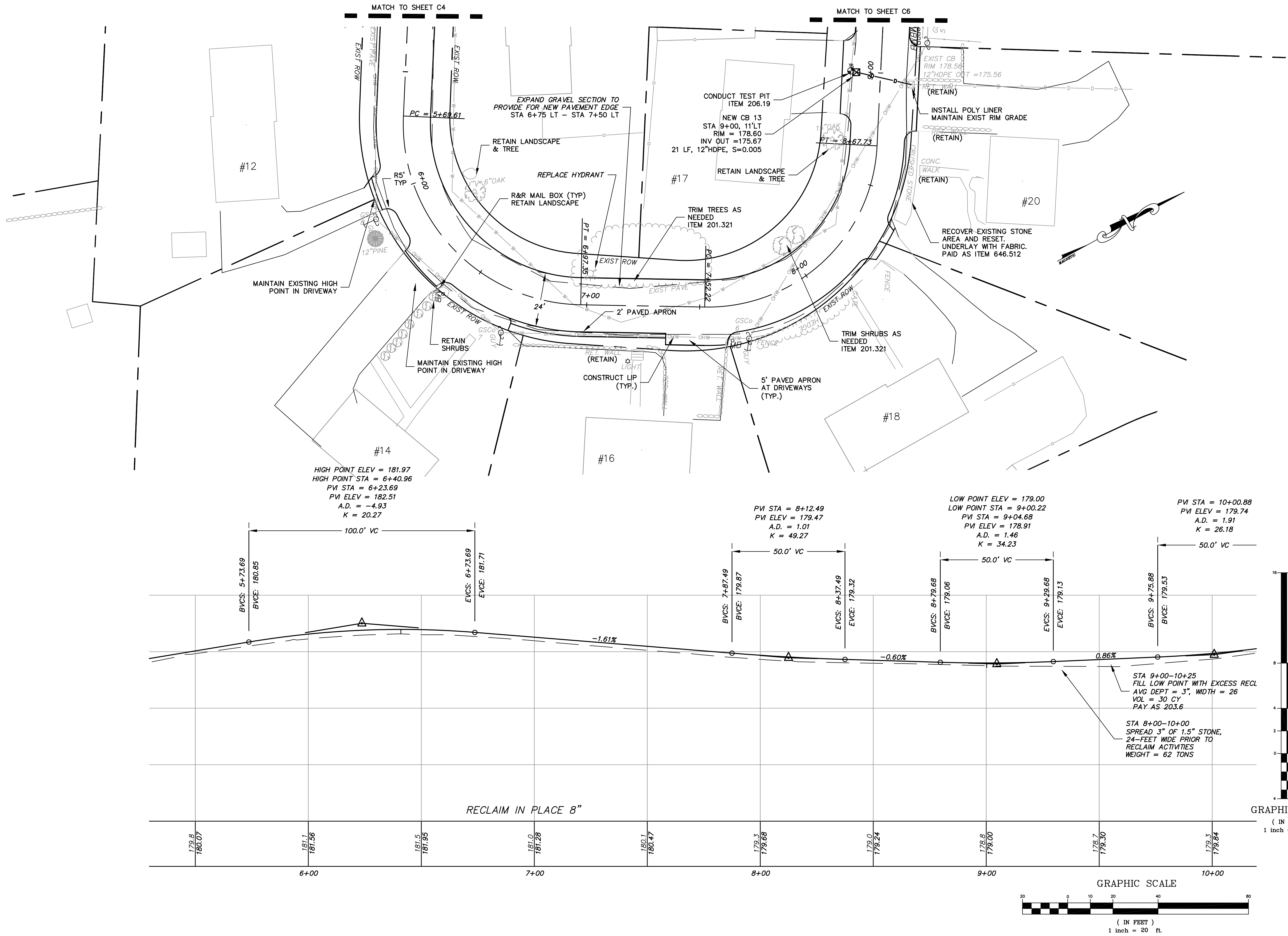


DATE: DEC 2015	SCALE: 1"=20'(H) 1"=4'(V)
SHEET: C2	PLAN NO. 3 OF 17

X:\Projects\2016 Road Program\Corwell CT\dwg\Corwell CT MD.dwg 1/13/2016 10:33:00 AM EST



2016 ROADWAY IMPROVEMENT PROGRAM		SALEM, NH 03079 ASSESSORS MAP 89	
CORWELL COURT		PLAN NO.	
PLAN & PROFILE		4 OF 17	
DATE:	DEC 2015	SHEET:	C3
SCALE:	1"=20'(H) 1"=4'(V)	PLAN NO.	
OWNER:		TOWN OF SALEM 33 GEREMONY DRIVE SALEM, NH 03079	
DESIGNED:		JAD	
CHECKED:		REP	
APPROVED:		REP	



<b>DATE:</b>	<b>SCALE:</b>
DEC 2015	"=20'(H) 1"=4'(V)
<b>SHEET:</b>	<b>PLAN NO.</b>
C5	6 OF 17

**2016 ROADWAY IMPROVEMENT PROGRAM**

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SALEM, NH 03079  
ASSESSORS MAP 89

## WINDWARD TERRACE PLAN & PROFILE

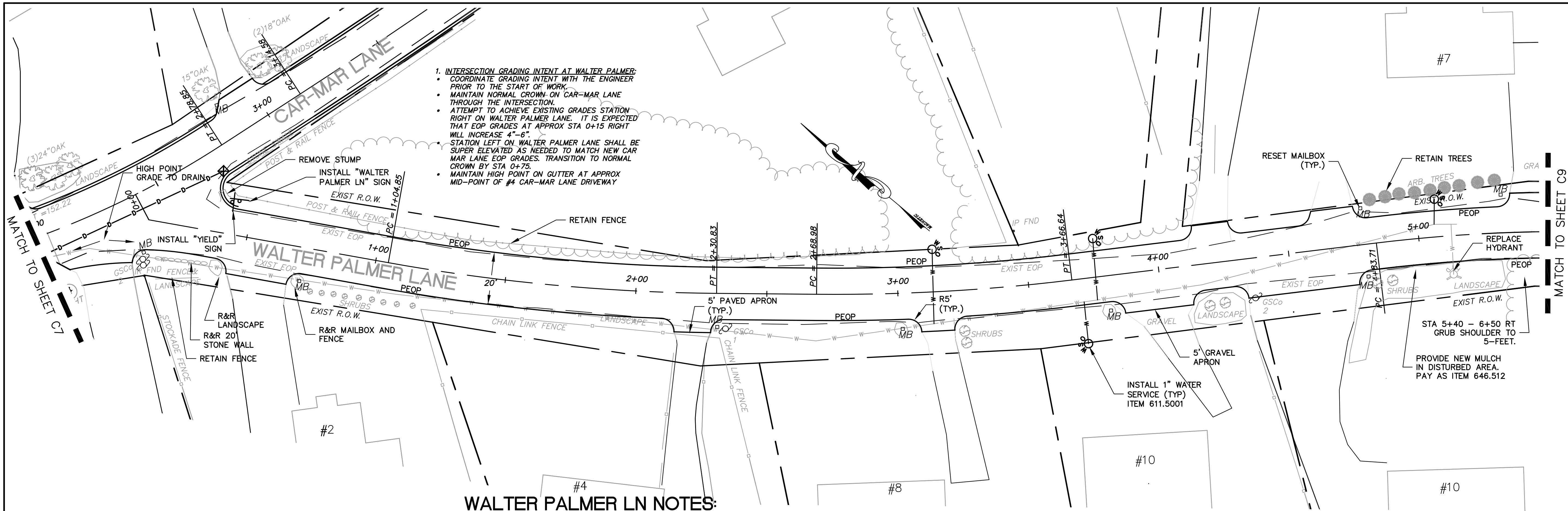
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**TOWN OF SALEM**  
**33 GEREMONY DRIVE**  
**SALEM, NH 03079**

**OWNER:**

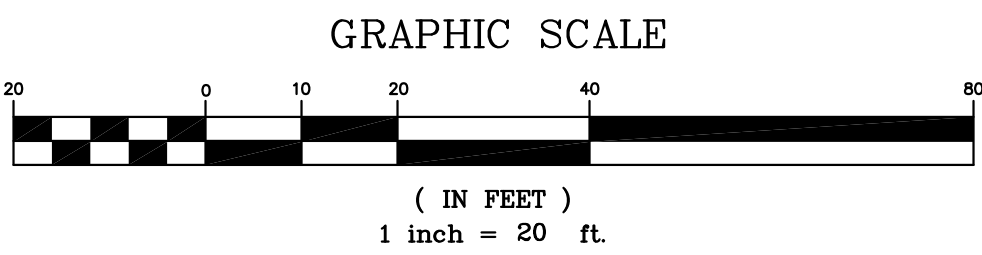
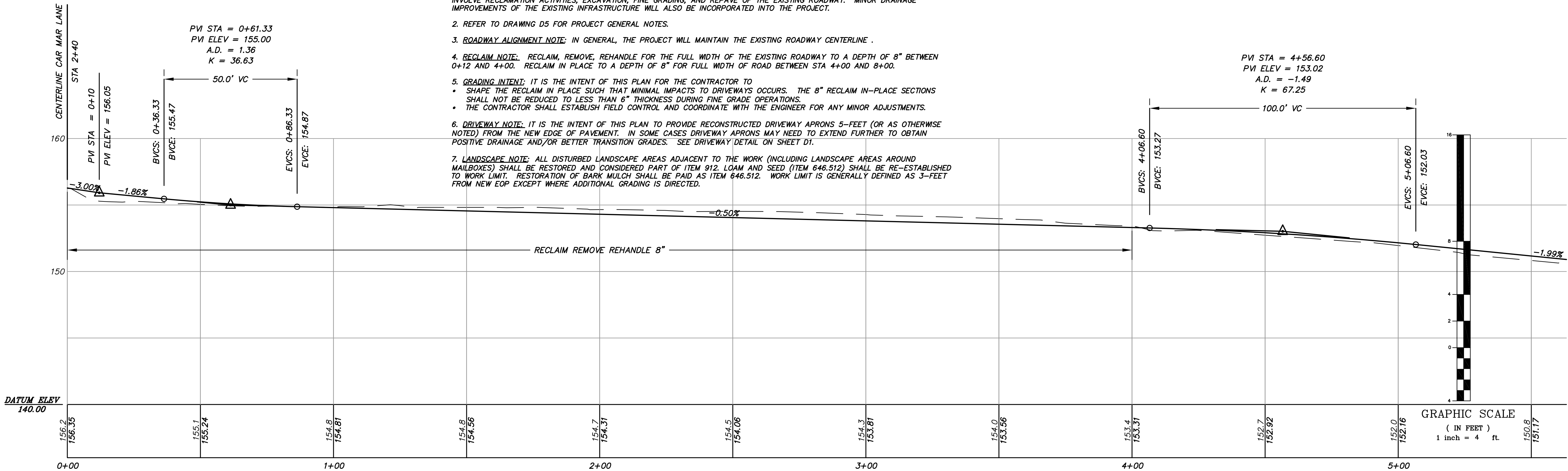
1	1/13/16	ADD HYDRANT REPLACEMENT			
NO.	DATE	REVISION			
DRAWN:	JAD	DESIGNED:	JAD	CHECKED:	REP
APPROVED:		APPROVED:		CHECKED:	REP





### WALTER PALMER LN NOTES:

1. THE INTENT OF THIS PLAN IS TO PROVIDE ROADWAY IMPROVEMENTS FOR WALTER PALMER LANE AS PART OF THE 2016 ROADWAY IMPROVEMENT PROJECT - CORNWELL COURT, WINDWARD TERRACE, CAR-MAR LANE, WALTER PALMER LANE. THE WORK WILL INVOLVE RECLAMATION ACTIVITIES, EXCAVATION, FINE GRADING, AND REPAVE OF THE EXISTING ROADWAY. MINOR DRAINAGE IMPROVEMENTS OF THE EXISTING INFRASTRUCTURE WILL ALSO BE INCORPORATED INTO THE PROJECT.
2. REFER TO DRAWING D5 FOR PROJECT GENERAL NOTES.
3. **ROADWAY ALIGNMENT NOTE:** IN GENERAL, THE PROJECT WILL MAINTAIN THE EXISTING ROADWAY CENTERLINE.
4. **RECLAIM NOTE:** RECLAIM, REMOVE, REHANDLE FOR THE FULL WIDTH OF THE EXISTING ROADWAY TO A DEPTH OF 8" BETWEEN 0+12 AND 4+00. RECLAIM IN PLACE TO A DEPTH OF 8" FOR FULL WIDTH OF ROAD BETWEEN STA 4+00 AND 8+00.
5. **GRADING INTENT:** IT IS THE INTENT OF THIS PLAN FOR THE CONTRACTOR TO
  - SHAPE THE RECLAIM IN PLACE SUCH THAT MINIMAL IMPACTS TO DRIVEWAYS OCCURS. THE 8" RECLAIM IN-PLACE SECTIONS SHALL NOT BE REDUCED TO LESS THAN 6" THICKNESS DURING FINE GRADE OPERATIONS.
  - THE CONTRACTOR SHALL ESTABLISH FIELD CONTROL AND COORDINATE WITH THE ENGINEER FOR ANY MINOR ADJUSTMENTS.
6. **DRIVEWAY NOTE:** IT IS THE INTENT OF THIS PLAN TO PROVIDE RECONSTRUCTED DRIVEWAY APRONS 5-FEET (OR AS OTHERWISE NOTED) FROM THE NEW EDGE OF PAVEMENT. IN SOME CASES DRIVEWAY APRONS MAY NEED TO EXTEND FURTHER TO OBTAIN POSITIVE DRAINAGE AND/OR BETTER TRANSITION GRADES. SEE DRIVEWAY DETAIL ON SHEET D1.
7. **LANDSCAPE NOTE:** ALL DISTURBED LANDSCAPE AREAS ADJACENT TO THE WORK (INCLUDING LANDSCAPE AREAS AROUND MAILBOXES) SHALL BE RESTORED AND CONSIDERED PART OF ITEM 912. LOAM AND SEED (ITEM 646.512) SHALL BE RE-ESTABLISHED TO WORK LIMIT. RESTORATION OF BARK MULCH SHALL BE PAID AS ITEM 646.512. WORK LIMIT IS GENERALLY DEFINED AS 3- FEET FROM NEW EOP EXCEPT WHERE ADDITIONAL GRADING IS DIRECTED.

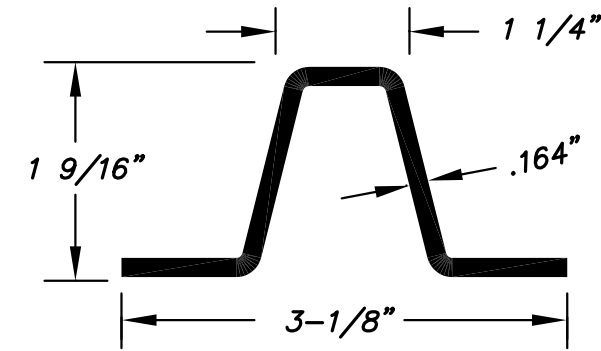


OWNER:		TOWN OF SALEM 33 GEREMONY DRIVE SALEM, NH 03079	
2015 ROADWAY IMPROVEMENT PROGRAM		SALEM, NH 03079 ASSESSORS MAP 89 WALTER PALMER LANE PLAN & PROFILE	
DATE:	SCALE:	SHEET:	11 OF 17
DEC 2015	1"=20'(H) 1"=4'(V)	C10	
NO. DATE REVISION		1 1/13/16 ADD HYDRANT REPLACEMENT AND SERVICES	
DRAWN: JAD		CHECKED: JAD	
DESIGNED: JAD		APPROVED: REP	

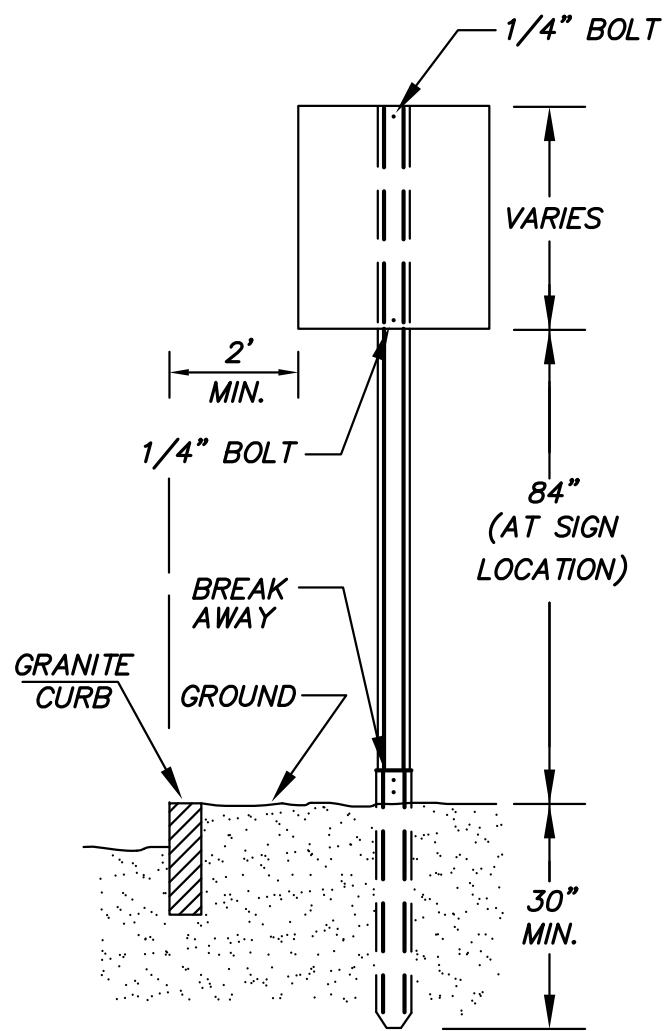
NOTE:  
1. REMOVE AND RESET OF MAILBOXES SHALL ONLY BE APPLICABLE IF THE EXISTING MAILBOX IS CLOSER THAN THE MINIMUM DISTANCE TO EDGE OF PAVE AS A RESULT OF CONSTRUCTION. NOT ALL MAILBOXES SHALL BE RESET.  
2. ELEVATION OF MAILBOX IS MEASURED FROM THE NEW EDGE OF PAVEMENT REGARDLESS OF WHETHER CURB HAS BEEN INSTALLED.  
3. WHERE NEW POSTS ARE REQUIRED CONTRACTOR SHALL PROVIDE 4x4 P.T. OR 2"DIA W/ANTI TWIST COLLARS. NEW POLES ARE SUBSIDIARY.

### Mailbox Installation Detail

LENGTH: P-12, 12'-0"; P-14, 14'-0"; P-16, 16'-0".  
WEIGHT PER LINEAR FOOT: 2.50 LBS. (MIN.)  
HOLES: 3/8" DIA. 1' C-C FULL LENGTH  
STEEL: SHALL CONFORM TO ASTM A-499 (GRADE 60) OR ASTM A-576 (GRADE 1070-1080).  
FINISH: SHALL BE GALVANIZED



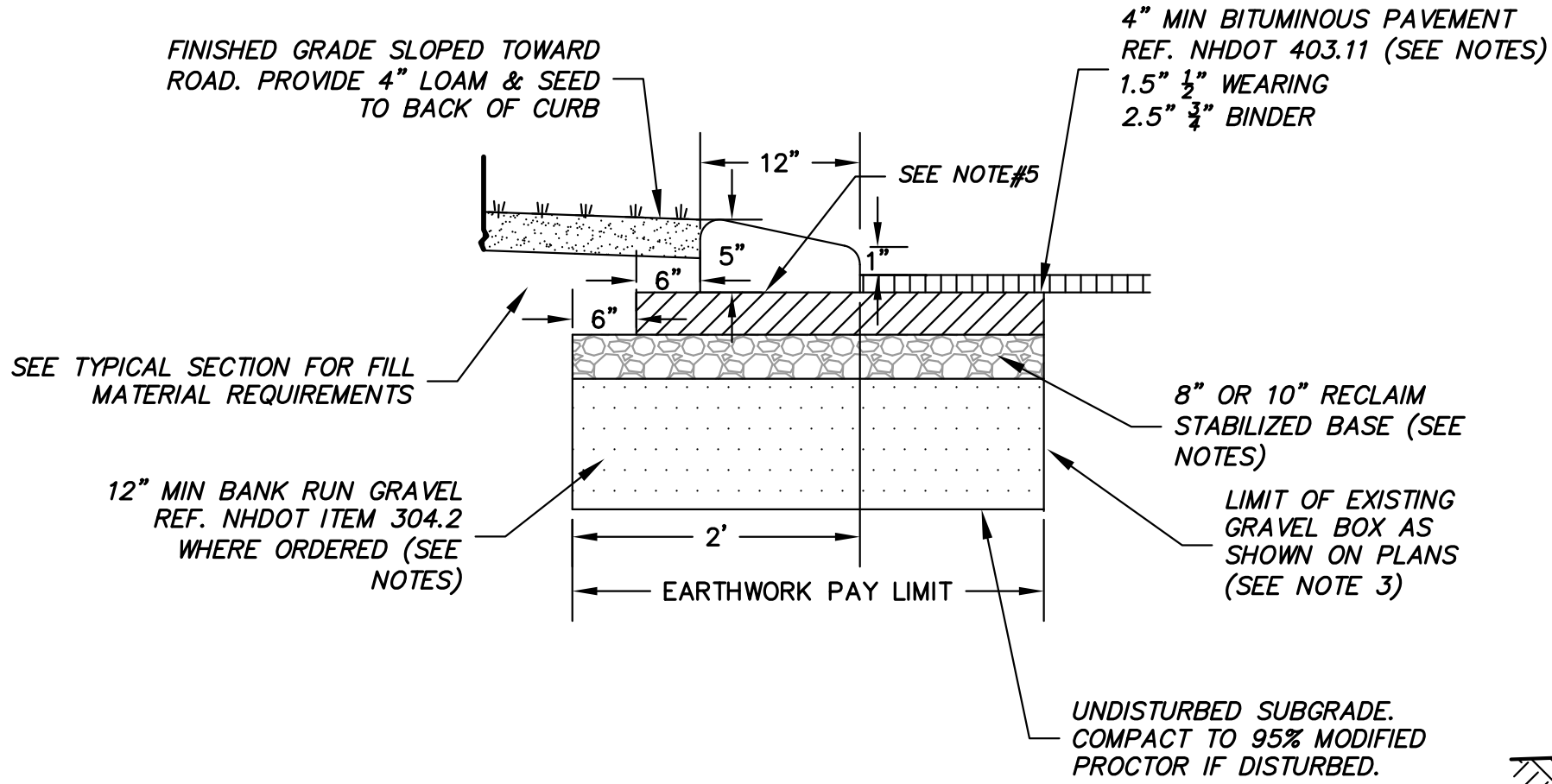
DIMENSIONS SHOWN ARE NOMINAL  
ALTERNATE SECTIONS MUST BE APPROVED PRIOR TO USE.



### Sign Post

#### NOTES:

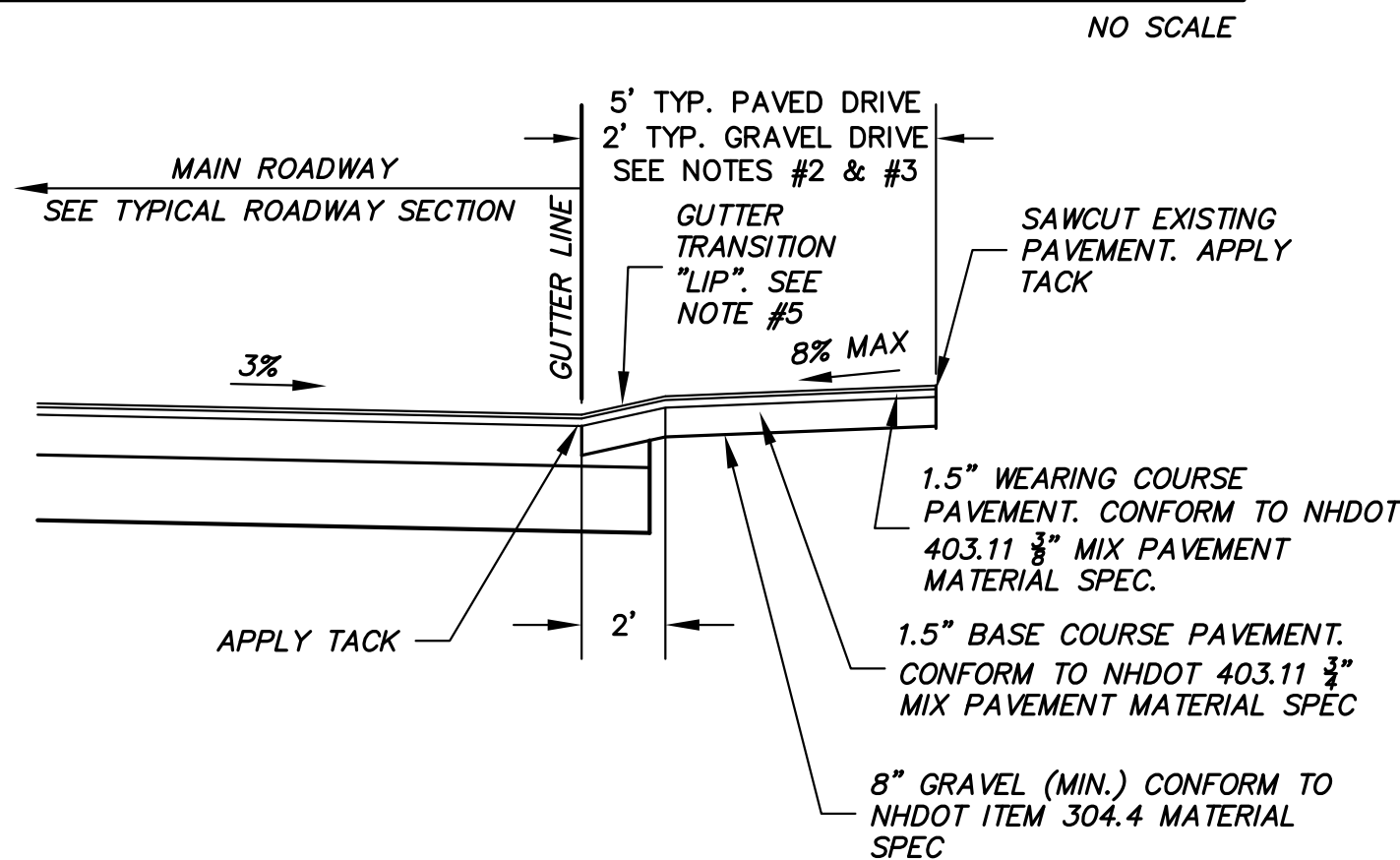
- SIGN CONSTRUCTION AND INSTALLATION SHALL MEET THE REQUIREMENTS OF NHDOT ITEM 615.
- POSTS SHALL BE BREAKAWAY CONSTRUCTION WITH STAINLESS STEEL HARDWARE.
- POSTS SHALL BE PLUMB; ANY POST BENT OR OTHERWISE DAMAGED SHALL BE REMOVED AND PROPERLY REPLACED. POSTS MAY BE SET OF DRIVEN.
- WHEN POSTS ARE SET, HOLES SHALL BE DUG TO THE PROPER DEPTH; AFTER INSERTING POSTS, THE HOLES SHALL BE BACK FILLED WITH SUITABLE MATERIAL IN LAYERS NOT TO EXCEED 6" DEEP AND THOROUGHLY COMPACTED, CARE BEING TAKEN TO PRESERVE THE ALIGNMENT OF THE POST.
- WHEN POSTS ARE DRIVEN, A SUITABLE DRIVING CAP SHALL BE USED AND AFTER DRIVING THE TOP OF THE POST SHALL HAVE SUBSTANTIALLY THE SAME CROSS-SECTIONAL DIMENSION AS THE BODY OF THE POST; BATTERED HEADS WILL NOT BE ACCEPTED.
- POSTS SHALL NOT BE DRIVEN WITH THE SIGN ATTACHED TO THE POST.
- SIGNS SHALL BE ERECTED IN CONFORMANCE WITH THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", CURRENT EDITION.
- WHEN SIGN IS IN PLACE NO PART OF POST SHALL EXTEND ABOVE THE SIGN.



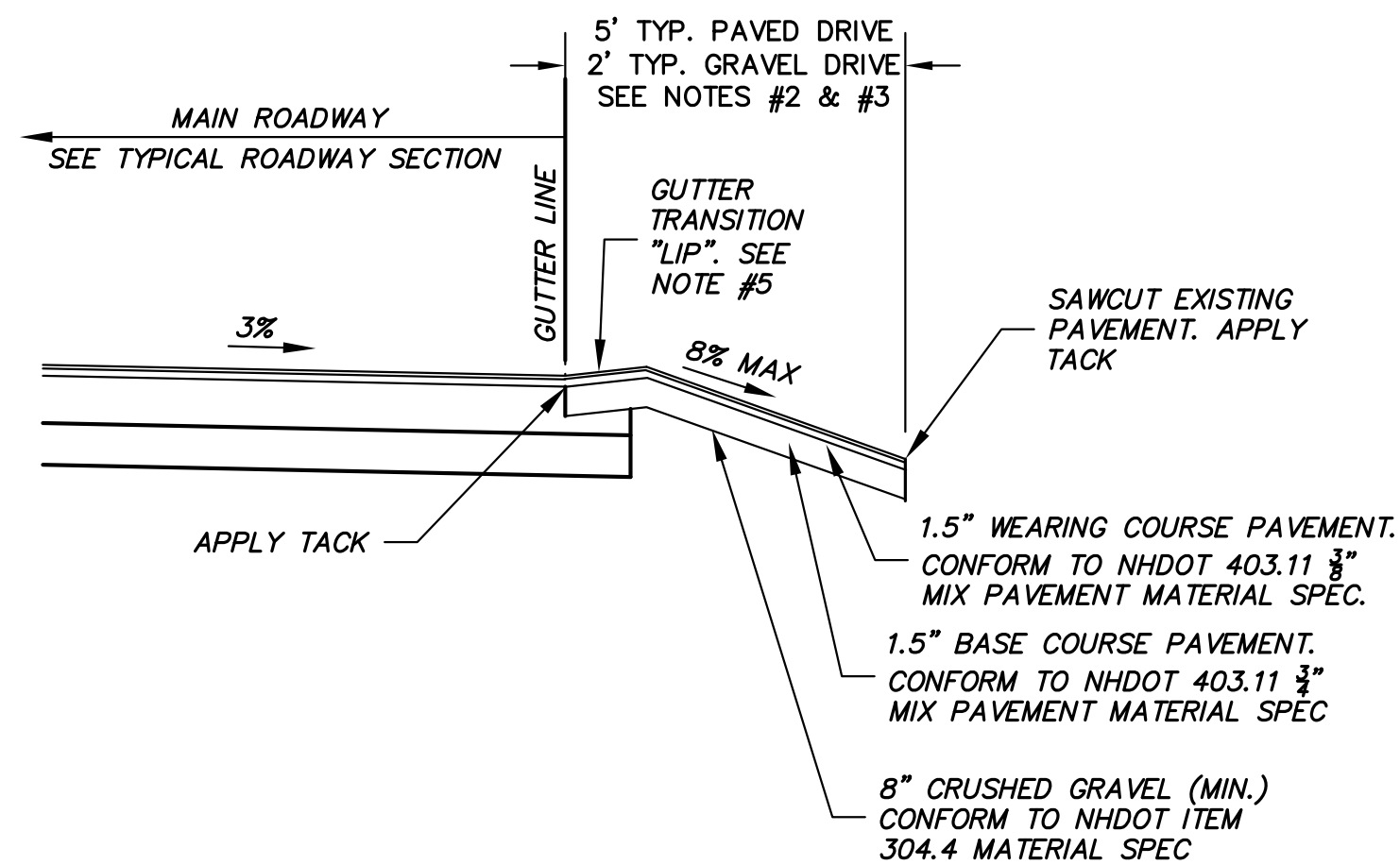
#### NOTES:

- REFERENCE NHDOT MATERIAL SPECIFICATIONS. ALL MATERIALS SHALL CONFORM TO APPLICABLE NHDOT MATERIAL SPECIFICATIONS.
- ALL GRAVELS TO BE COMPACTED TO 95% MODIFIED PROCTOR.
- WHERE NECESSARY TO WIDEN THE GRAVEL BOX FOR BITUMINOUS CURB THE GRAVEL DEPTHS SHALL BE MINIMUM SPECIFIED OR MATCH THE EXISTING, WHICHEVER IS GREATER TO PAY LIMITS.
- EXISTING WORK: EARTHWORK PAY LIMIT FOR BOX WIDENING SHALL BEGIN AT THE LIMIT OF THE EXISTING GRAVEL BOX. NOTE THAT IF SUITABLE GRAVELS EXIST THEN WIDENING SHALL NOT APPLY.
- CURB TO BE PAID PER UNIT LENGTH COMPLETE IN PLACE. PAVEMENT, ROAD BED GRAVELS AND CUT/FILLS SHALL BE PAID UNDER RESPECTIVE ITEM NUMBERS.
- BINDER SHALL BE TACK COATED BELOW BERM. TACK IS A SUBSIDIARY ITEM TO THE BERM.

### Bituminous Berm Curb

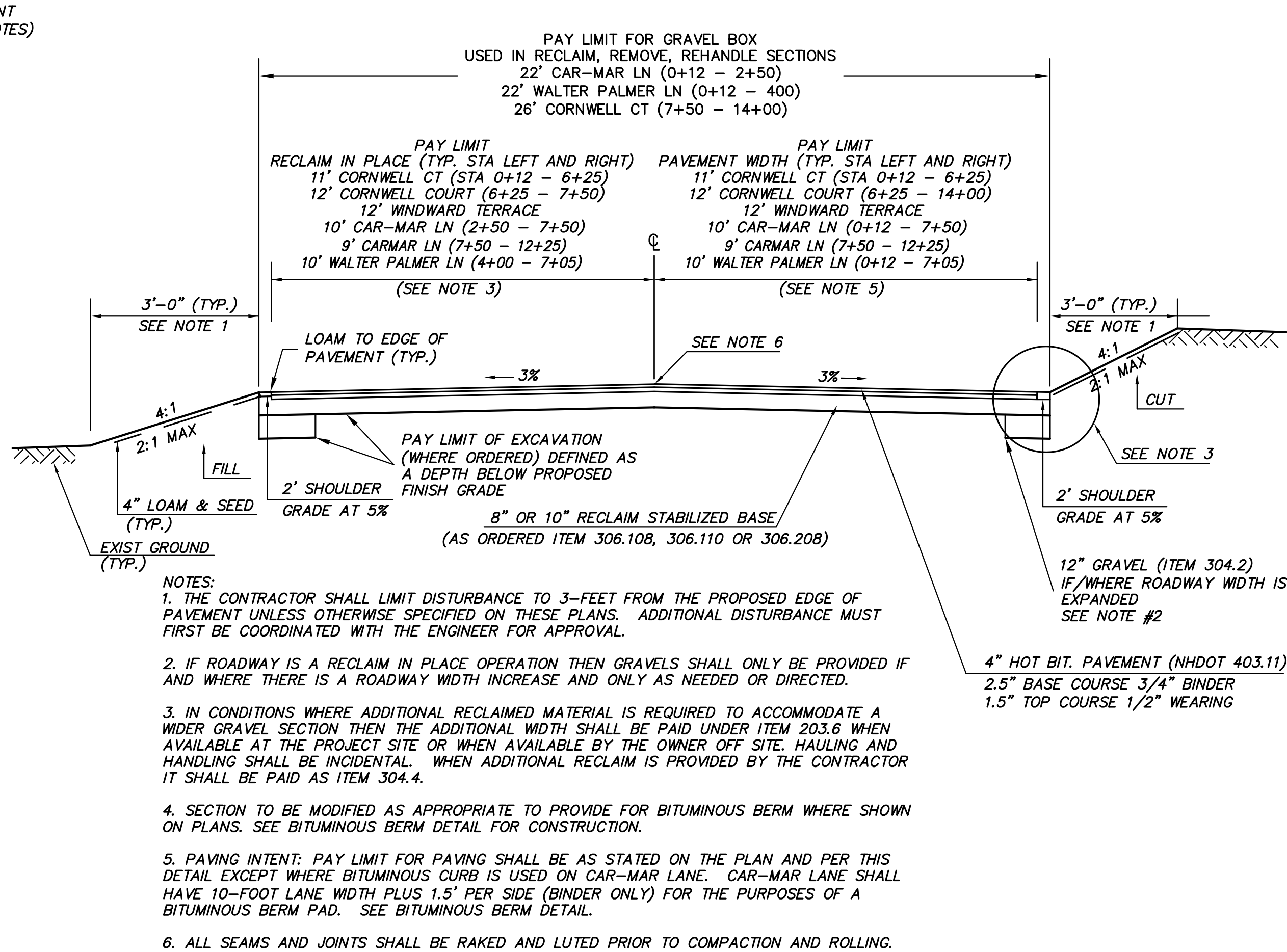


### DRIVEWAY APRON ABOVE ROAD GRADE



### DRIVEWAY APRON BELOW ROAD GRADE

### Typical Driveway Apron Profile Detail



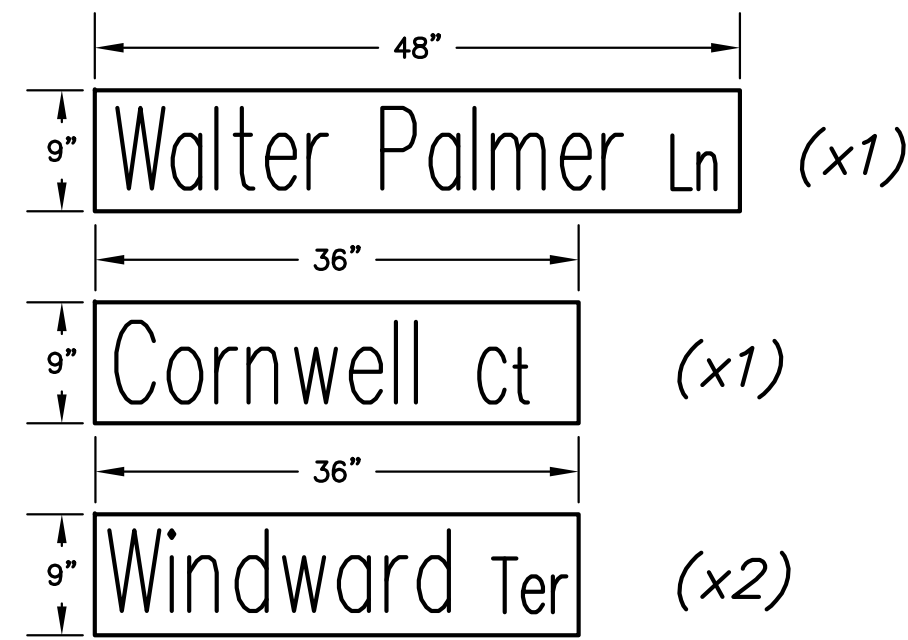
### Typical Roadway Section

#### NOTES:

- REFERENCE PROJECT SPECIFICATIONS FOR MEASUREMENT AND PAYMENT.
- UNLESS OTHERWISE NOTED HORIZONTAL DIMENSION FOR DRIVEWAYS SHALL BE GOVERNED BY MAXIMUM SLOPE REQUIREMENTS AND AS DIRECTED BY THE ENGINEER. ANY DIMENSION GREATER THAN 5'-FEET SHALL BE COORDINATED WITH THE ENGINEER FIRST. ANY AREA NOT PRE-APPROVED BY THE ENGINEER SHALL NOT BE PAID.
- UNLESS OTHERWISE NOTED TOTAL HORIZONTAL DIMENSION OF GRAVEL DRIVE APRONS SHALL BE 5'-FEET WITH A MAXIMUM OF 2'-FEET BEING PAVED. THE REMAIN DISTANCE SHALL BE CRUSHED GRAVEL.
- MAXIMUM 8% SLOPE FOR DRIVEWAY APRONS MAY BE MODIFIED ONLY AS DIRECTED BY THE ENGINEER BASED ON FIELD CONDITIONS.
- GUTTER TRANSITION ("LIP"): ALL DRIVEWAYS SHALL RECEIVE A GUTTER TRANSITION WITH AN INCLINE OF 2-INCHES IN 2'-FEET FROM THE EDGE OF PAVEMENT. NO ADDITIONAL COMPENSATION SHALL BE GRANTED FOR TRANSITIONS.
- TACK COAT SHALL BE APPLIED TO THE JOINTS AND AS NEEDED AND AS DIRECTED AND SHALL BE SUBSIDIARY TO UNIT ITEM COST.
- GRAVELS SHALL BE COMPACTED TO 95% MODIFIED PROCTOR.
- SUB-BASE SHALL BE COMPACTED AS NEEDED AND/OR AS DIRECTED.

#### NOTES:

- SIGNS SHALL MEET THE REQUIREMENTS OF THE CURRENT EDITION THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- SIGNS SHALL BE GREEN WITH WHITE LETTERS. NO BORDER.
- THE FIRST LETTER OF THE NAME SHALL BE 6" UPPER CASE. THE REMAINING LETTERS SHALL BE 4.5" LOWER CASE.
- THE FIRST LETTER OF ROAD INDICATORS (AVE, ST, RD) SHALL BE 4". THE REMAINING LETTERS SHALL BE 3.5"
- TOP AND BOTTOM EDGES OF THE SIGN SHALL BE EXTRUDED CONSTRUCTION.



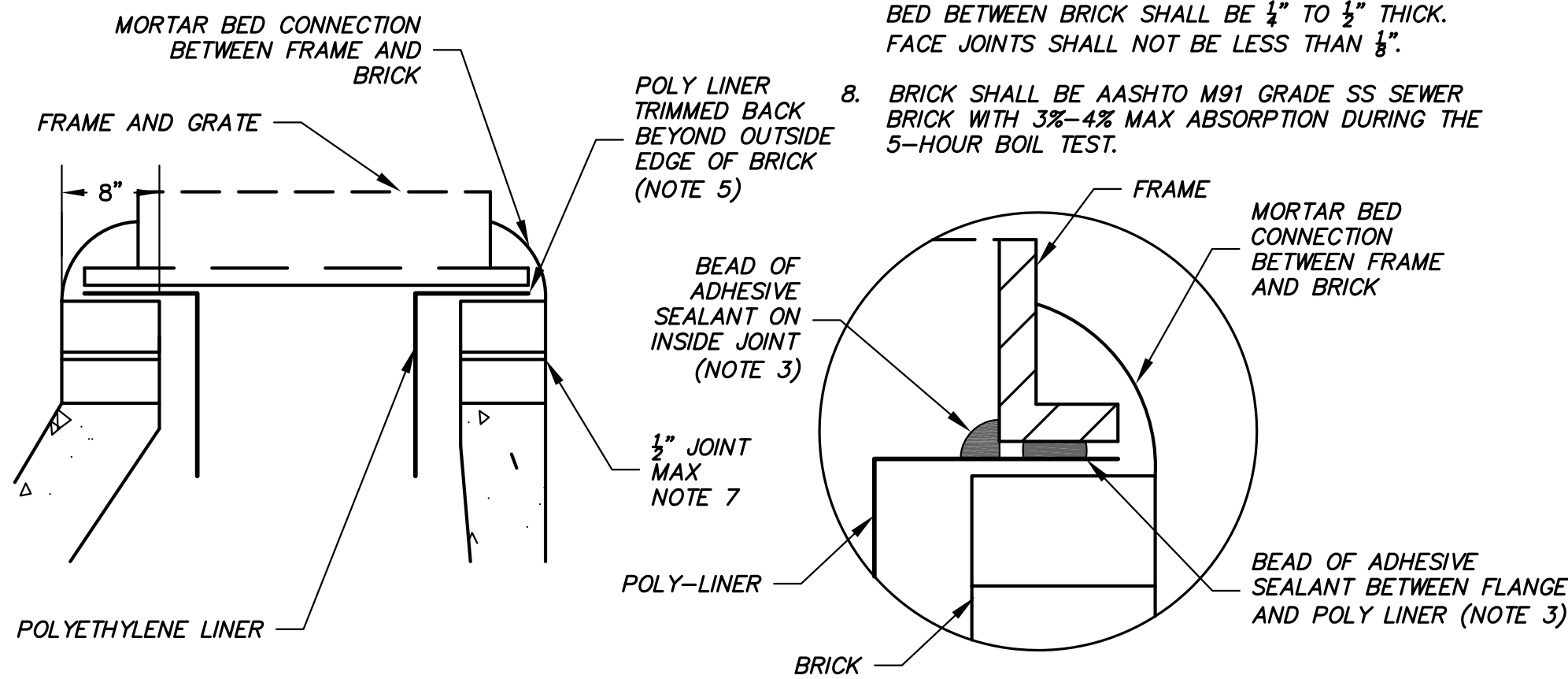
#### D3-1

### Street Name Signs

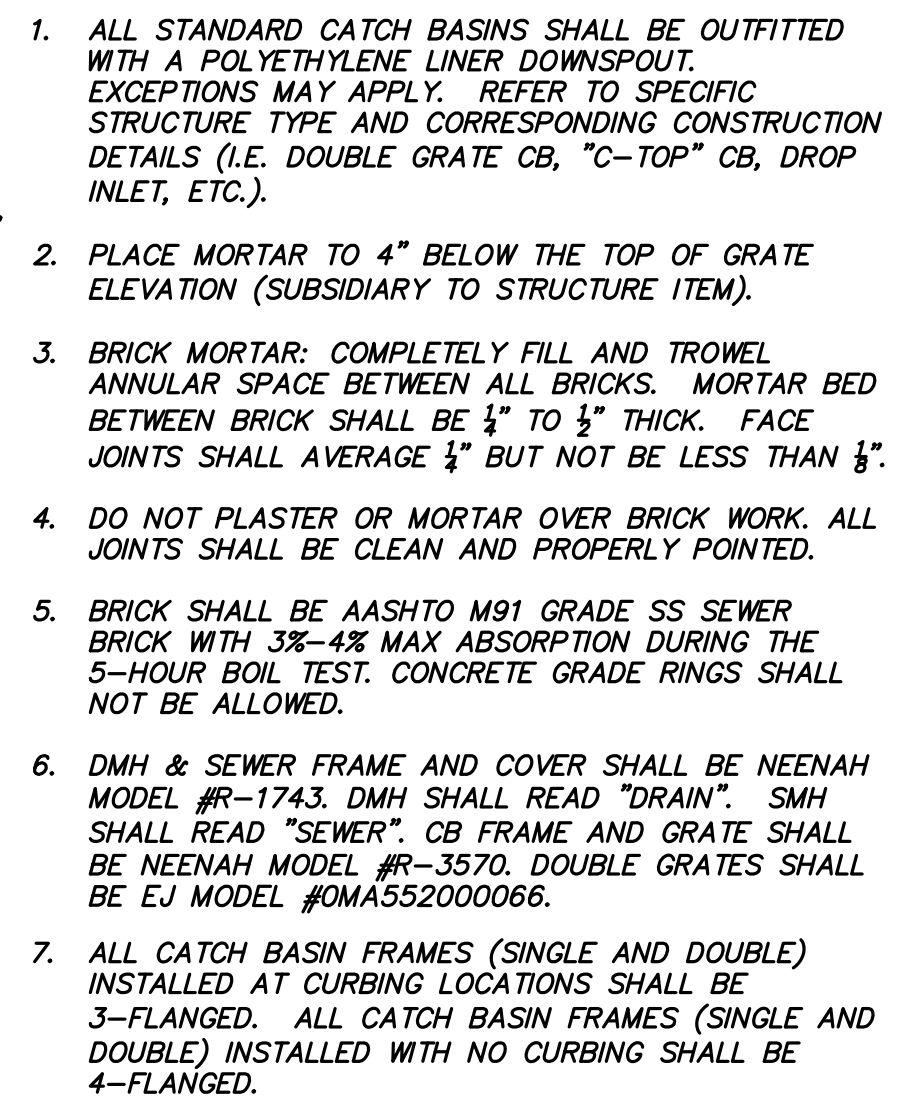
TOWN OF SALEM 33 GEREMONTY DRIVE SALEM, NH 03079				OWNER:	
2016 ROADWAY IMPROVEMENT PROGRAM				SALEM, NH 03079 ASSESSORS MAP 22, 55, 65, 73	
CIVIL DETAILS				DATE: DEC 2015	
SCALE: N.T.S.				SHEET: D1	
12 OF 17				APPROVED: _____	
DESIGNED: JAD				CHECKED: _____	
DRAWN: JAD				REVISION	
NO. DATE				1 1/7/16	
REVISED TYPICAL SECTION					



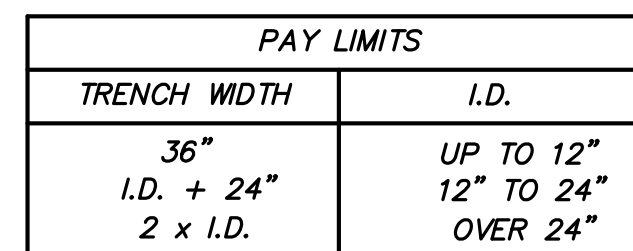
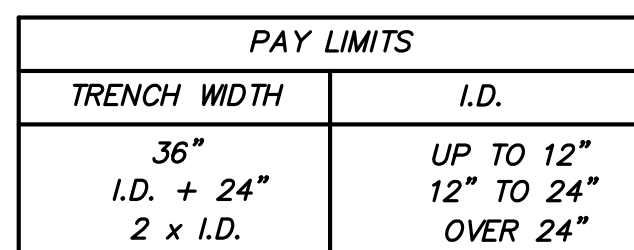
1. ALL CATCH BASINS SHALL BE OUTFITTED WITH A POLYETHYLENE LINER DOWNSPOUT.
2. POLYETHYLENE LINER SHALL BE FABRICATED AT THE SHOP. DOWNSPOUT SHALL BE EXTRUSION FILLET WELDED TO THE POLYETHYLENE SHEET.
3. PLACE A CONTINUOUS BEAD OF AN APPROVED BONDING ADHESIVE SEALANT BETWEEN FRAME AND POLYETHYLENE SHEET AND AT THE INSIDE JOINT AFTER ASSEMBLY IS COMPLETE.
4. PLACE CLASS AA CONCRETE TO 2" BELOW THE TOP OF GRATE ELEVATION (SUBSIDIARY TO DRAINAGE STRUCTURE).
5. TRIM POLYETHYLENE BEYOND THE OUTSIDE EDGE OF BRICK TO PROVIDE A MORTAR CONNECTION BETWEEN THE FRAME AND BRICK. ALTERNATE TRIMMING METHODS MAY/SHALL BE REQUIRED BY THE OWNER WHEN USED WITH CURBING AND GUTTER INLETS.
6. THE CENTER OF THE GRATE & FRAME MAY BE SHIFTED A MAXIMUM OF 1" FROM THE CENTER OF THE DOWNSPOUT IN ANY DIRECTION.
7. BRICK MORTAR: COMPLETELY FILL AND TROWEL ANNULAR SPACE BETWEEN ALL BRICKS. MORTAR BED BETWEEN BRICK SHALL BE  $\frac{1}{4}$ " TO  $\frac{1}{2}$ " THICK. FACE JOINTS SHALL NOT BE LESS THAN  $\frac{3}{8}$ ".
8. BRICK SHALL BE AASHTO M91 GRADE SS SEWER BRICK WITH 3%-4% MAX ABSORPTION DURING THE 5-HOUR BOIL TEST.



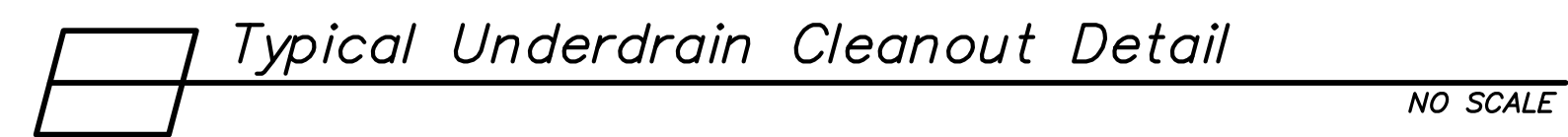
1. IF SITE CONDITIONS DO NOT ALLOW FOR SETTING CATCH BASINS AS SHOWN, THE CONTRACTOR SHALL NOTIFY THE ENGINEER PRIOR TO SETTING STRUCTURE.
2. CONTRACTOR SHALL VERIFY ALL STRUCTURE LOCATIONS PRIOR TO LAYING PIPE
3. ALL CB RIMS (SINGLE AND DOUBLE) AGAINST CURBING SHALL BE
  - 3-FLANGED. ALL CB RIMS NOT AGAINST CURBING SHALL BE 4-FLANGED.



1. REFERENCE TOWN OF SALEM STANDARD SPECIFICATIONS FOR METHOD OF SALEM PAVEMENT AND PAYMENT.
2. PAVEMENT REPAIR IN EXISTING ROADWAYS SHALL CONFORM TO STREET OPENING REGULATIONS.
3. ALL LOCAL STATE AND FEDERAL SAFETY STANDARDS SHALL BE STRICTLY ADHERED TO.
4. NEW ROADWAY CONSTRUCTION SHALL CONFORM TO TOWN OF SALEM SUBDIVISION REQUIREMENTS
5. TRENCH PATCH: AFTER THE BASE COURSE HAS BEEN ROLLED TO THE REQUIRED GRADE, ANY BROKEN OR IRREGULAR EDGES OF THE EXISTING PAVEMENT SHALL BE SAW CUT IN STRAIGHT LINES LEAVING A SOUND VERTICAL FACE 24-INCHES BACK FROM THE EDGE OF THE TRENCH OR OTHER EXCAVATIONS TO ACCEPT PLACEMENT OF A 24-INCH MINIMUM OVERLAP OF BITUMINOUS BASE COURSE PAVEMENT ON UNDISTURBED MATERIAL.
6. BITUMINOUS PAVEMENT, DEPTH EQUAL TO EXISTING PAVEMENT WITH 4" MIN. (1.5" OF  $\frac{1}{2}$ " WEARING, 2.5" OF  $\frac{3}{4}$ " BINDER). PAVEMENT SHALL CONFORM TO NHDOT ITEM 403.11.
7. DAMAGED OR OTHERWISE DEFICIENT PIPE SHALL BE REJECTED AND REMOVED FROM THE JOB SITE
8. INSPECTION: FOLLOWING INSTALLATION DRAIN LINES SHALL BE CLEANED AND VISUALLY INSPECTED. PIPES SHALL BE TRUE TO LINE AND GRADE PRIOR TO ACCEPTANCE AND USE.
9. UNSUITABLE MATERIAL & OVER EXCAVATION: ANY EXCAVATION OUTSIDE OF DEFINED PAY LIMIT SHALL BE STRICTLY COORDINATED AND MEASURED WITH THE ENGINEER FOR PAYMENT. ANY MATERIAL REMOVED WITHOUT PRIOR AUTHORIZATION SHALL NOT BE PAID. EXCAVATION AREAS SHALL BE BACKFILLED WITH APPROPRIATE BEDDING MATERIALS. UNSUITABLES WITHIN TRENCH PAY LIMITS ARE SUBSIDIARY.
10. MATERIAL SHALL BE REPLACED IN KIND OF WHENEVER POSSIBLE.
11. SUITABLE MATERIAL: IN ROADS, ROAD SHOULDERS, WALKWAYS AND TRAVELED WAYS, SUITABLE MATERIAL FOR TRENCH BACKFILL SHALL BE THE NATURAL MATERIAL EXCAVATED DURING THE COURSE OF CONSTRUCTION, SHALL EXCLUDE DEBRIS, REMAINS OF PAVEMENT, ORGANIC MATTER, ROCK SOIL, ALL WET OR SOFT MUCK, PEAT OR CLAY, ALL EXCAVATED LEDGE MATERIAL AND ALL ROCKS OVER SIX INCHES IN THE LARGEST DIMENSION, OR ANY MATERIAL WHICH, AS DETERMINED BY THE TOWN OF SALEM DEPARTMENT OF ENGINEERING, WILL NOT PROVIDE SUFFICIENT SUPPORT OR MAINTAIN THE COMPLETED CONSTRUCTION IN A STABLE CONDITION. SUITABLE MATERIAL SHALL BE PLACED IN 6" LIFTS AND THOROUGHLY COMPACTED.
12. COMPACTION: BACKFILL OF THE TRENCHES SHALL BE COMPACTED TO TO 95% MAX. DRY DENSITY UNDER ALL PAVED AREAS AND 92% MAX. DRY DENSITY UNDER OTHER AREAS IN ACCORDANCE WITH NHDOT STANDARD SPECIFICATIONS -- SECTION 304.
13. IF TRENCH BOTTOM IS DISTURBED THEN CONTRACTOR SHALL COMPACT AS APPROPRIATE.



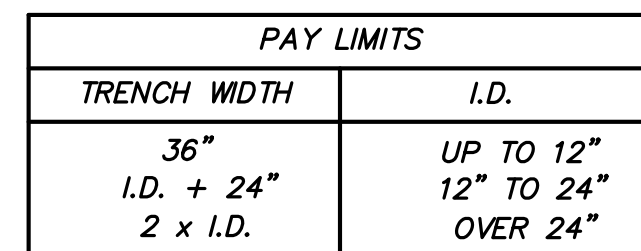
4. PROVIDE DELINEATOR FOR  
UNDERDRAIN CLEANOUT. NHDOT ITEM#  
621.331



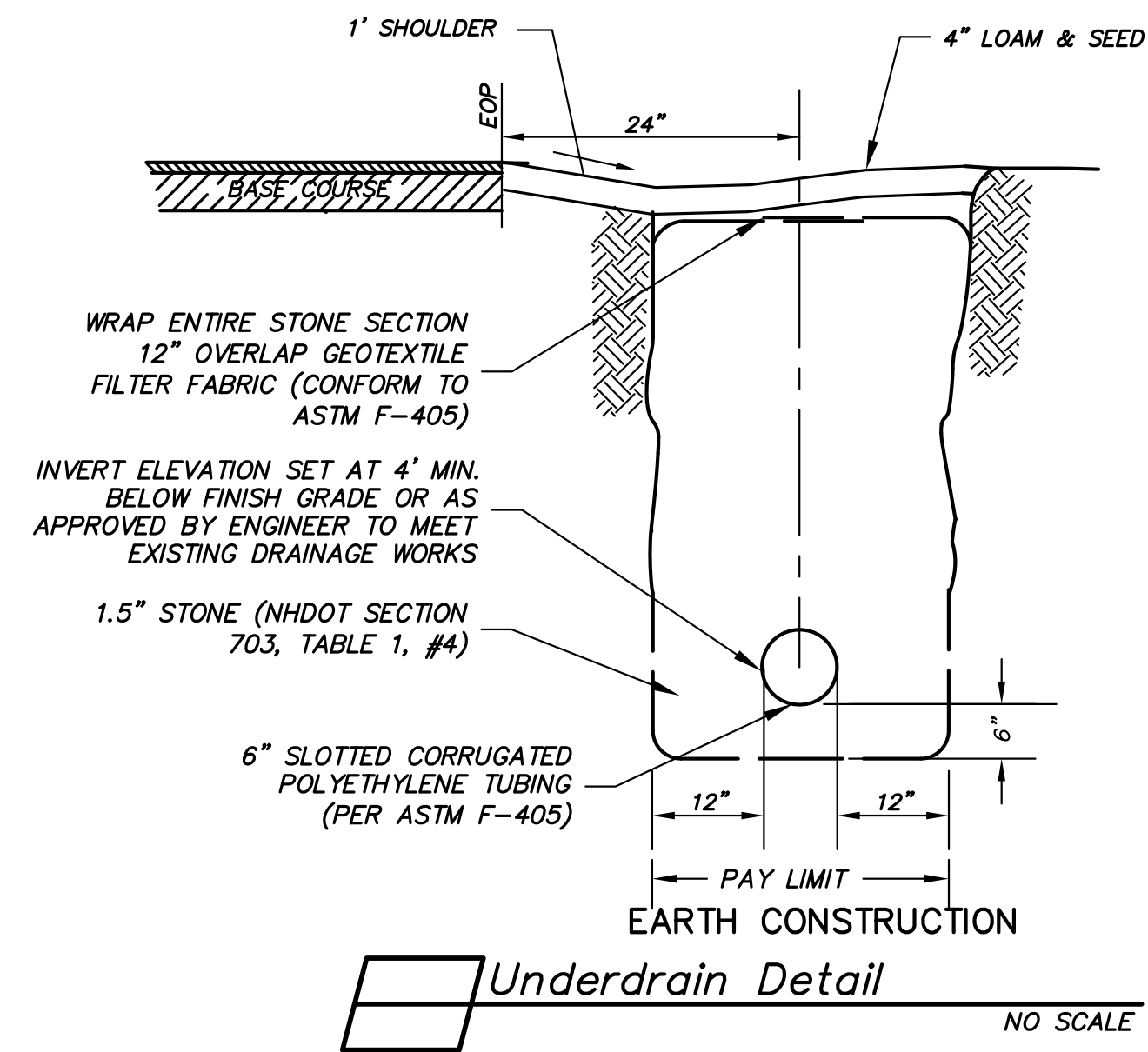
Standard Salem Pavement Depression Detail  
NO SCALE

NOTE: ALL STONE TO BE SIZE "C1" EXCEPT WHERE NOTED.

Typical Headwall & Stone Apron Detail  
NO SCALE



Typical Water Trench Detail



16. MOUNDING UNDER THE PIPE SHALL BE PROVIDED AT THE TIME OF PIPE INSTALLATION TO ENSURE PROPER PIPE ALIGNMENT, LEVEL TRENCH BOTTOM, AND PROPER DEPTH OF SAND BEDDING.



