

1. AGENDA

Documents:

[PB AGENDA-01-13-26.PDF](#)

2. 11 Mall Road - Game On & Loretta's Last Call

Documents:

[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-RTC-STAFF.PDF](#)
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-RTC-TRAFFIC.PDF](#)
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-SITE PLAN.PDF](#)
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-TRAFFIC AND
PARKING UTILIZATION STUDY.PDF](#)
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-CUP-SIGNAGE.PDF](#)
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-ELEVATIONS AND
SIGNAGE PACKAGE.PDF](#)
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-ELEVATIONS
LEGEND.PDF](#)

3. 59 Millville Street - Subdivision

Documents:

[59 MILLVILLE STREET-WAIVER REQUEST.PDF](#)
[59 MILLVILLE STREET-WAIVER REQUEST-MINIMUM BASEMENT FLOOR
ELEVATION.PDF](#)
[59 MILLVILLE STREET-SUBDIVISION PLAN.PDF](#)



Town of Salem, New Hampshire

Community Development Department - Planning Division
33 Geremonty Drive, Salem, New Hampshire 03079
(603) 890-2080 - Fax (603) 898-1223

PLANNING BOARD AGENDA FOR JANUARY 13, 2026 - 7:00 PM

Knightly Meeting Room, Salem Town Hall

ROLL CALL:

____ Joseph Feole, Chair	____ Bianca Carlson	____ Beverly Donovan, Alt.
____ Sean Lewis, Vice Chair	____ Jim Harvey	____ Victor Helenic, Alt.
____ Damon Norcross, Secretary	____ Chuck Saba	
____ Jeff Hatch, Town Council Rep.		

REVIEW OF MINUTES

1. December 9, 2025 – Regular Meeting

OLD BUSINESS

1. **Salem NIM, LLC Site Plan** – Public Hearing for change of use for 19,960 sq. ft. restaurant and entertainment use with conditional use permit for reduced parking and additional signage at 11 Mall Road, Map 107, Lot 11154. Continued from 12/9/2025.

BUSINESS

1. **Canobie Lake Toyota Site Plan** – Public Hearing for conditional use permit for additional wall signage at 417 South Broadway, Map 144, Lot 204.
2. **Fuschetti Subdivision Plan** – Preliminary Hearing for two-lot subdivision at 59 Millville Street, Map 81, Lot 2946.
3. **Suraj Realty, LLC Site Plan** – Public Hearing for amended site plan and phasing scheduled for 6,000 sq. ft. office, vehicle maintenance and dry ice manufacturing building, 20,000 sq. ft. warehouse/storage building, and paved tractor trailer storage area with conditional use permit for reduced wetland setback at 44 Pelham Road, Map 96, Lot 7775.

PUBLIC MATTERS

1. Unscheduled

PLANNING BOARD MATTERS

1. Proposed Zoning Amendments
2. Correspondence
3. Other

Note: No new agenda items will be heard after 10:30 PM. If necessary, the Board will recess to another date to take up the remainder of the agenda.

Per Section 13.7 of the Planning Board Rules of Procedure, all discussions between Planning Board members and applicants or their agents regarding matters to be decided by the Board shall take place at public hearings as part of the public record.

100 Cambridge Street, Suite 1310 Boston, MA 02114 T: 617.824.9100 F: 617.824.9101

To: Jacob LaFontaine

From: Richard Schlichting

Date: December 29, 2025

Re: Exterior design
11 Mall Road-Loretta's Last Call & Game On!
Langan Project No.: 151064501

Below please find our responses comments discussed in our December 9, 2025 meeting and at the PB meeting. Below please find each comment followed by our response in **bold**.

- Elevations:
 - Add legend – materials and color

COMMENT RESPONSE: Additional page has been added to the plans with the legend.
 - See [Design Guidelines](#) for Façade Design – pages 30-33
 - “For retail structures, any facade that faces a public or private street should have display windows, entry areas, or other transparent features along 40% or more of its horizontal length... Facades should not extend for more than 50 horizontal feet in length without incorporating architectural features, such as pilasters, windows, cornices, porches, corners, or offsets.”

COMMENT RESPONSE: This concern was addressed by adding the large window on either side of the entrance as well as two additional large windows beyond those two windows that are decorative.
 - Please see staff comments: “elevation drawings provided: proposing to retain significant portion of existing façade and paint to match Fenway Park motif – existing brick to be painted red, existing concrete wall to be painted green, cornice to be painted black; adding new Game On entryway on west side of building with brick columns, - have eliminated awning, patio enclosed by black steel fence, new windows and new projection on façade for signage; Loretta’s Last Call to have western motif – rusted tin roof panel, catenary lights, new windows, brick façade entrance, new patio enclosed with rebar and wood railing

fence; adding new brick for equipment storage and dumpster enclosure. Proposed modifications are attractive and welcomed improvements however building has blank walls on large portions of western, northern and eastern facades and eliminated transparency on western facade Elevations should still be revised to better comply with Site Plan Regulations and Design Guidelines (see excerpts attached) – specifically to reduce blank walls, increase transparency, vary roofline; dumpster enclosure should be revised to match building.”

COMMENT RESPONSE: This concern was addressed by adding the large window on either side of the entrance as well as two additional large windows beyond those two windows that are decorative. In addition, we have added banners on the North side of the building as well as an additional pilaster on the east face behind the equipment enclosure.

- Signage:

- Add true sign dimensions instead of “rough dimensions”

COMMENT RESPONSE: Sizes are true sizes. The sizes have been called out on the Signage Package, on the Elevations and in the Signage CUP.

- Revise CUP to include sign dimensions and quantity of signage – please find example attached

COMMENT RESPONSE: The Signage CUP has been updated to note all dimensions, locations and quantities of each sign.

- Please try to conform with the [Tuscan Village Sign Standards](#) which allow for more flexibility – One of your Kings locations is actually used as an example for a tagline sign (see page 15)
- Please also refer to Design Guidelines – see pages 81-92 for Signage

COMMENT RESPONSE: The new signage plan and elevations has been discussed and worked through with the Planning Director to ensure the signage plan is matching the aesthetic of the area and following guidelines.

- Site Plan:

- Add pedestrian connection to north sidewalk

COMMENT RESPONSE: Curb ramp with crosswalk has been added to the plans. Please see CS101.

- Add tip down at ADA spaces to ped. connection to south

COMMENT RESPONSE: Tip down to ADA spaces has been added to south sidewalk connection, as seen on CS101.

- Add dimension of striped space to south

COMMENT RESPONSE: Dimensions have been added to restriped southern parking area, seen on CS101.

- Clarify square footage: 19,775 sf or 19,960?

COMMENT RESPONSE: Square footage has been updated to 19,960 SF.

- Note height of flagpole (max 80ft)

COMMENT RESPONSE: Exact flagpole height to be coordinated as construction plans progress. Flagpole will be less than 80 feet.

- Eliminate zoning table on Existing Conditions Plan

COMMENT RESPONSE: Zoning Table has been removed from VT901.

- Clarify materials and colors of dumpster enclosure – site plan and elevations conflict
 - Please note Design Guidelines state “Structural screens and fencing should complement the design of the main structure by repetition of materials, detailing, scale, and color.”

COMMENT RESPONSE: The equipment enclosure will mimic the building with the Brick wainscoting and Fenway green. The garbage enclosure will be black chainlink fence with black slats to hide the dumpster.

- Show existing landscaping in addition to proposed – Design Guidelines state “parking lots with 10 or more spaces should have at least one tree per eight spaces, planted in or within five feet of the lot... Larger and more visible parking lots should have more intensive landscape treatments.
 - Please note any dead, diseased, dying or missing vegetation will be replaced

COMMENT RESPONSE: Regarding the landscaping comments for the Lyons Group project, we would like to clarify that the proposed design actually decreases the total parking count by 16 spaces. This reduction means the project is well within the required thresholds; however, we are still proposing additional trees beyond the required one per eight spaces minimum.

Furthermore, we are not removing any existing trees, and the project remains in full compliance with the Town's Design Guidelines.

- PB Comments:
 - Enhance elevations and landscaping

COMMENT RESPONSE: Updated elevations and plans have been attached to this application.
 - Confirm whether overflow employee parking to north is available for use
 - Address parking agreement with Mall

COMMENT RESPONSE: Exhibit "B" Site Plan from 11/09/2023 has been attached to this comment response showing shared parking areas in the parking lot.
 - Connect sidewalk to pedestrian connection to Pleasant Street

COMMENT RESPONSE: Southern sidewalk now has a connection to the parking lot. This sidewalk connects to a crosswalk to cross at the intersection where they can head north to other pathing.
 - Submit truck turn plan for trash pickup

COMMENT RESPONSE: Truck turning plan has been added to the set on TM101.

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Comment Response\Response to Comments Memo 12-23-25.docx

December 29, 2025

Mr. Stephen G. Pernaw, P.E.
Stephen G. Pernaw & Company, Inc.
P.O. Box 1721
Concord, NH 03302

**Re: Proposed Reuse of Former Sears Auto Center
Langan Project #151064501**

Dear Mr. LaFontaine:

Enclosed please find our responses to your letter dated December 19, 2025. Below please find each comment followed by our response in **bold**.

1. It is not clear if the outdoor seating is included in the 337 seat total, as this could affect all study projections.

COMMENT RESPONSE: Outdoor seating is included in the 337 total seat count. A clarification has been added to the project description.

2. ITE description for LUC 932 does not necessarily reflect bowling lanes, pool tables, arcades, and a dancing venue...using "seats" rather than the "gross floor area" as the independent variable produced the lowest estimates...we believe it is prudent to rely on the higher GFA-based estimates for planning purposes (Estimate C).

COMMENT RESPONSE: We first attempted to use LUC 437 (Bowling Alley) to produce a parking demand rate but found that the land use generates very low parking demand. LUC 932 was determined to be the best fit for this development, as both "Game On!" and "Loretta's Last Call" will serve food and have table seating for customers. "Seats" was chosen to be the independent variable, as "Game On!" has large spaces that are not occupiable such as the 10 proposed bowling lanes within the establishment. Using square footage for the independent variable in this situation would produce overly conservative parking demand estimates, and it is our opinion that Alternate Estimate C is an overestimation.

3. The results reflect average parking demand rather than the 85th percentile parking demand

COMMENT RESPONSE: The report has been amended to include both average and 85th (peak) parking demand presented in Tables 1 and 2.

Key Finding 1: "Existing parking demand in Parking Lot C peaks at 28% utilization on Friday and 27% on Saturday in the evenings."

SGP Comment: This finding pertains to July 2025 conditions, and does not reflect peak-month conditions.

COMMENT RESPONSE: Confirmed, no comment.

Key Finding 2: “The combination of existing parking utilization and anticipated parking demand would be less than 50% utilization of Parking Lot C during off-peak season.”

SGP Comment: According to the Alternative Estimate C, the utilization of Lot C could be on the order of 65% during July conditions.

COMMENT RESPONSE: *Using 85th percentile parking demand, the utilization of Lot C is estimated to be 53%. This matches with Alternative Estimate A. The independent variable of seats was maintained due to the reasoning outlined in the response for Comment #2.*

Key Finding 3: “The Mall will require an amended Conditional Use Permit (CUP) to reduce the required parking to 4,563 to account for the removal of 16 parking spaces for the proposed restaurants.”

SGP Comment: We do not have sufficient documentation to verify that the required parking is 4,563 spaces. However, the removal of 16 parking spaces for the proposed restaurants is considered de minimis.

COMMENT RESPONSE: *Comment acknowledged.*

Key Finding 4: “The estimated trip generation for the proposed restaurants is 26 less evening peak hour trips and 549 daily trips less than the previous proposals.”

SGP Comment: The alternative trip estimates for the PM peak hour case range from 155 to 179 trips. When compared to the previous proposal estimate of 158 trips, it is reasonable to expect anything from a minor decrease to a small increase in trips. The weekday daily estimate on Table 2 (1,473 trips) should be used with caution, as this estimate is based on only one ITE data point. The weekday daily estimate for the two restaurants based on the 12th Edition (and the total GFA) results in an estimate of 2,023 trips (5 ITE data points). According to Table 2, the previously approved restaurant/retail uses generated 2,022 trips; essentially a “wash;” not a reduction.

COMMENT RESPONSE: *As reasoned in the response for Comment #2, it is our opinion that “seats” is a more appropriate independent variable to use for this development due to the large unoccupiable space taken up by the proposed bowling lanes. Therefore, we believe that the proposed development is expected to generate less peak hour and daily trips.*

CUP Request – It is our understanding that the request is for the removal of 16 parking spaces for the proposed restaurant project. Given that the required parking supply for the Mall exceeds four thousand spaces, 16 fewer spaces is considered de minimis.

COMMENT RESPONSE: *Comment acknowledged.*

Site Development Plan – A vehicle tracking analysis should be prepared that demonstrates: 1) compatibility with Salem fire apparatus, and 2) how the dumpsters will be serviced. According to Sheet CS101, 68 spaces are proposed within the subject site. It should be noted that the peak parking demand estimates range from 158 to 395 occupied spaces. This means that there is the

potential for the entire C1 parking section (with 373 net spaces) to be fully occupied during peak periods.

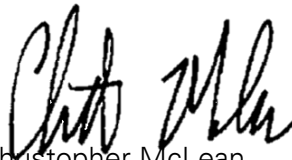
COMMENT RESPONSE: *Vehicle turning movement demonstrations are provided on Sheets TM101 and TM102. The 85th percentile (peak) parking estimates for Loretta's and Game On! are 266 spaces, which is 71% of the available parking spaces within Lot C1.*

Traffic Impact Fee – The Applicant should submit an impact fee calculation for Town review based on the current version of the “Impact Fees For New Development” schedule. The current rate for high-turnover restaurants is \$5.89 per sf. The impact fee calculation should reflect a “credit” for the former use (auto center).

COMMENT RESPONSE: *See the attached impact fee calculation.*

Sincerely,

Langan Engineering and Environmental Services, Inc.



Christopher McLean
Project Engineer

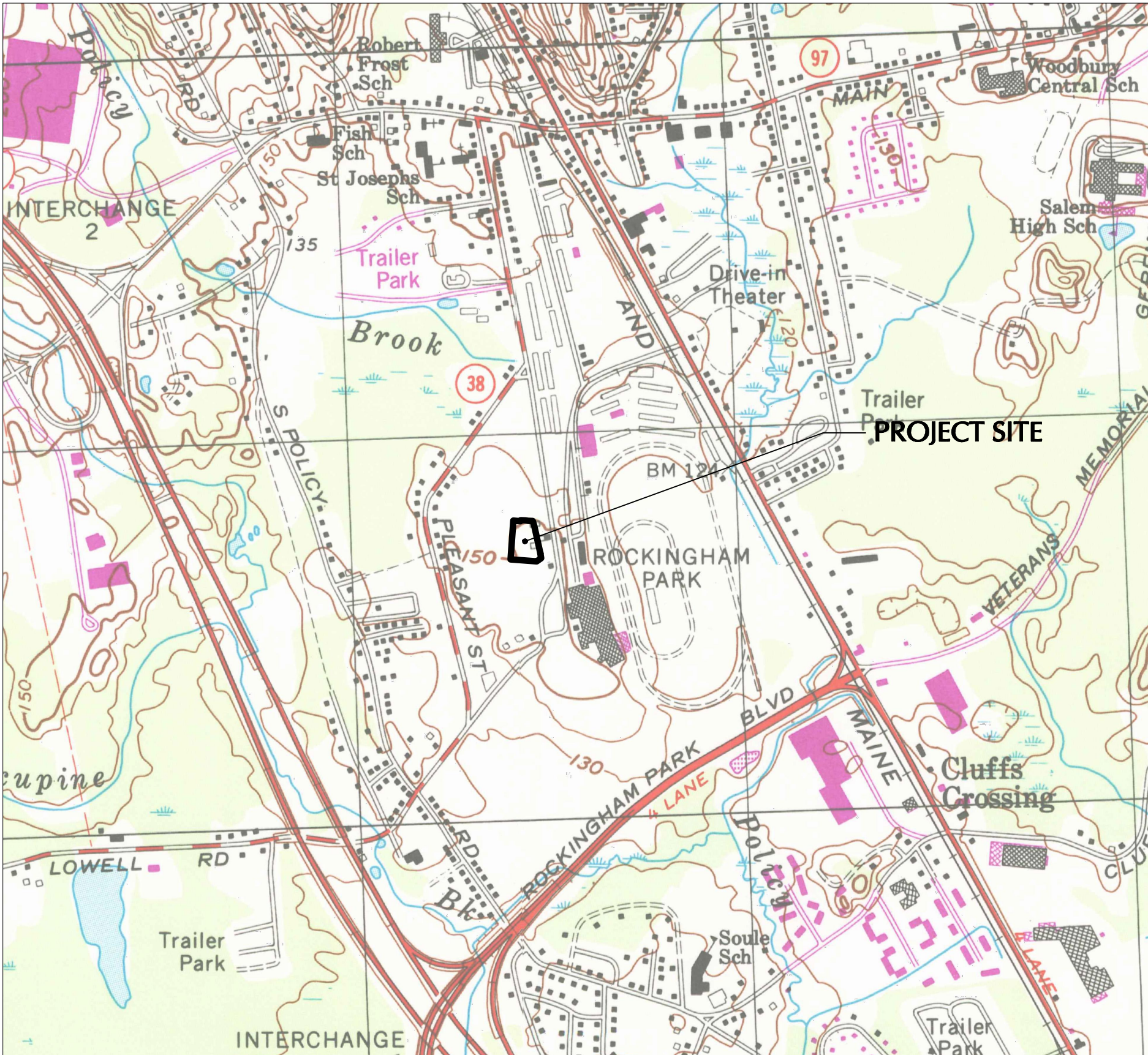
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Impact Fee Calculation Table				
	Existing		Proposed	
	Square Footage	\$/SF	Square Footage	\$/SF
Sears Auto Center (Small Retail)	19,900	\$ 5.57	-	
Loretta's Last Call & Game On! (High-Turnover Sit Down Restaurant)	-		19,900	\$ 5.89
Estimated Impact Fee	\$ 110,843.00		\$ 117,211.00	
New Impact Fee	\$ 6,368.00			

LYONS GROUP RESTAURANT
AT THE MALL AT ROCKINGHAM PARK
PLANNING BOARD SUBMISSION
MAP: 107 LOT: 11154
1 MALL ROAD
TOWN OF SALEM, ROCKINGHAM COUNTY, NEW HAMPSHIRE

CIVIL DRAWING INDEX			
SHEET NUMBER	DRAWING TITLE	DATE	LAST REVISED
CS001	SITE COVER SHEET	11/18/2025	12/29/2025
CS002	MASTER LEGEND & NOTES	11/18/2025	-
CS003	AREA MAP	11/18/2025	-
VT901	EXISTING CONDITIONS SURVEY	10/23/2025	-
CD101	SITE PREPARATION & DEMOLITION PLAN	11/18/2025	-
CS101	EXTERIOR SITE PLAN	11/18/2025	12/29/2025
CS501	SITE DETAILS I	11/18/2025	-
CS502	SITE DETAILS II	11/18/2025	-
TM101	GARBAGE TRUCK MOVEMENT PLAN	12/29/2025	-
TM102	FIRE TRUCK MOVEMENT PLAN	12/29/2025	-
CG101	GRADING & DRAINAGE PLAN	11/18/2025	12/29/2025
CU101	UTILITY PLAN	11/18/2025	-
CU501	UTILITY DETAILS	11/18/2025	-
LL101	SITE LIGHTING PLAN	11/18/2025	-
LL501	SITE LIGHTING DETAILS	11/18/2025	-
LP101	PLANTING PLAN	11/18/2025	-
LP501	PLANTING DETAILS	11/18/2025	-



MAP REFERENCE: USGS SALEM QUADRANGLE (7.5-MINUTE SERIES)

LOCATION MAP

SCALE: 1" = 1000'

ABUTTERS LIST			
MAP	LOT	NAME	ADDRESS
98	11094	ROCKSAL MALL LLC C/O SIMON PROPERTY GROUP	PO BOX 6120 INDIANAPOLIS, IN 46207
98	12604	TUSCAN VILLAGE MASTER DEVELOPMENT LLC	PO BOX 1648 SALEM, NH 03079
107	12574	ARTISAN AT TUSCAN VIL DEV HOLDINGS, LLC	3520 PIEDMONT RD NE SUITE 410 ATLANTA, GA 30305
116	7884	ROCKSAL MALL LLC C/O SIMON PROPERTY GROUP	PO BOX 6120 INDIANAPOLIS, IN 46207

RELEASE DATES	
DATE	ISSUED FOR
11/18/2025	PLANNING BOARD SUBMISSION
12/29/2025	PLANNING BOARD RESPONSE TO COMMENTS

OWNER / APPLICANT
SALEM NIM, LLC
C/O: LYONS GROUP
355 CONGRESS STREET
BOSTON, MA 02210
PHONE: 617-262-2605

ARCHITECT
GRAVITY ARCHITECTURE & DESIGN, LLC
C/O: RAYMOND R SCHAEFER
777 WEST SMITH STREET
ORLANDO, FL 32804
PHONE: 407-832-0949

CIVIL ENGINEER, LAND SURVEYOR, TRAFFIC
ENGINEER & LANDSCAPE ARCHITECT
LANGAN ENGINEERING & ENVIRONMENTAL
SERVICES, INC
C/O: RICHARD SCHLICHTING
100 CAMBRIDGE STREET
BOSTON, MA 02114
PHONE: 617-824-9100

CS001	SITE COVER SHEET	
<div>Site Development Plan For: LYONS GROUP RESTAURANT AT THE MALL AT ROCKINGHAM PARK 11 MALL ROAD SALEM, NH Assessors Map 107 & Lot 11154</div>		
Prepared by: Langan Engineering & Environmental Services, LLC 100 Cambridge Street, Suite 1310 Boston, MA 02114		
Scale: As noted		October 30, 2025
	Owner: SALEM NIM, LLC C/O LYONS GROUP 355 CONGRESS ST BOSTON, MA 02210	Salem Planning Board Approval
Zoning Classification: COMMERCIAL-INDUSTRIAL DISTRICT, SUBDISTRICT C (CI-C)		

LANGAN

GENERAL NOTES

1. PLANIMETRIC & TOPOGRAPHIC INFORMATION SHOWN HEREON HAS BEEN OBTAINED FROM GROUND SURVEYS PERFORMED IN DECEMBER 2014 AND LAGAN CT, INC IN JUNE 2016 AND SUPPLEMENTED BY FURTHER GROUND SURVEYS PERFORMED BY LANGAN ENGINEERING & ENVIRONMENTAL SERVICES, INC IN MAY 2023.
2. THE SITE LIES IN ZONE X (UNSHADED) AS SHOWN ON THE "FLOOD INSURANCE RATE MAP" ROCKINGHAM COUNTY, NEW HAMPSHIRE, FEMA MAP NUMBERS 33015C0563E, EFFECTIVE MAY 17, 2005.
3. PROPOSED SITE WORK IMPROVEMENTS SHALL CONFORM TO ANY STANDARD DETAILS AND SPECIFICATIONS OF THE TOWN OF SALEM IN THE ABSENCE OF LOCAL STANDARDS. SITE WORK SHALL CONFORM TO THE REQUIREMENTS OF NEW HAMPSHIRE DOT STANDARD DETAILS.
4. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING DIG SAFE (WWW.DIGSAFE.COM), EXCAVATION TEST HOLES, PERFORMING TEST BORINGS, AND PERFORMING WHATEVER ADDITIONAL INVESTIGATION NECESSARY TO PROTECT AND MAINTAIN ALL EXISTING UTILITIES TO REMAIN THROUGHOUT THE CONSTRUCTION PERIOD. ANY CONFLICTS BETWEEN EXISTING UTILITIES AND PROPOSED UTILITIES DISCOVERED DURING CONSTRUCTION SHALL BE PROMPTLY REPORTED TO THE PROJECT ENGINEER.
5. SEE ARCHITECTURAL DRAWINGS FOR LOCATIONS, DIMENSIONS, AND DETAILS OF ALL DOORS, AND WALLS ASSOCIATED WITH THE BUILDING.
6. FOR AREAS OUTSIDE THE PROPERTY LINES, REPAIR AND/OR REPLACE ALL DAMAGE DONE TO EXISTING ELEMENTS (SIDEWALKS, PAVING, LANDSCAPING, ETC) AS REQUIRED BY OWNER AND/OR GOVERNING AUTHORITY.
7. ALL SIGNS AND PAVEMENT MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD AND NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION REGULATIONS.
8. CONTRACTOR SHALL PREVENT DUST, SEDIMENT AND DEBRIS FROM EXITING THE SITE AND SHALL BE RESPONSIBLE FOR CLEANUP, REPAIRS AND CORRECTIVE ACTION IF SUCH OCCURS, ADJOINING STREETS AND PROPERTIES TO BE KEPT FREE OF DEBRIS RESULTING FROM DEMOLITION AND SHALL BE CLEANED ON A DAILY BASIS OR AS NEEDED.
9. DUST CONTROL TREATMENTS SHALL BE APPLIED AS NECESSARY TO CONTROL AND REDUCE THE AMOUNT OF DUST WHICH MAY CAUSE OFF-SITE DAMAGE, BE A HEALTH HAZARD TO HUMANS, WILDLIFE AND PLANT LIFE, OR POSE A HAZARD TO TRAFFIC SAFETY.
10. ABBREVIATIONS:
- | | |
|---------------------------------------|--|
| ARCH = ARCHITECTURAL | R.O.W = RIGHT OF WAY |
| CONC. = CONCRETE | CO = CLEAN OUT |
| INV = INVERT | DIP = DUCTILE IRON PIPE |
| EX = EXISTING | PVC = POLYVINYL CHLORIDE PIPE (SDR-35) |
| HDPE = HIGH DENSITY POLYETHYLENE PIPE | OCS = OUTLET CONTROL STRUCTURE |
| PROP. = PROPOSED | RCR = REINFORCED CONCRETE PIPE |
| LF = LINEAR FEET | R&D = REMOVE & DISPOSE |
| RL = ROOF LEADER | R&R = REMOVE & REPLACE |
| TC = TOP OF CURB | THE TYPICAL |
| BC = BOTTOM OF CURB | SWL = SINGLE WHITE LINE |
| TW = TOP OF WALL | SYL = SINGLE YELLOW LINE |
| BW = BOTTOM OF WALL | CLCB = CURBLESS CATCH BASIN |
| HP = HIGHPOINT | MH = MANHOLE |
| LA = LANDSCAPED AREA | GR = GRATE |
| N.T.S. = NOT TO SCALE | TF = TOP OF FRAME |
| INV = INVERT | CB = CURB CATCH BASIN |
| SMH = SANITARY SEWER MANHOLE | YRD = YARD DRAIN |
| FFE = FINISHED FLOOR ELEVATION | FES = FLARED END SECTION |

CONTRACTOR NOTES

1. THESE PLANS REPRESENT THE OVERALL SITEWORK IMPROVEMENTS REQUIRED FOR PROJECT CONSTRUCTION. THE CONTRACTOR SHALL FURNISH, INSTALL, TEST AND COMPLETE ALL WORK TO THE SATISFACTION OF THE ENGINEER AND OWNER IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR MEANS AND METHODS OF CONSTRUCTION; AS SUCH, THESE PLANS DO NOT COMPLETELY REPRESENT, NOR ARE THEY INTENDED TO REPRESENT, ALL SPECIFIC INSTRUCTIONS REQUIRED FOR SITEWORK CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE TO CONSTRUCT ALL IMPROVEMENTS DEPICTED ON THESE PLANS IN ACCORDANCE WITH ALL APPLICABLE RULES, REGULATIONS AND LAWS IN EFFECT AT THE TIME OF CONSTRUCTION.
2. THE CONTRACTOR SHALL ACCEPT THE SITE AS IS. THE CONTRACTOR SHALL ASSESS CONDITIONS, AND THE KIND, QUALITY AND QUANTITY OF WORK REQUIRED. THE OWNER AND ENGINEER MAKES NO GUARANTEE IN REGARD TO THE ACCURACY OF ANY INFORMATION THAT WAS OBTAINED DURING INVESTIGATIONS. THE CONTRACTOR SHALL MAKE A THOROUGH SITE INSPECTION IN ORDER TO FIELD CHECK EXISTING SITE CONDITIONS; CORRELATE CONDITIONS WITH THE DRAWINGS, AND, RESOLVE ANY POSSIBLE CONSTRUCTION CONFLICTS WITH THE OWNER AND ENGINEER PRIOR TO COMMENCEMENT OF WORK. THE CONTRACTOR SHALL PERFORM ADDITIONAL TOPOGRAPHIC SURVEYS HE/SHE DEEMS NECESSARY, PROVIDED THEY ARE COORDINATED WITH THE OWNER. ANY CONDITIONS DETERMINED BY THE CONTRACTOR THAT DIFFER FROM THE INFORMATION SHOWN ON THE DRAWINGS THAT ARE NOT BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER PRIOR TO THE START OF WORK SHALL NOT BE CONSIDERED GROUNDS FOR ADDITIONAL PAYMENT OR CHANGES TO THE CONTRACT DURATION, OR ANY OTHER CLAIMS AGAINST THE OWNER OR OWNER'S ENGINEER.
3. THE CONTRACTOR SHALL, WHEN HE/SHE DEEMS NECESSARY, PROVIDE A WRITTEN REQUESTS FOR INFORMATION (RFI) TO THE OWNER AND/OR OWNER'S DESIGNATED REPRESENTATIVE, AND ENGINEER PRIOR TO THE CONSTRUCTION OF ANY SPECIFIC SITEWORK ITEM. THE (RFI) SHALL BE IN A FORM ACCEPTABLE TO OWNER AND/OR OWNER'S DESIGNATED REPRESENTATIVE, AND ENGINEER AND SHALL ALLOW FOR A MINIMUM OF THREE WORK DAYS FOR A WRITTEN REPLY. RFIS SHALL BE NUMBERED CONSECUTIVELY BY DATE SUBMITTED. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITEWORK ITEMS CONSTRUCTED DIFFERENTLY THAN INTENDED OR AS DEPICTED ON THE PLANS.
4. INFORMATION RELATED TO ELEVATIONS AND PROPOSED UTILITIES (SUCH AS ROADWAY GRADES, INVERT ELEVATIONS, RIM ELEVATIONS, GRATE ELEVATIONS, BUILDING FINISHED FLOOR ELEVATIONS, ETC.) MAY BE FOUND IN MORE THAN ONE LOCATION IN THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL SUFFICIENTLY REVIEW ALL PLANS, PROFILES AND ANY OTHER INFORMATION IN THE CONTRACT DOCUMENTS FOR CONSISTENCY PRIOR TO BID. ANY INCONSISTENCIES OR DISCREPANCIES THAT ARE FOUND BY THE CONTRACTOR OR HIS ASSIGNS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER IN WRITING, IN THE FORMAT OF AN RFI PRIOR TO BID.
5. THERE ARE ADDITIONAL NOTES, SPECIFICATIONS AND REQUIREMENTS CONTAINED THROUGHOUT THE PLAN SET AS WELL AS REFERENCES TO SPECIFICATIONS FROM APPLICABLE GOVERNING AUTHORITIES AND INDUSTRY STANDARDS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN, REVIEW AND ADHERE TO ALL THESE DOCUMENTS.
6. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT ALL CONSTRUCTION STAKEOUT FOR THIS PROJECT MUST BE COMPLETED FROM THE SITE SPECIFIC SURVEY CONTROL (HORIZONTAL AND VERTICAL) UPON WHICH THE DESIGN IS BASED. THE CONTRACTOR SHOULD NOT RELY ON RE-ESTABLISH SURVEY CONTROL BY GPS OR OTHER METHODS FOR USE IN CONSTRUCTION STAKEOUT OR ANY OTHER PURPOSE FOR THIS PROJECT. ANY DISCREPANCIES BETWEEN THE EXISTING HORIZONTAL OR VERTICAL DATA SHOWN ON THESE DRAWINGS AND THAT ENCOUNTERED IN THE FIELD MUST BE REPORTED TO THE DESIGN TEAM PRIOR TO CONSTRUCTION FOR RESOLUTION.

DEMOLITION NOTES

- GENERAL:
1. THE CONTRACTOR SHALL INSPECT THE SITE THOROUGHLY AND FIELD-VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITIES. SOME UNDERGROUND UTILITIES MAY BE LOCATED DIFFERENTLY THAN SHOWN ON THIS PLAN. IN ADDITION, THERE MAY BE EXISTING UNDERGROUND UTILITIES PRESENT THAT ARE NOT SHOWN ON THIS PLAN. ALL CONTRACTORS USING THESE PLANS FOR DEMOLITION OR CONSTRUCTION SHALL CONFIRM ALL UTILITIES IN THE FIELD TO ESTABLISH LOCATIONS, SIZES, MATERIALS, AND ELEVATIONS. ANY DISCREPANCIES SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND THE CONSTRUCTION MANAGER.
2. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING DIG SAFE, EXCAVATING THE HOLES, AND PREPARING ANY ADDITIONAL RESEARCH OR INVESTIGATIONS REQUIRED IN ORDER TO ENSURE THAT PROPOSED UTILITIES TO REMAIN ARE PROTECTED FROM DAMAGE DURING DEMOLITION. ANY DAMAGE TO EXISTING UTILITIES TO REMAIN IS REQUIRED TO BE REPAIRED BY THE CONTRACTOR.
3. THE CONTRACTOR SHALL NOTIFY AND OBTAIN ALL SHUTOFFS FOR ALL APPLICABLE UTILITIES PRIOR TO THE COMMENCEMENT OF DEMOLITION.
4. THE CONTRACTOR SHALL LOCATE/CONFIRM ALL DRAINAGE INFRASTRUCTURE AND MAINTAIN ADEQUATE STORM DRAINAGE THROUGHOUT CONSTRUCTION.
5. THE CONTRACTOR SHALL FIELD LOCATE AND PROPERLY DISCONNECT APPROPRIATE LATERALS TO LIVE MAINS PRIOR TO DEMOLITION AND IN ACCORDANCE WITH UTILITY COMPANY STANDARDS.
6. THE CONTRACTOR SHALL MAINTAIN THE SAFETY AND PROTECTION OF BOTH PEDESTRIANS AND MOTORISTS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE ALL NECESSARY PRECAUTIONARY AND PROTECTIVE MEASURES, INCLUDING SIGNS, LIGHTS, BARRIERS, FENCING AND TRAFFIC FLAGMEN.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY DEMOLITION ASSOCIATED PERMITS.
8. CONTRACTOR TO PROTECT AND MAINTAIN ALL UTILITIES TO REMAIN UNTIL TEMPORARY OR PERMANENT NEW LINES ARE INSTALLED.
- DEMOLITION:
9. THE CONTRACTOR SHALL REMOVE AND LEGALLY DISPOSE OF ALL STRUCTURES WITHIN THE PROJECT LIMITS INDICATED TO BE REMOVED.
10. STUMPS SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH NEW HAMPSHIRE DEPARTMENT OF ENVIRONMENTAL SERVICES STANDARDS. PRIOR TOWN APPROVAL IS REQUIRED FOR ANY ON-SITE STUMP BURIALS, STUMPS CANNOT BE BURIED OR OTHERWISE DISPOSED OF WITHIN THE TOWN OWNED RIGHT-OF-WAY.
11. THE CONTRACTOR SHALL INCLUDE IN HIS BID ALL TEMPORARY FACILITIES AND SERVICES NECESSARY TO SATISFY FEDERAL, STATE, & LOCAL REQUIREMENTS INCLUDING BUT NOT LIMITED TO BRACING, SHORING, PAVEMENT REPAIR, FENCING, PEDESTRIAN AND VEHICLE ACCESS, CONCRETE PADS, ETC.

ACCESSIBILITY NOTES

1. WALKWAYS ALONG ACCESSIBLE ROUTES NOT TO EXCEED 5% RUNNING SLOPE AND 2% CROSS SLOPE.
2. ALL ADA PARKING SPACES AND PASSENGER LOADING ZONES SHALL NOT EXCEED 2% IN ANY DIRECTION.
3. ALL ADA EGRESSES SHALL BE FLUSH WITH ADJOINING MATERIAL UNLESS NOTED OTHERWISE.
4. ALL RAMPS SHALL NOT EXCEED 1:12 AND 2% CROSS SLOPE.
5. CONTRACTOR SHALL CONFIRM FIELD CONDITIONS ARE CONSISTENT WITH CONSTRUCTION DOCUMENTS AND NOTIFY LANGAN ENGINEERING & ENVIRONMENTAL SERVICES, INC. OF ANY COMPLIANCE ISSUES PRIOR TO CONSTRUCTION.
6. PROPOSED SITE WORK IMPROVEMENTS SHALL CONFORM WITH THE LATEST EDITION OF THE ADA STANDARDS FOR ACCESSIBLE DESIGN TO THE BEST OF THE DESIGNER'S KNOWLEDGE.

GRADING & DRAINAGE NOTES

1. ALL PROPOSED STORM DRAINAGE PIPING TO UTILIZE WATER-TIGHT JOINTS.
2. LOCATIONS AND ELEVATIONS OF ROOF LEADERS SHOULD BE COORDINATED WITH ARCHITECTURAL & MEP DRAWNGS PRIOR TO CONSTRUCTION.
3. CLEANOUTS SHALL BE PROVIDED FLUSH TO GRADE AT ALL LOCATIONS OF ROOF DRAIN INTERSECTIONS, BENDS AND UPSTREAM ENDS.
4. ALL REQUIRED STORM LATERALS SERVICING THE BUILDINGS SHALL BE COORDINATED AND CONSTRUCTED TO WITHIN FIVE FEET OF EACH BUILDING LATERAL ENTRANCE LOCATION AT THE INVERTS NOTED. ANY NECESSARY EXTENSIONS, RELOCATIONS, OR CORRECTIONS WITHIN FIVE FEET OF THE BUILDING NECESSARY TO COMPLETE CONNECTION OF LATERALS TO THE BUILDINGS SHALL BE MADE BY THE BUILDING CONTRACTOR.
5. CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE APPROPRIATE SIZES OF THE DRAINAGE STRUCTURES (CATCH BASINS, MANHOLES, YARD DRAINS, ETC.) TO ACCOMMODATE THE PIPING SHOWN.
6. STORM DRAINAGE PIPING INSTALLATION SHALL COMMENCE AT THE FURTHEST DOWNSTREAM POINT AND PROCEED UPSTREAM "IN THE DRY".
7. CONTRACTOR SHALL CONNECT ANY NEW ROOF DRAINS TO THE DRAINAGE SYSTEM. LOCATIONS AND ELEVATIONS OF ROOF LEADERS SHOULD BE COORDINATED WITH MEP AND ARCHITECTURAL DRAWINGS PRIOR TO CONSTRUCTION.
8. THE CONTRACTOR WILL BE REQUIRED TO CLEAN THE ENTIRE DRAINAGE SYSTEM OF ALL DEBRIS AND OBSTRUCTIONS BOTH DURING CONSTRUCTION AND AT THE END OF CONSTRUCTION PRIOR TO ACCEPTANCE. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, REMOVAL OF ALL FORMWORK FROM STRUCTURES, CONCRETE AND MORTAR DROPPINGS, CONSTRUCTION DEBRIS, AND DIRT. THE SYSTEM SHALL BE THOROUGHLY FLUSHED CLEAN AND THE CONTRACTOR SHALL FURNISH ALL NECESSARY HOSE, PUMPS, PIPE, AND OTHER EQUIPMENT THAT MAY BE REQUIRED FOR THIS PURPOSE. NO DEBRIS SHALL BE FLUSHED INTO EXISTING STORM DRAINS, WETLANDS, OR WATERCOURSES; ALL DEBRIS SHALL BE REMOVED FROM THE SYSTEM AND DISPOSED OF IN ACCORDANCE WITH ALL GOVERNING AGENCIES.
9. ALL MANHOLE COVERS, GRATES, INLETS, AND RIMS TO REMAIN SHALL BE ADJUSTED TO PROPOSED GRADE.
10. CONTRACTOR TO PROVIDE ALL FITTINGS AND BENDS NECESSARY TO ACCOMPLISH WORK.

LEGEND

	EXISTING	PROPOSED
PROPERTY LINE		
SETBACK LINE		
BUILDING LINE		
DOOR LOCATION		
6" CURB LINE		
TRAFFIC SIGN		
STEEL BOLLARD		
CONCRETE		
BITUMINOUS CONCRETE		
LANDSCAPE AREA		
GRAVITY SANITARY LINE		
WATER LINE		
UNDERGROUND TELECOMMUNICATIONS		
UNDERGROUND ELECTRIC		
UNDERGROUND GAS		
STORM PIPE		
STORM CATCH BASIN		
GATE VALVE		
HYDRANT		
CONTOUR		
SPOT ELEVATION		
COMPOST FILTER SOCK		
LIMIT OF DISTURBANCE		
INLET PROTECTION		
TREE		
MANHOLES		
MANHOLES		
POWER STRUCTURE		
BENCHMARK		
SAWCUT		
CURB REMOVAL		

CS002 MASTER LEGEND & NOTES

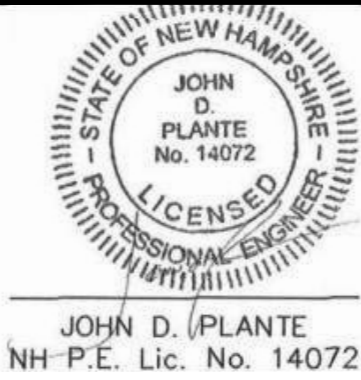
Site Development Plan For:
LYONS GROUP RESTAURANT
AT THE MALL AT ROCKINGHAM PARK
11 MALL ROAD
SALEM, NH

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC
100 Cambridge Street, Suite 1310
Boston, MA 02114

Scale: As noted

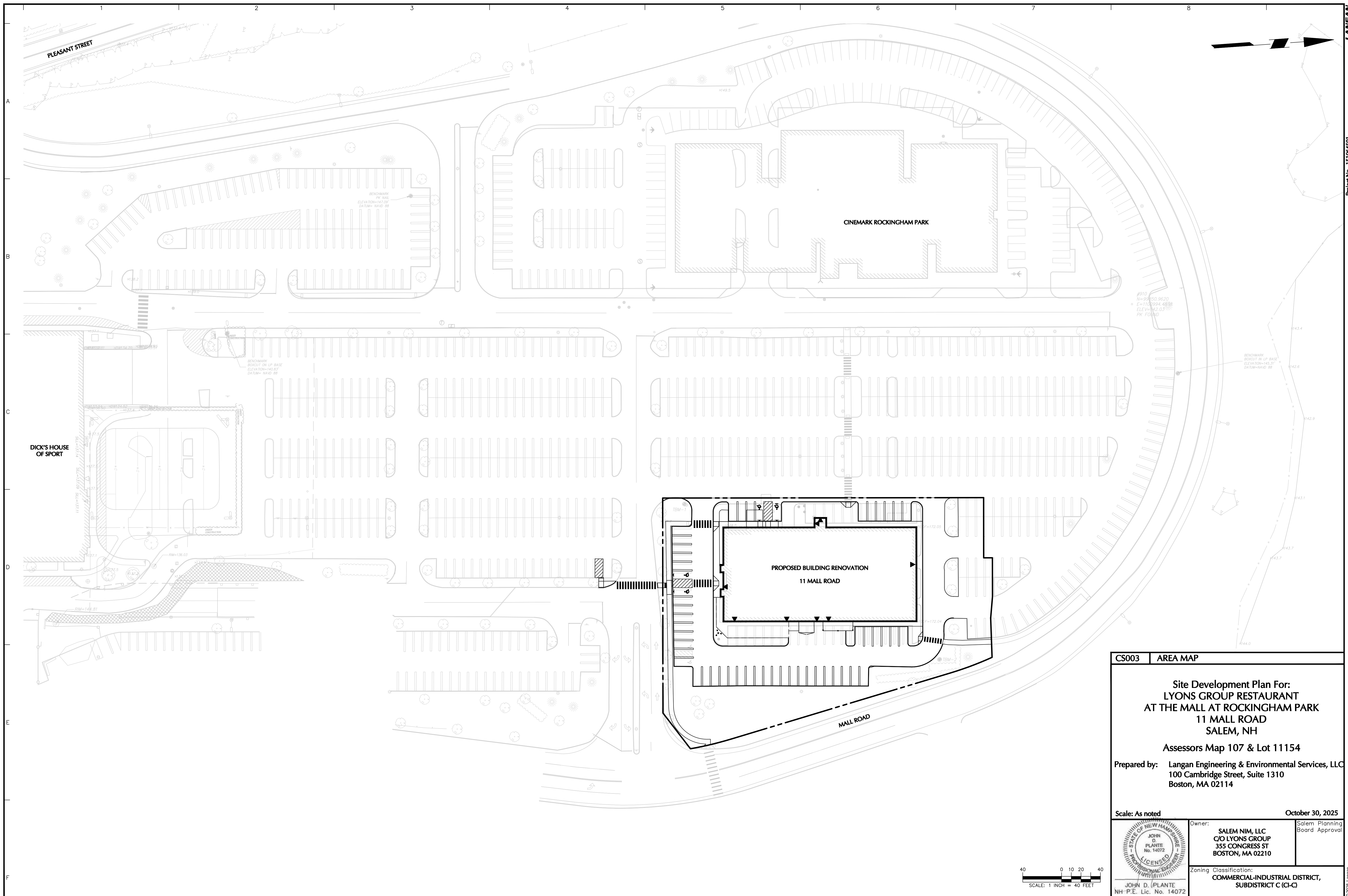
October 30, 2025



Owner: SALEM NIM, LLC
C/O LYONS GROUP
355 CONGRESS ST
BOSTON, MA 02210

Salem Planning Board Approval

Zoning Classification:
COMMERCIAL-INDUSTRIAL DISTRICT,
SUBDISTRICT C (CI-C)



NOTES

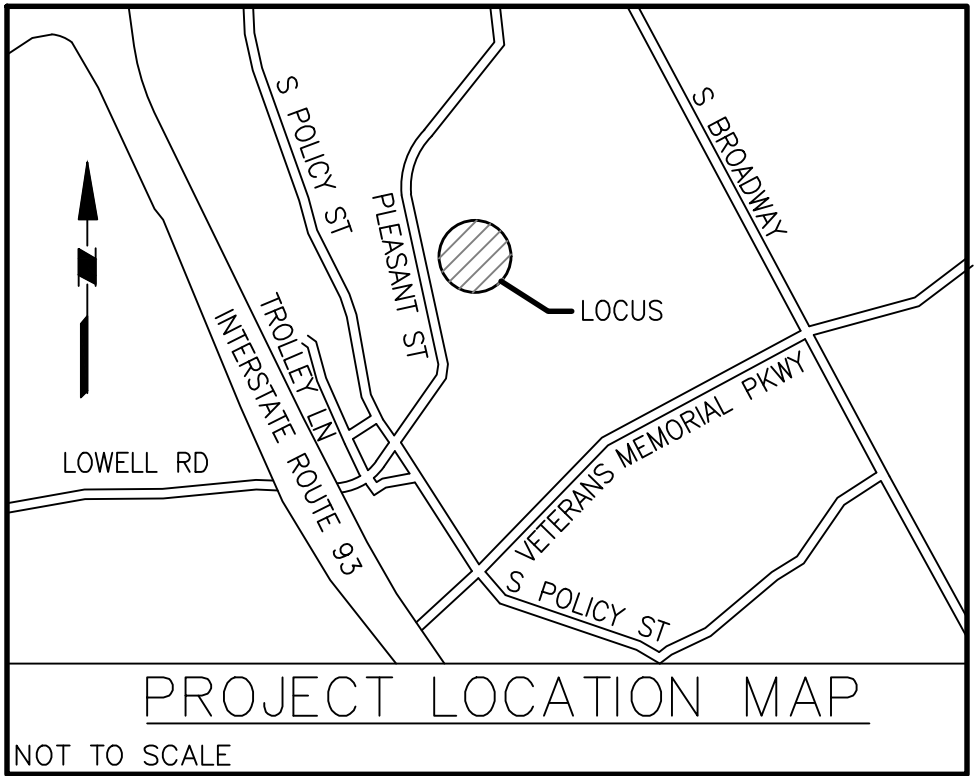
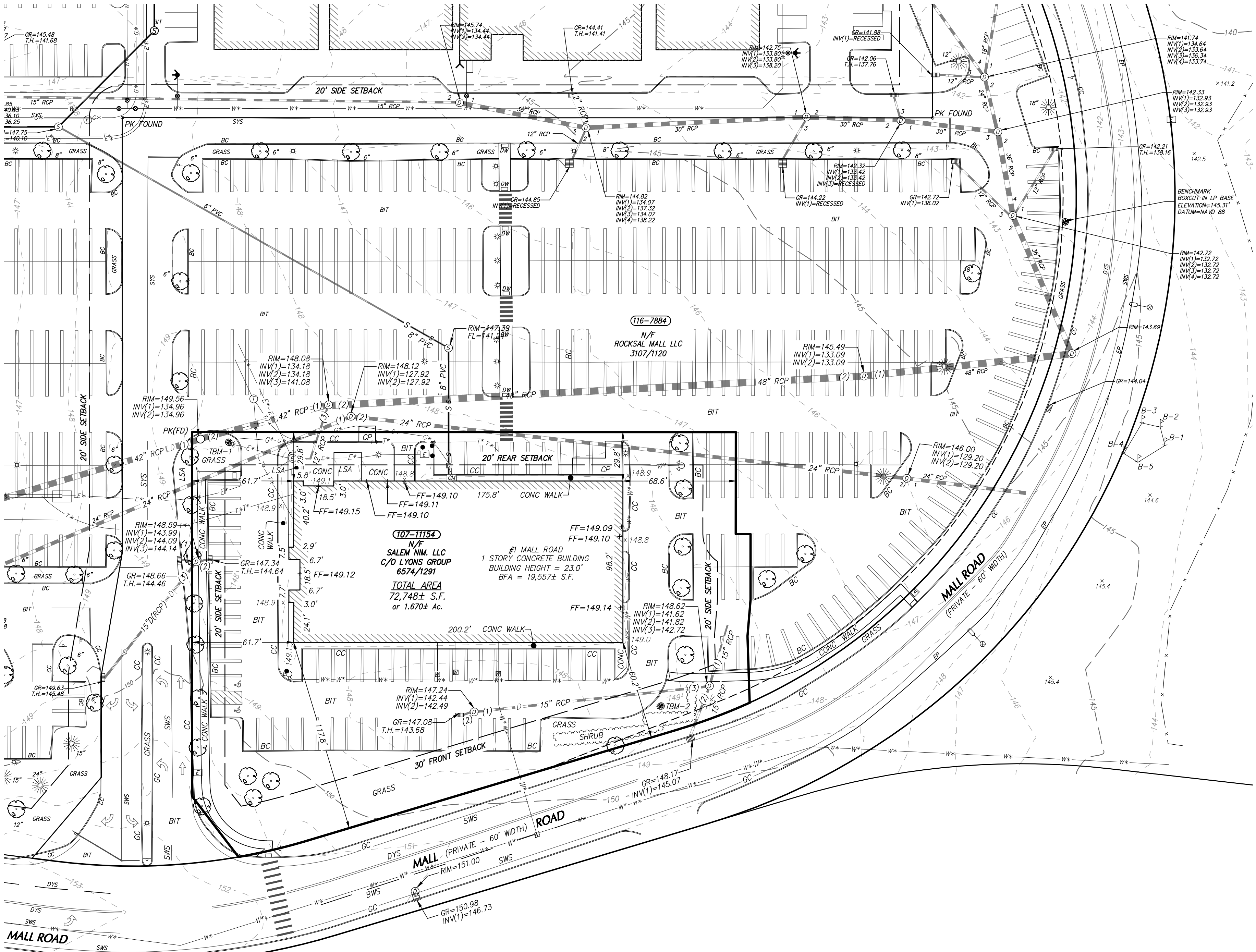
1. THIS SURVEY IS BASED UPON EXISTING PHYSICAL CONDITIONS FOUND AT THE SUBJECT SITE, DEED INFORMATION AND THE FOLLOWING REFERENCES:
- A. COMMITMENT FOR TITLE INSURANCE ISSUED BY STEWART TITLE GUARANTY COMPANY. COMMITMENT NUMBER: 2024-61, EFFECTIVE DATE JULY 2, 2024.
2. BENCH MARK INFORMATION:
- TEMPORARY BENCH MARKS SET:
- TBM-1: PK NAIL SET IN A LIGHT POLE BASE (AS SHOWN HEREON)
ELEVATION = 151.66
- TBM-2: PK NAIL SET IN A LIGHT POLE BASE (AS SHOWN HEREON)
ELEVATION = 151.53
3. ELEVATIONS SHOWN ARE REFERENCED TO NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) (GEOID 12B) AS DETERMINED BY GNSS
4. THIS PLAN IS THE RESULT OF AN ON-THE-GROUND SURVEY PERFORMED BY LANGAN ENGINEERING AND ENVIRONMENTAL SERVICES, LLC., ON MARCH 6, 2024.
4. BY GRAPHIC PLOTTING ONLY, THIS PROPERTY IS IN ZONE X, OF THE FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NO. 330142 0563 E, WHICH BEARS AN EFFECTIVE DATE OF MAY 17, 2005 AND IS NOT IN A SPECIAL FLOOD HAZARD AREA. AS SHOWN ON THE FEMA WEBSITE (HTTP://MSC.FEMA.GOV) BY FIRMETTE CREATED ON MARCH 19, 2024. WE HAVE LEARNED THIS COMMUNITY DOES CURRENTLY PARTICIPATE IN THE PROGRAM.
5. UNLESS SPECIFICALLY NOTED HEREON, STORM AND SANITARY SEWER INFORMATION (INCLUDING PIPE INVERT, PIPE MATERIAL, AND PIPE SIZE) WAS OBSERVED AND MEASURED AT FIELD LOCATED STRUCTURES (MANHOLES/CATCH BASINS, ETC.). CONDITIONS CAN VARY FROM THOSE ENCOUNTERED AT THE TIMES WHEN AND LOCATIONS WHERE DATA IS OBTAINED. DESPITE MEETING THE REQUIRED STANDARD OF CARE, THE SURVEYOR CANNOT, AND DOES NOT WARRANT THAT PIPE MATERIAL AND/OR PIPE SIZE THROUGHOUT THE PIPE RUN ARE THE SAME AS THOSE OBSERVED AT EACH STRUCTURE, OR THAT THE PIPE RUN IS STRAIGHT BETWEEN THE LOCATED STRUCTURES.
6. ADDITIONAL UTILITY (WATER, GAS, ELECTRIC ETC.) DATA IS SHOWN FROM FIELD LOCATED SURFACE MARKINGS (BY OTHERS), EXISTING STRUCTURES, AND/OR FROM EXISTING DRAWINGS.
7. UNLESS SPECIFICALLY NOTED HEREON, THE SURVEYOR HAS NOT EXCAVATED TO PHYSICALLY LOCATE THE UNDERGROUND UTILITIES. THE SURVEYOR MAKES NO GUARANTEES THAT THE SHOWN UNDERGROUND UTILITIES ARE EITHER IN SERVICE, ABANDONED OR SUITABLE FOR USE, NOR ARE IN THE EXACT LOCATION OR CONFIGURATION INDICATED HEREON.
8. ALL BUILDINGS AND STRUCTURES WERE LOCATED AND MEASURED AT GROUND LEVEL. THE SURVEYOR MAKES NO DETERMINATIONS OR GUARANTEES AS TO THE ABSENCE, EXISTENCE OR LOCATION OF UNDERGROUND STRUCTURES, FOUNDATIONS, FOOTINGS, PROJECTIONS, WALLS, TANKS, SEPTIC SYSTEMS, ETC. NO TEST PITS, EXCAVATIONS OR GROUND PENETRATING RADAR WERE PERFORMED AS PART OF THIS SURVEY.
9. PRIOR TO ANY DESIGN OR CONSTRUCTION, THE PROPER UTILITY AGENCIES MUST BE CONTACTED FOR VERIFICATION OF UTILITY TYPE AND FOR FIELD LOCATIONS.
10. THE PROPERTY HAS DIRECT ACCESS TO MALL ROAD, A PRIVATE WAY.

LEGEND

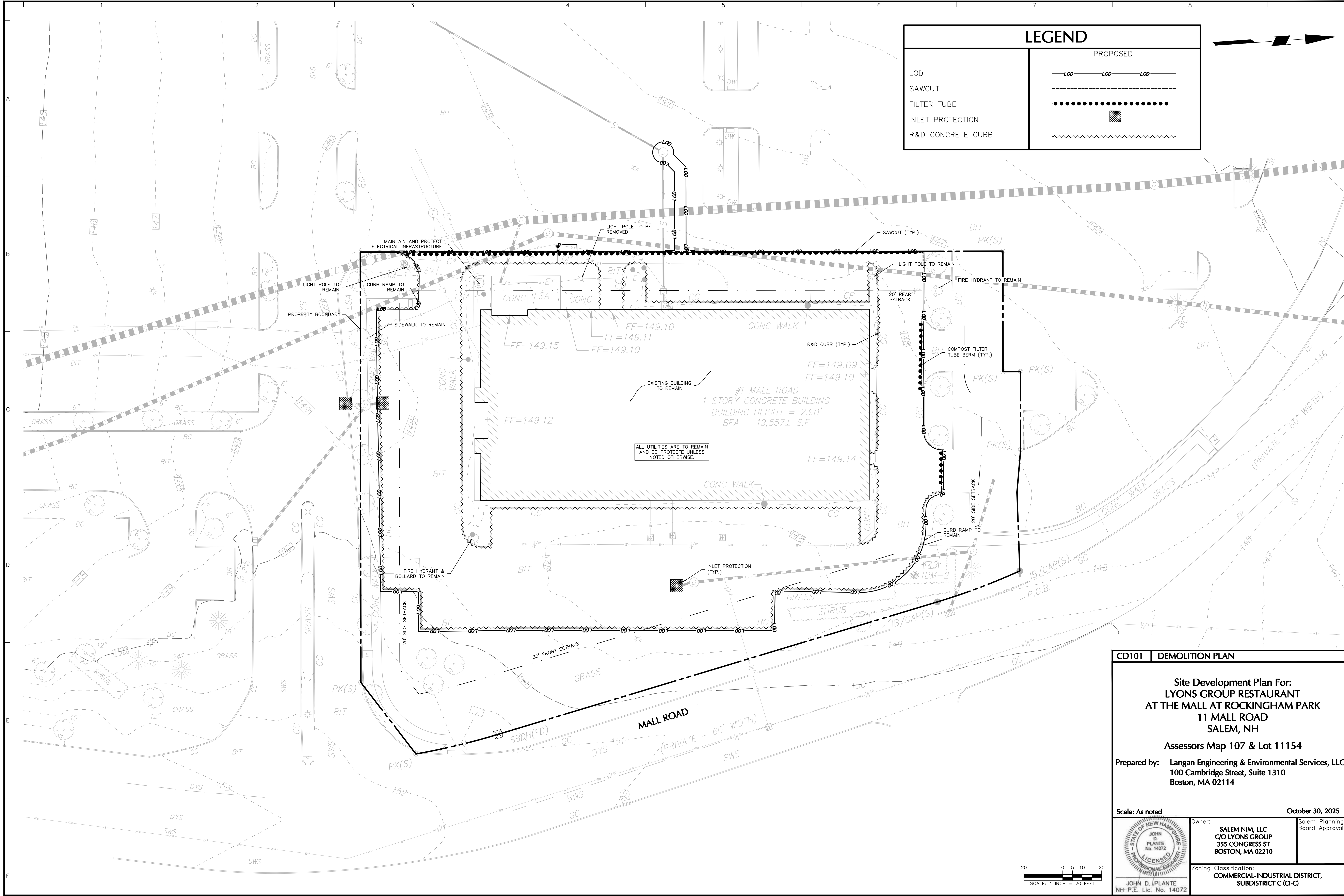
- PEDESTAL LIGHT
- MANHOLE (TYPE AS LABELED)
- GAS VALVE
- WATER VALVE
- UNKNOWN VALVE
- CATCH BASIN
- SPOT ELEVATION
- TREE
- SIGN
- ELECTRIC METER
- ELECTRIC BOX
- GAS METER
- TEMPORARY BENCHMARK
- ACRES
- BITUMINOUS CURB
- BITUMINOUS
- BROKEN WHITE STRIPE
- CONCRETE CURB
- CONCRETE
- CONCRETE PAD
- DRILL HOLE
- DETECTABLE WARNING
- DOUBLE YELLOW STRIPE
- FOUND
- FINISHED FLOOR ELEVATION
- FLOW LINE
- GRANITE CURB
- GRATE
- IRON BAR
- LANDSCAPED AREA
- PK NAIL
- POLYVINYL CHLORIDE
- REINFORCED CONCRETE PIPE
- SET
- STONE BOUND
- SQUARE FEET
- SINGLE WHITE STRIPE
- TOP OF HOOD
- TEMPORARY BENCH MARK

LEGEND

- PROPERTY LINE
- CONTOUR LINE
- GAS MARK OUT LINE
- WATER MARK OUT LINE
- ELECTRIC MARK OUT LINE
- TELEPHONE MARK OUT LINE
- SANITARY MARK OUT LINE
- DRAINAGE MARK OUT LINE
- REFERENCE UTILITY LINE (TYPE AS NOTED) - PLOTTED FROM EXISTING MAPPING



Date	Description	No.
REVISIONS		
		
10/23/2025		
IAN C. WISURI, PLS No. 988 DATE SIGNED		
LANGAN		
Langan Engineering and Environmental Services, LLC. 100 Cambridge Street, Suite 1310 Boston, MA 02114		
T: 617.824.9100 F: 617.824.9101 www.langan.com		
Project		
LYONS GROUP RESTAURANT AT THE MALL AT ROCKINGHAM		
1 MALL ROAD SALEM, 03079		
ROCKINGHAM COUNTY NEW HAMPSHIRE		
Prepared for		
SALEM NIM, LLC C/O LYONS GROUP		
335 CONGRESS STREET BOSTON, MA 02210		
Record Owner		
SALEM NIM, LLC C/O LYONS GROUP		
6574/1291		
Drawing Title		
EXISTING CONDITIONS		
Project No.		Drawing No.
151041901		VT901
Date		
OCTOBER 23, 2025		
Drawn By		
BAL		Sheet 1 of 1
Checked By		
ICW		



LEGEND

LOD

SAWCUT

FILTER TUBE

INLET PROTECTION

R&D CONCRETE CURB

PROPOSED

.....

■

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CD101 | DEMOLITION PLAN

Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

Scale: As noted

October 30, 2025

STATE OF NEW HAMPSHIRE  
JOHN D. PLANTE  
No. 14072  
PROFESSIONAL ENGINEER  
LICENSED

JOHN D. PLANTE  
NH P.E. Lic. No. 14072

Owner:  
SALEM NIM, LLC  
C/O LYONS GROUP  
355 CONGRESS ST  
BOSTON, MA 02210

Zoning Classification:  
COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (C1-C)

Salem Planning  
Board Approval

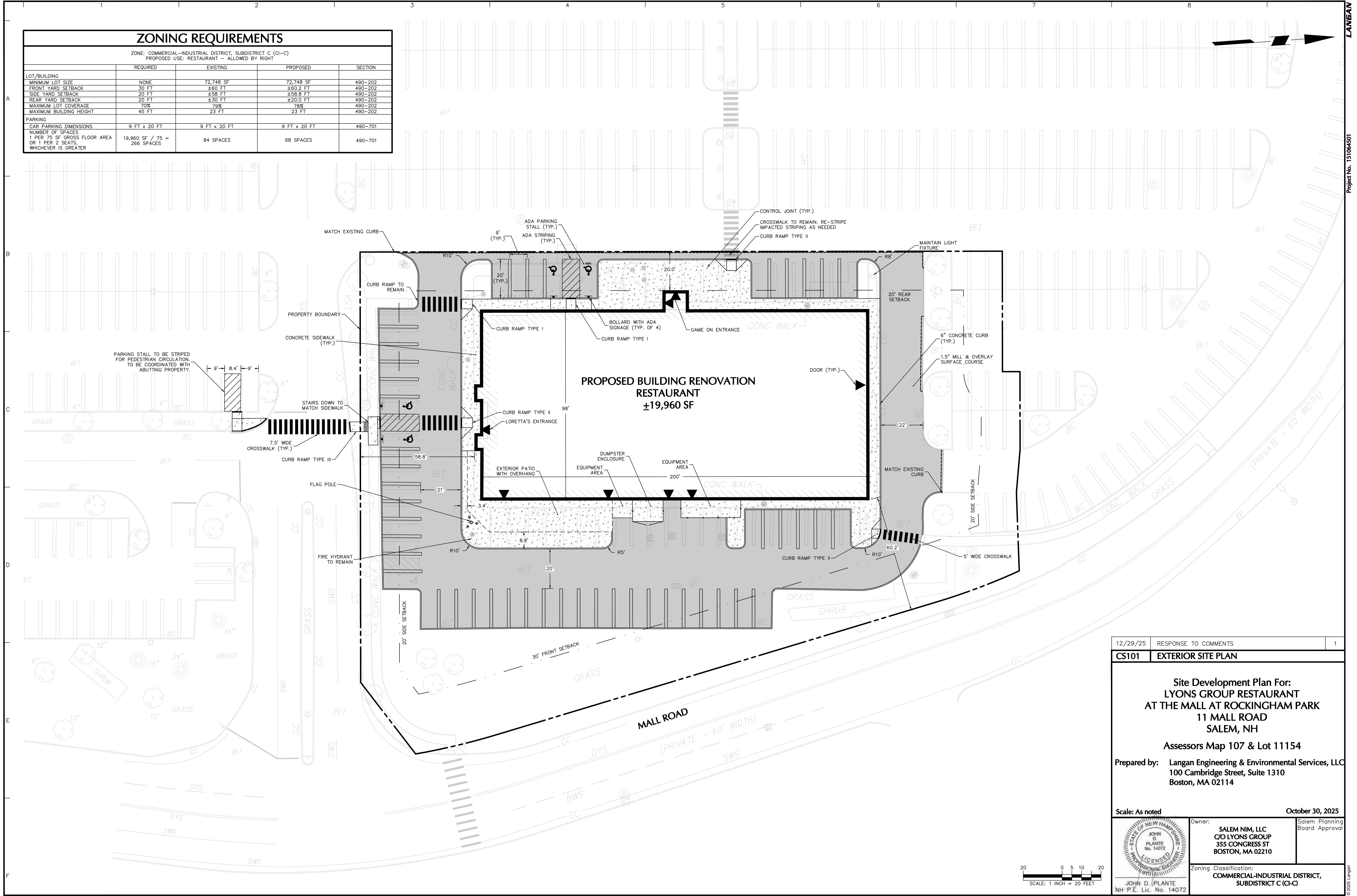
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ZONING REQUIREMENTS

ZONE: COMMERCIAL-INDUSTRIAL DISTRICT, SUBDISTRICT C (CI-C)  
PROPOSED USE: RESTAURANT - ALLOWED BY RIGHT

|                                                                           | REQUIRED                       | EXISTING     | PROPOSED     | SECTION |
|---------------------------------------------------------------------------|--------------------------------|--------------|--------------|---------|
| LOT/BUILDING                                                              |                                |              |              |         |
| MINIMUM LOT SIZE                                                          | NONE                           | 72,748 SF    | 72,748 SF    | 490-202 |
| FRONT YARD SETBACK                                                        | 30 FT                          | ±60 FT       | ±60.2 FT     | 490-202 |
| SIDE YARD SETBACK                                                         | 20 FT                          | ±58 FT       | ±58.8 FT     | 490-202 |
| REAR YARD SETBACK                                                         | 20 FT                          | ±30 FT       | ±20.0 FT     | 490-202 |
| MAXIMUM LOT COVERAGE                                                      | 70%                            | 79%          | 78%          | 490-202 |
| MAXIMUM BUILDING HEIGHT                                                   | 45 FT                          | 23 FT        | 23 FT        | 490-202 |
| PARKING                                                                   |                                |              |              |         |
| CAR PARKING DIMENSIONS                                                    | 9 FT x 20 FT                   | 9 FT x 20 FT | 9 FT x 20 FT | 490-701 |
| NUMBER OF SPACES                                                          |                                |              |              |         |
| 1 PER 75 SF GROSS FLOOR AREA<br>OR 1 PER 2 SEATS,<br>WHICHEVER IS GREATER | 19,960 SF / 75 =<br>266 SPACES | 84 SPACES    | 68 SPACES    | 490-701 |



12/29/25 RESPONSE TO COMMENTS 1

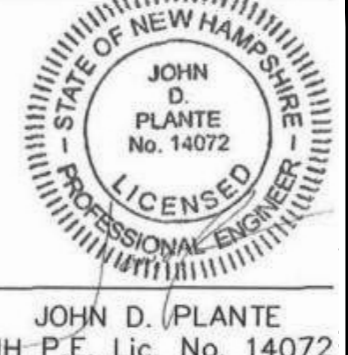
CS101 EXTERIOR SITE PLAN

Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

Scale: As noted October 30, 2025



JOHN D. PLANTE  
NH P.E. Lic. No. 14072

Owner:

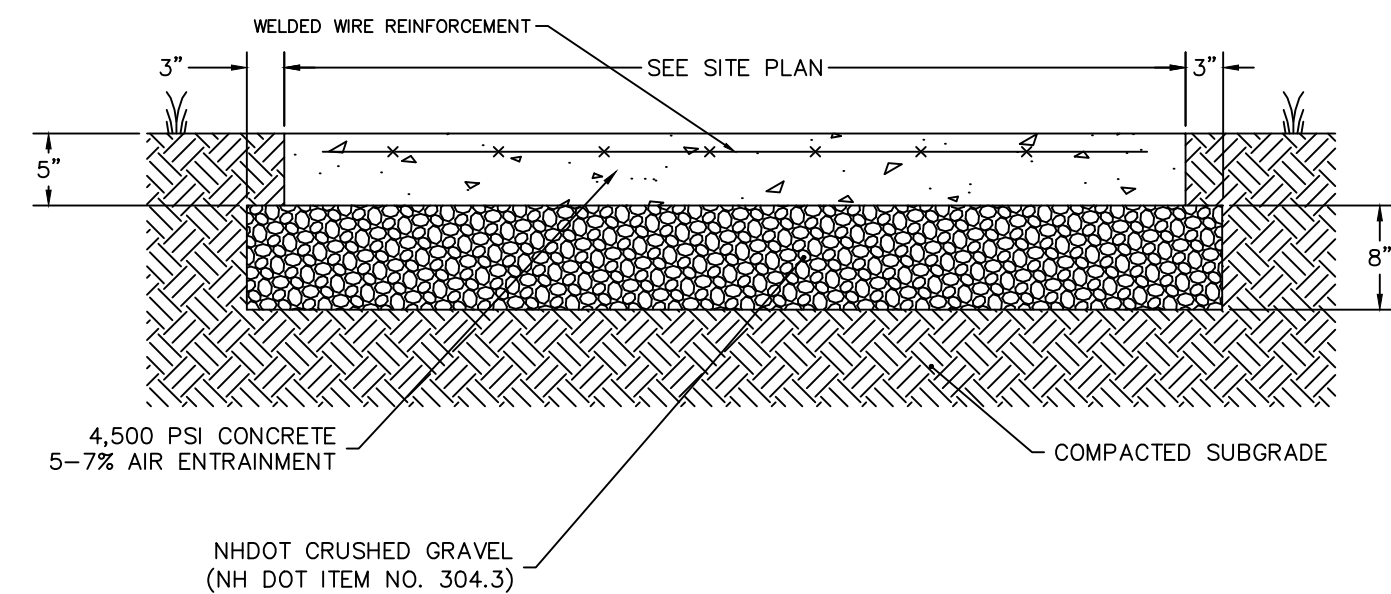
SALEM NIM, LLC  
C/O LYONS GROUP  
355 CONGRESS ST  
BOSTON, MA 02210

Salem Planning Board Approval

Zoning Classification:

COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (CI-C)

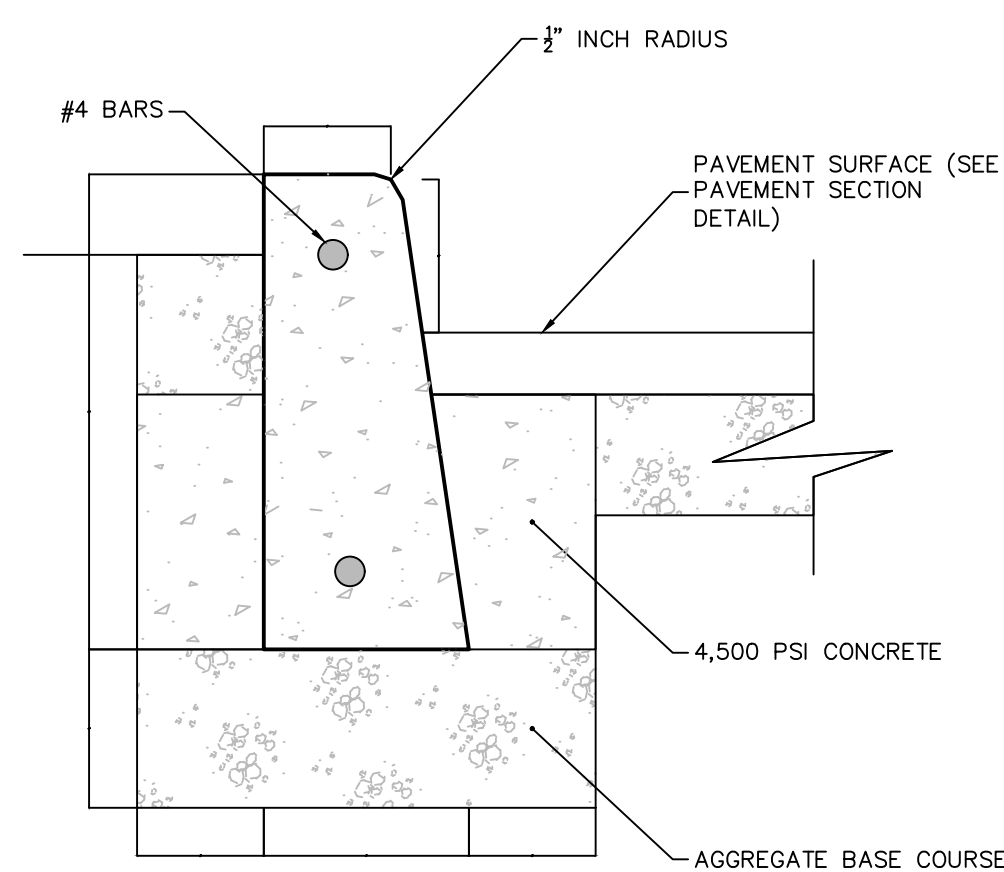
20 0 5 10 20  
SCALE: 1 INCH = 20 FEET



- NOTES:
1. BITUMINOUS CONCRETE SHALL CONFORM TO NHDOT SPECIFICATION
  2. ACTUAL MILL DEPTH TO BE "GRADE-CONTROLLED" AND SHOULD BE ADJUSTED TO MATCH GRADES AS SHOWN. MILL DEPTH MAY BE VARIABLE AND A WEDGE COARSE MAY BE REQUIRED TOP COURSE TO BE A CONSISTENT 2 INCHES
  3. AFTER MILLING, CONTRACTOR TO INSPECT REMAINING PAVEMENT AND REPAIR FULL DEPTH AS REQUIRED

## ASPHALT PAVEMENT MILL AND OVERLAY

N.T.S.

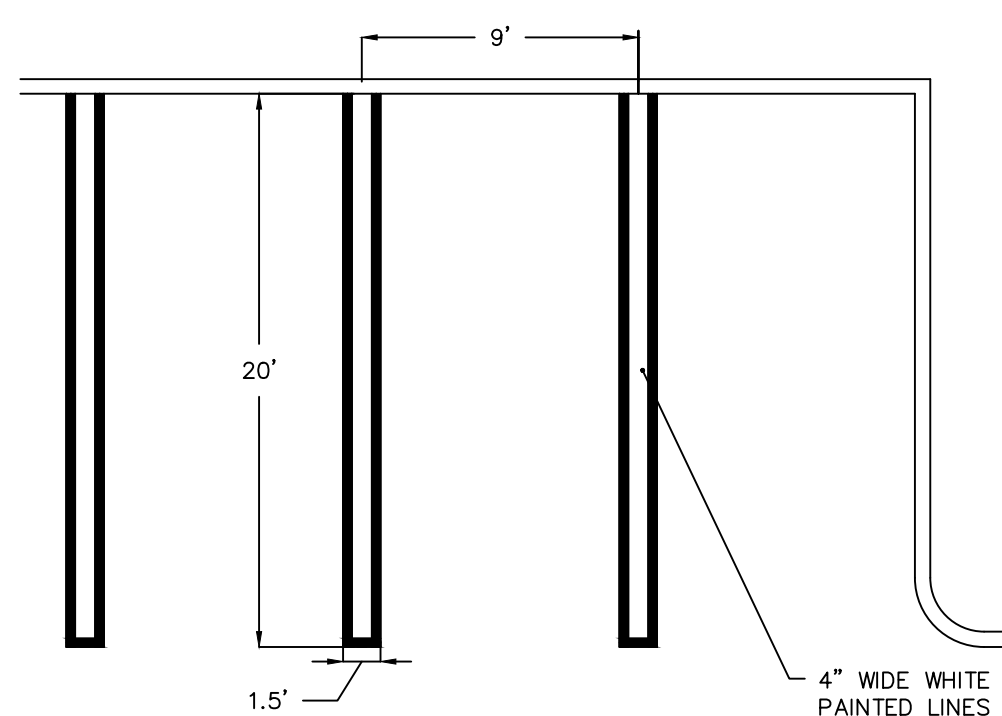


- NOTES:  
1. CONCRETE TO TEST 4,500 PSI MINIMUM ON 28 DAY TEST

- NOTES:  
1. CONCRETE TO TEST 4,500 PSI MINIMUM ON 28 DAY TEST.

## CONCRETE CURB

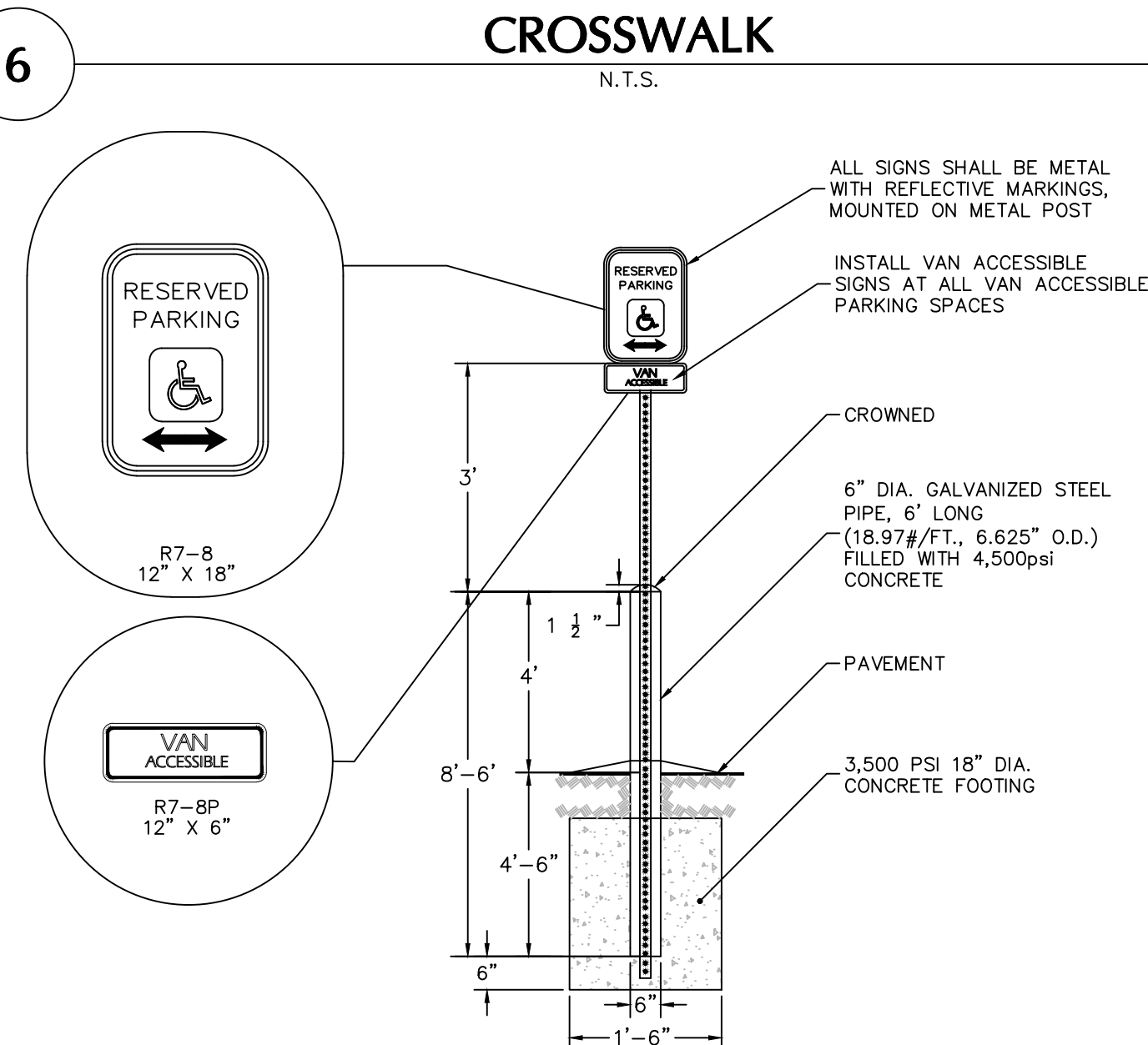
N.T.S.



- NOTES:
1. ALL PAINT SHALL BE FAST DRYING WATERBORNE TRAFFIC PAINT, MEETING THE REQUIREMENTS OF TT-P-1952F TYPE II, PAINT SHALL BE APPLIED AS SPECIFIED BY THE MANUFACTURER.
  2. SYMBOLS & PARKING STALLS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICAN DISABILITIES ACT (ADA) AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
  3. APPLY TWO COATS OF TRAFFIC PAINT. ALLOW FOR A MINIMUM CURE TIME PER MANUFACTURER.

## PARKING STALL STRIPING

N.T.S.

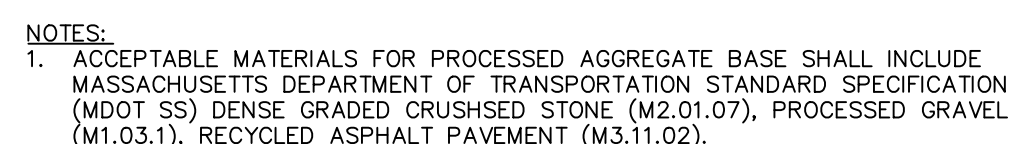


- ACCESSIBLE PARKING SIGN SHALL CONFORM WITH CURRENT STATE & LOCAL CODES AND REGULATIONS

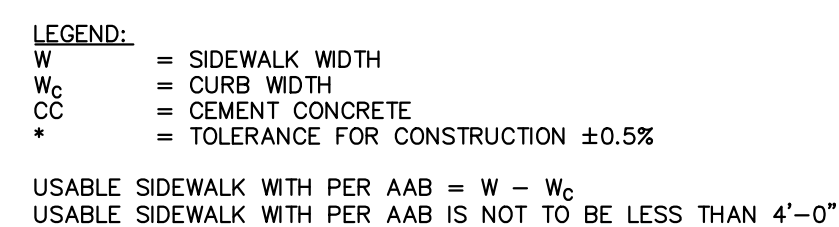
- NOTES:
1. ALL POSTS SHALL BE OF ADEQUATE LENGTH TO MEET THE REQUIREMENTS FOR ERECTION AS STATED IN THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS".
  2. ALL POSTS SHALL BE EMBEDDED 4'-6" MINIMUM BELOW GRADE.
  3. ALL STEEL POSTS AND BRACKETS SHALL BE CUT, BENT, AND HOLES PUNCHED AND DRILLED BEFORE GALVANIZING. GALVANIZING SHALL BE IN CONFORMANCE WITH CURRENT A.S.T.M. SPECIFICATION A123-78 (OR LATEST REVISED).
  4. POSTS SHALL BE BREAKAWAY TYPE II TWO-PIECE U-POST OR OTHER BREAKAWAY STRUCTURAL SUPPORT HARDWARE AS NOTED IN THE LATEST EDITION OF AASHTO'S SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS.
  5. SIGN PANEL SIZES SHALL DETERMINE POST TYPE AND NUMBERS AS SHOWN ON THIS DETAIL AND DIRECTIONAL SIGN SHEET.
  6. BOLTS SHALL NOT PROTRUDE MORE THAN 3/4" BEYOND THE NUT WHEN TIGHT BUT SHALL ENGAGE ALL THREADS IN THE NUT.

N.T.S

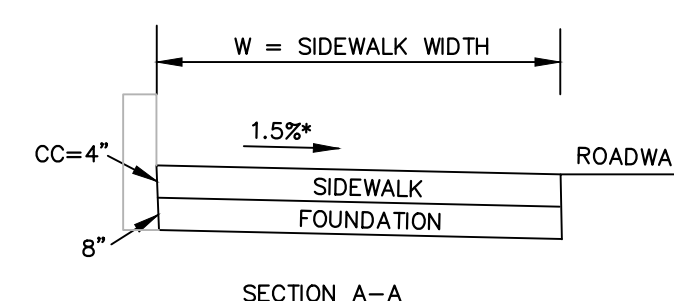
|                                                                                                                                                                                                                                                                                                                                                                                                                                                              |                                                                                                                                                                                                   |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| CS501 SITE DETAILS I                                                                                                                                                                                                                                                                                                                                                                                                                                         |                                                                                                                                                                                                   |
| <p align="center"><b>Site Development Plan For:</b><br/> <b>LYONS GROUP RESTAURANT</b><br/> <b>AT THE MALL AT ROCKINGHAM PARK</b><br/> <b>11 MALL ROAD</b><br/> <b>SALEM, NH</b></p> <p align="center"><b>Assessors Map 107 &amp; Lot 11154</b></p> <p><b>Prepared by:</b> Langan Engineering &amp; Environmental Services, LLC<br/>         100 Cambridge Street, Suite 1310<br/>         Boston, MA 02114</p> <p align="right"><b>October 30, 2025</b></p> |                                                                                                                                                                                                   |
|  <p>JOHN D. (PLANTE)<br/>         NH P.E. Lic. No. 14072</p>                                                                                                                                                                                                                                                                                                            | <p><b>Owner:</b></p> <p align="center"><b>SALEM NIM, LLC</b><br/> <b>C/O LYONS GROUP</b><br/> <b>355 CONGRESS ST</b><br/> <b>BOSTON, MA 02210</b></p> <p><b>Salem Planning Board Approval</b></p> |
| <p><b>Zoning Classification:</b></p> <p align="center"><b>COMMERCIAL-INDUSTRIAL DISTRICT,</b><br/> <b>SUBDISTRICT C (CI-C)</b></p>                                                                                                                                                                                                                                                                                                                           |                                                                                                                                                                                                   |



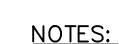
NTS



NOTE:  
ROADWAY, GUTTER, AND FIRST 6" OF SIDEWALK TO BE ADJUSTED FOR FIELD  
CONDITIONS



NTS



1. ALL POSTS SHALL BE OF ADEQUATE LENGTH TO MEET THE REQUIREMENTS FOR ERECTION AS STATED IN THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS".
2. ALL POSTS SHALL BE EMBEDDED 4"-6" MINIMUM BELOW GRADE.
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5. SIGN PANEL SIZES SHALL DETERMINE POST TYPE AND NUMBERS AS SHOWN ON THIS DETAIL AND DIRECTIONAL SIGN SHEET.
6. BOLTS SHALL NOT PROTRUDE MORE THAN 3/4" BEYOND THE NUT WHEN TIGHT BUT SHALL ENGAGE ALL THREADS IN THE NUT.
7. ALL TRAFFIC AND PEDESTRIAN SIGNAGE AND LOCATION SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND ALL CURRENT AMENDMENTS.

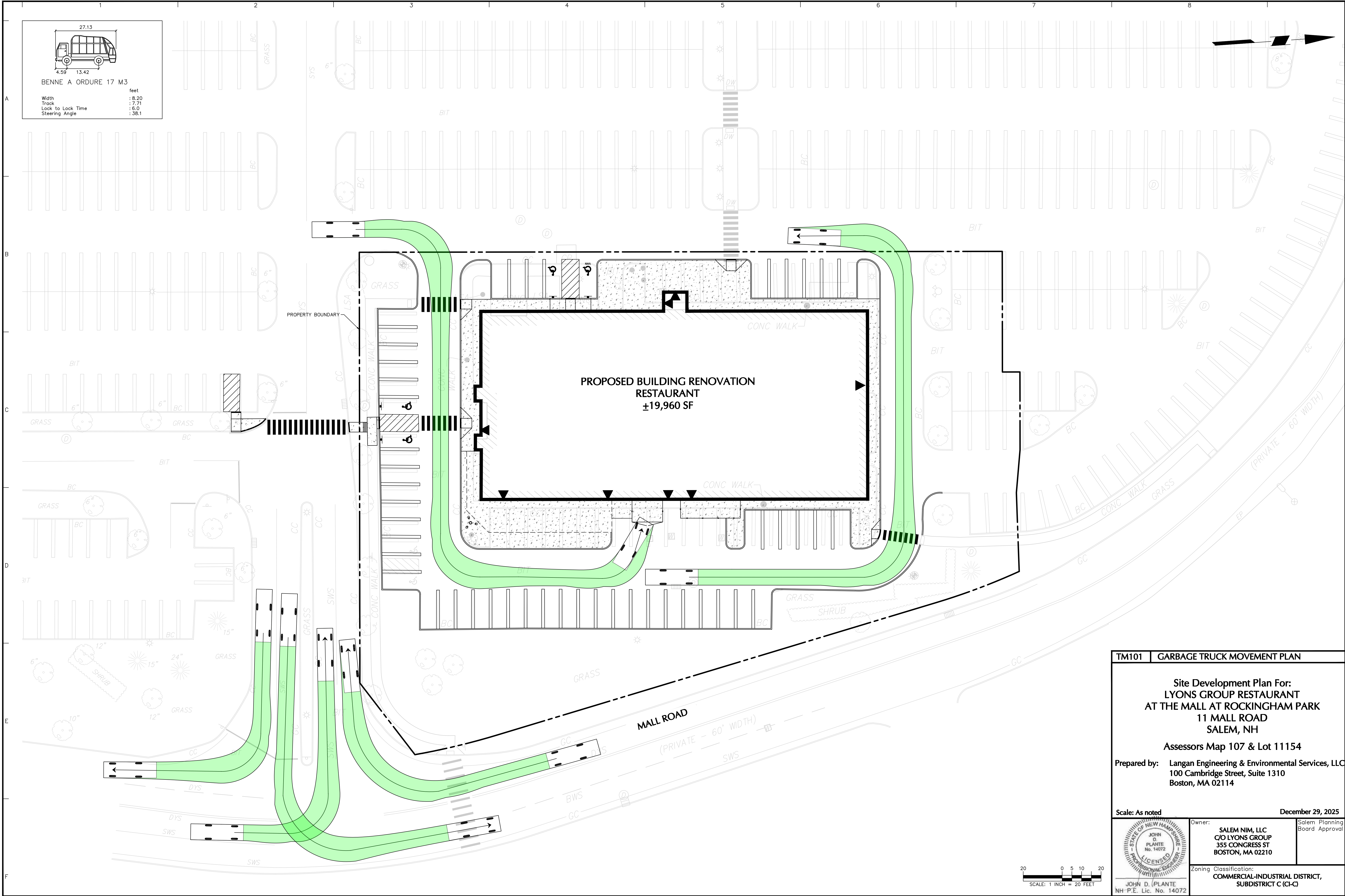
N.T.S.



N.T.S.

Date: 12/29/2025 Time: 12:51 User: jemotsko Style Table: Langan.stb Layout: CS502 Document Code: 151064501-0501-CS501-0102

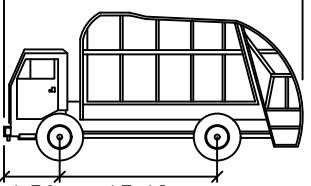




27.13

4.59

13.42



BENNE A ORDURE 17 M3

feet

Width : 8.20

Track : 7.71

Lock to Lock Time : 6.0

Steering Angle : 38.1

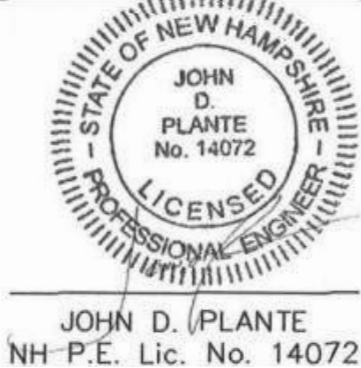
TM101 | GARBAGE TRUCK MOVEMENT PLAN

Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

Scale: As noted December 29, 2025



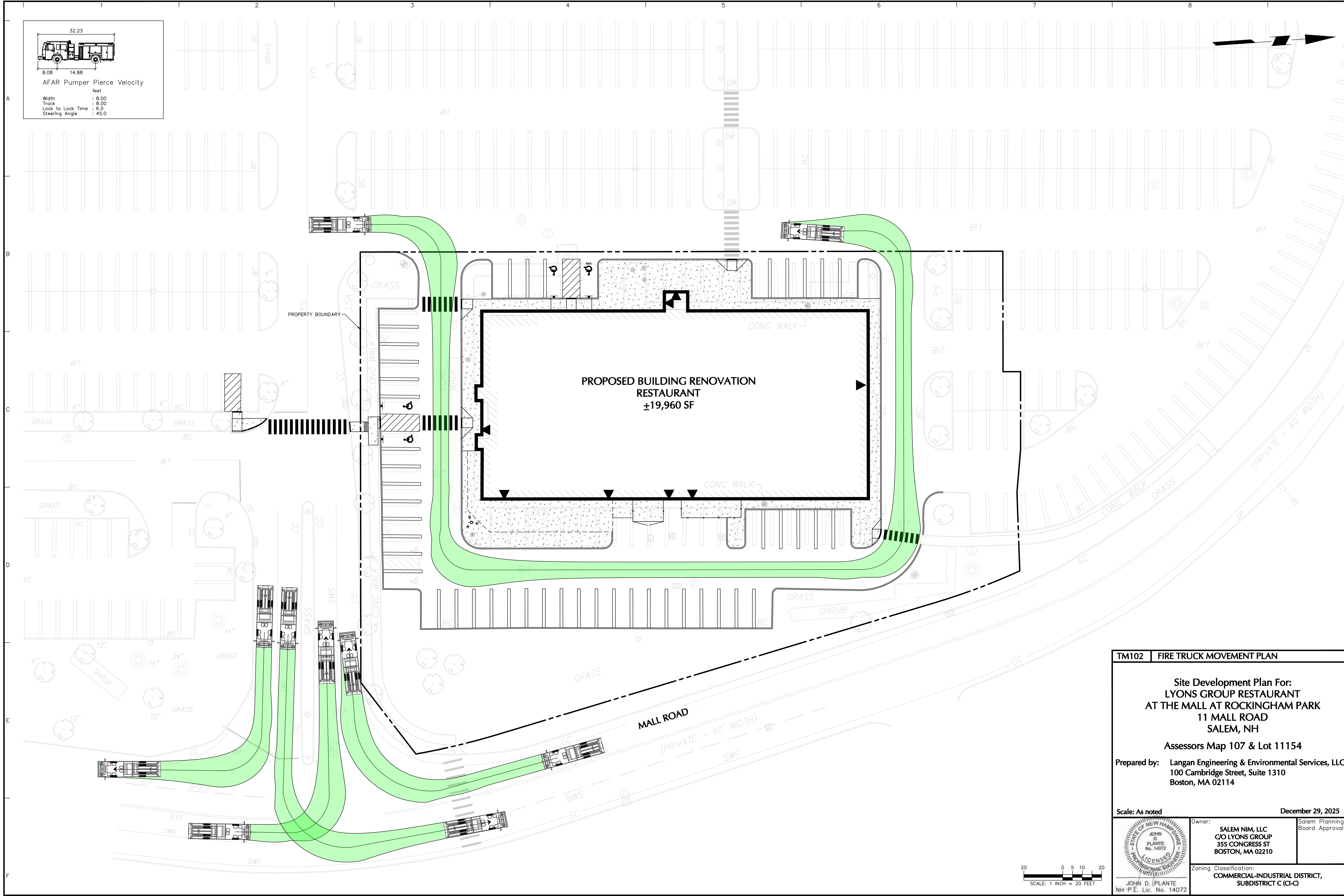
Owner:  
SALEM NIM, LLC  
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355 CONGRESS ST  
BOSTON, MA 02210

Salem Planning  
Board Approval

Zoning Classification:  
COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (C-C)

20 0 5 10 20  
SCALE: 1 INCH = 20 FEET





32.23

8.08

14.88

AFAR Pumper Pierce Velocity

feet

Width

: 8.00

Track

: 8.00

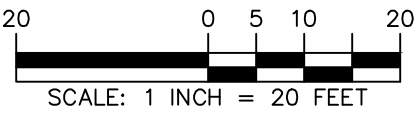
Lock to Lock Time

: 6.0

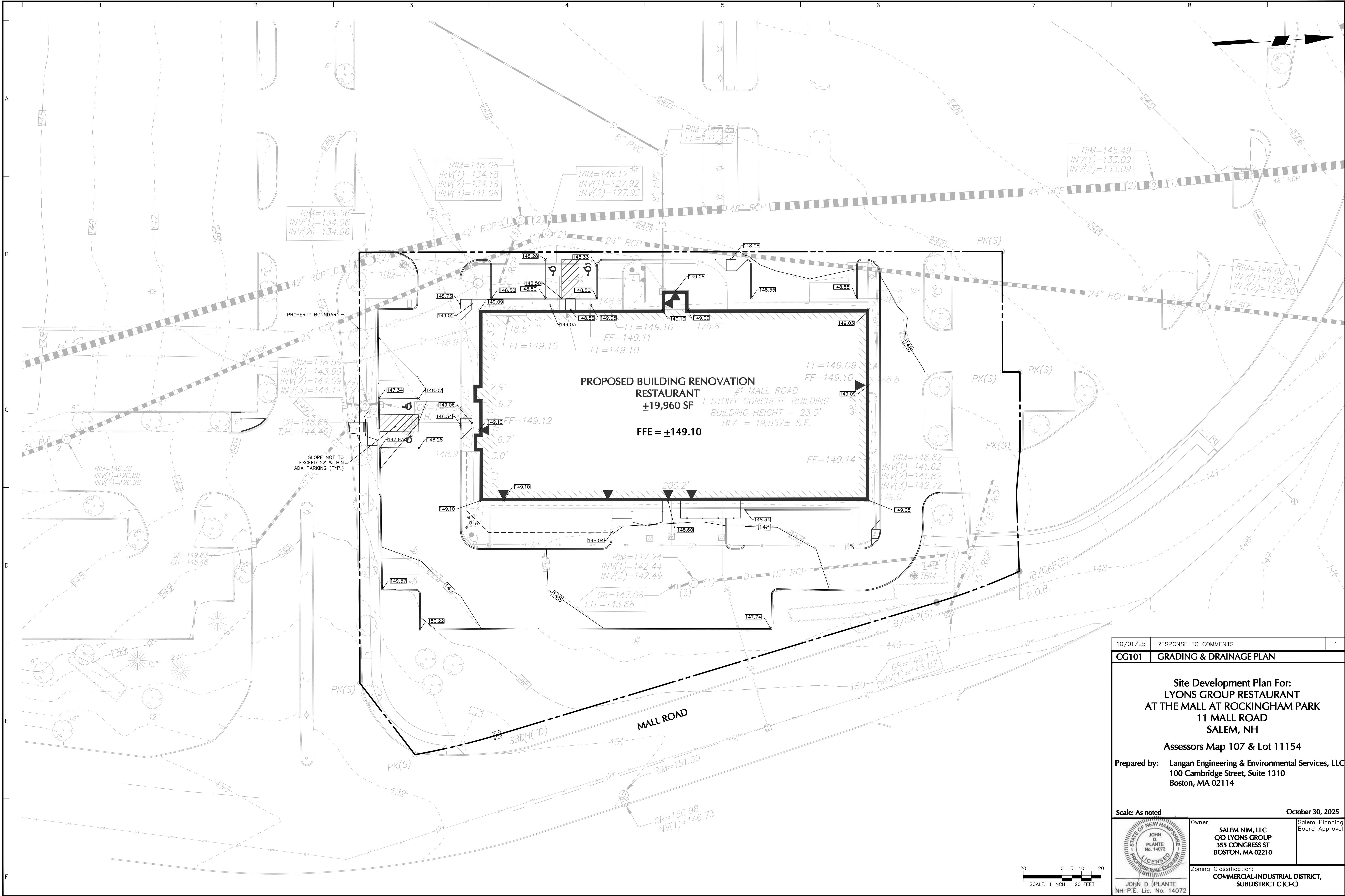
Steering Angle

: 45.0

|                                                                                                                                                                               |                                                                                                          |                                              |  |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------|----------------------------------------------|--|
| TM102                                                                                                                                                                         |                                                                                                          | FIRE TRUCK MOVEMENT PLAN                     |  |
| <div>Site Development Plan For:<br/>LYONS GROUP RESTAURANT<br/>AT THE MALL AT ROCKINGHAM PARK<br/>11 MALL ROAD<br/>SALEM, NH<br/><br/>Assessors Map 107 &amp; Lot 11154</div> |                                                                                                          |                                              |  |
| <div>Prepared by: Langan Engineering &amp; Environmental Services, LLC<br/>100 Cambridge Street, Suite 1310<br/>Boston, MA 02114</div>                                        |                                                                                                          |                                              |  |
| Scale: As noted                                                                                                                                                               |                                                                                                          | December 29, 2025                            |  |
| <div></div>                                                                                                                                                                   | <div>Owner:<br/><b>SALEM NIM, LLC</b><br/>C/O LYONS GROUP<br/>355 CONGRESS ST<br/>BOSTON, MA 02210</div> | <div>Salem Planning<br/>Board Approval</div> |  |
| <div>Zoning Classification:<br/><b>COMMERCIAL-INDUSTRIAL DISTRICT,<br/>SUBDISTRICT C (C-C)</b></div>                                                                          |                                                                                                          |                                              |  |







|          |                      |   |
|----------|----------------------|---|
| 10/01/25 | RESPONSE TO COMMENTS | 1 |
|----------|----------------------|---|

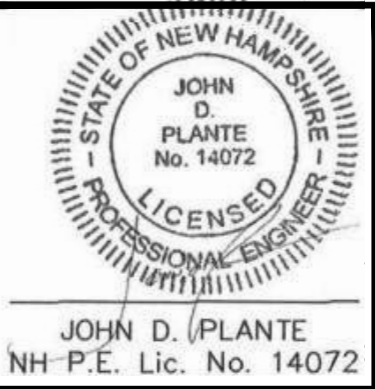
|       |                         |
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| CG101 | GRADING & DRAINAGE PLAN |
|-------|-------------------------|

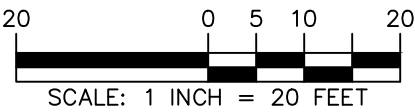
Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH

Assessors Map 107 & Lot 11154

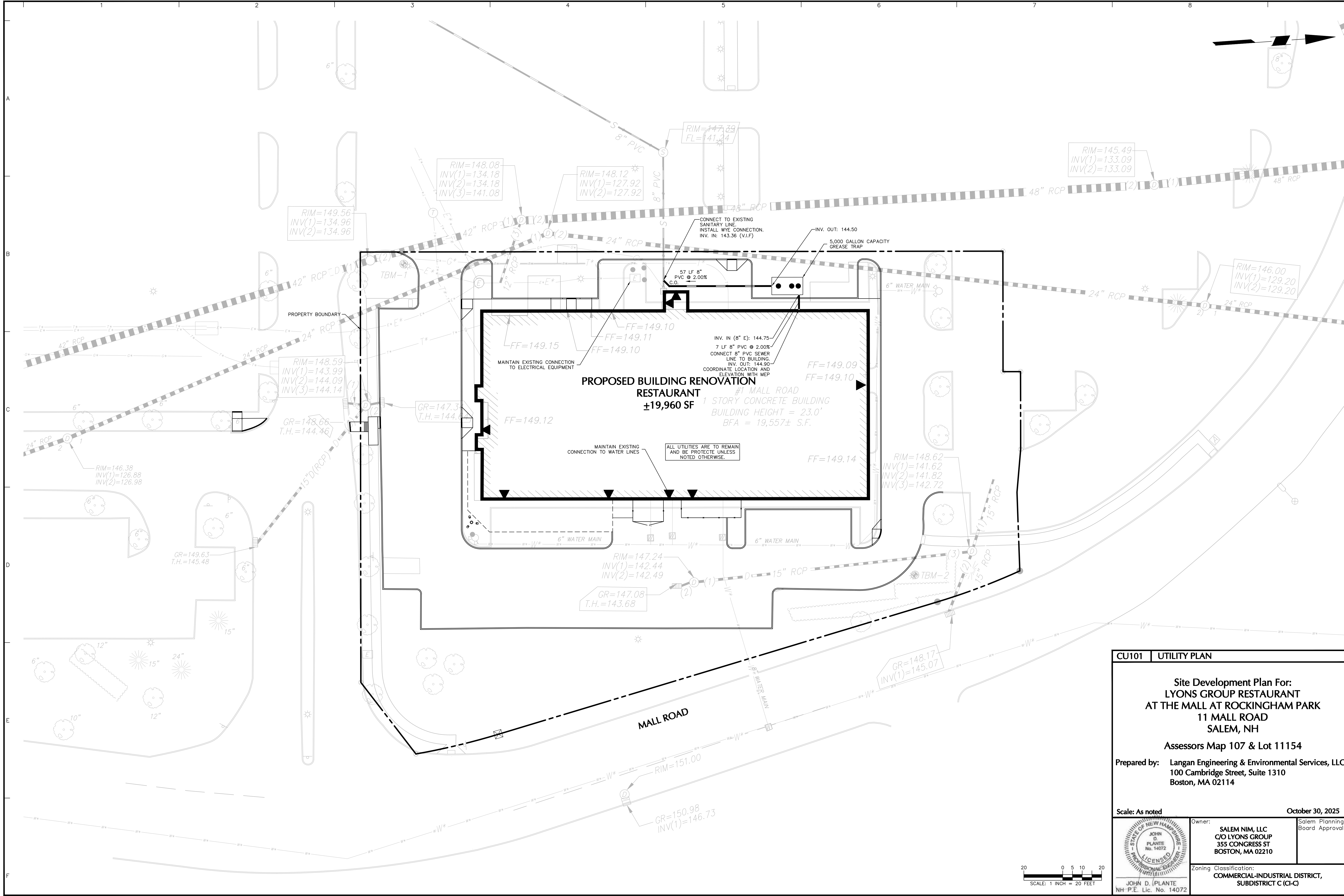
Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

Scale: As noted October 30, 2025

|                                                                                       |                                                                                    |                                  |
|---------------------------------------------------------------------------------------|------------------------------------------------------------------------------------|----------------------------------|
|  | Owner:<br>SALEM NIM, LLC<br>C/O LYONS GROUP<br>355 CONGRESS ST<br>BOSTON, MA 02210 | Salem Planning<br>Board Approval |
| Zoning Classification:<br>COMMERCIAL-INDUSTRIAL DISTRICT,<br>SUBDISTRICT C (C-I-C)    |                                                                                    |                                  |







CU101 | UTILITY PLAN

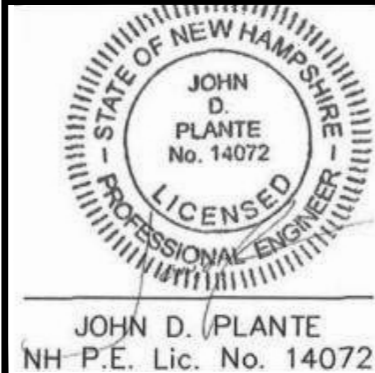
Site Development Plan For:  
**LYONS GROUP RESTAURANT**  
**AT THE MALL AT ROCKINGHAM PARK**  
**11 MALL ROAD**  
**SALEM, NH**

Assessors Map 107 & Lot 11154

Prepared by: **Langan Engineering & Environmental Services, LLC**  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

Scale: As noted

October 30, 2025

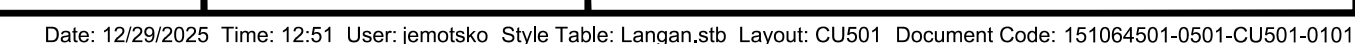


Owner: **SALEM NIM, LLC**  
**C/O LYONS GROUP**  
**355 CONGRESS ST**  
**BOSTON, MA 02210**

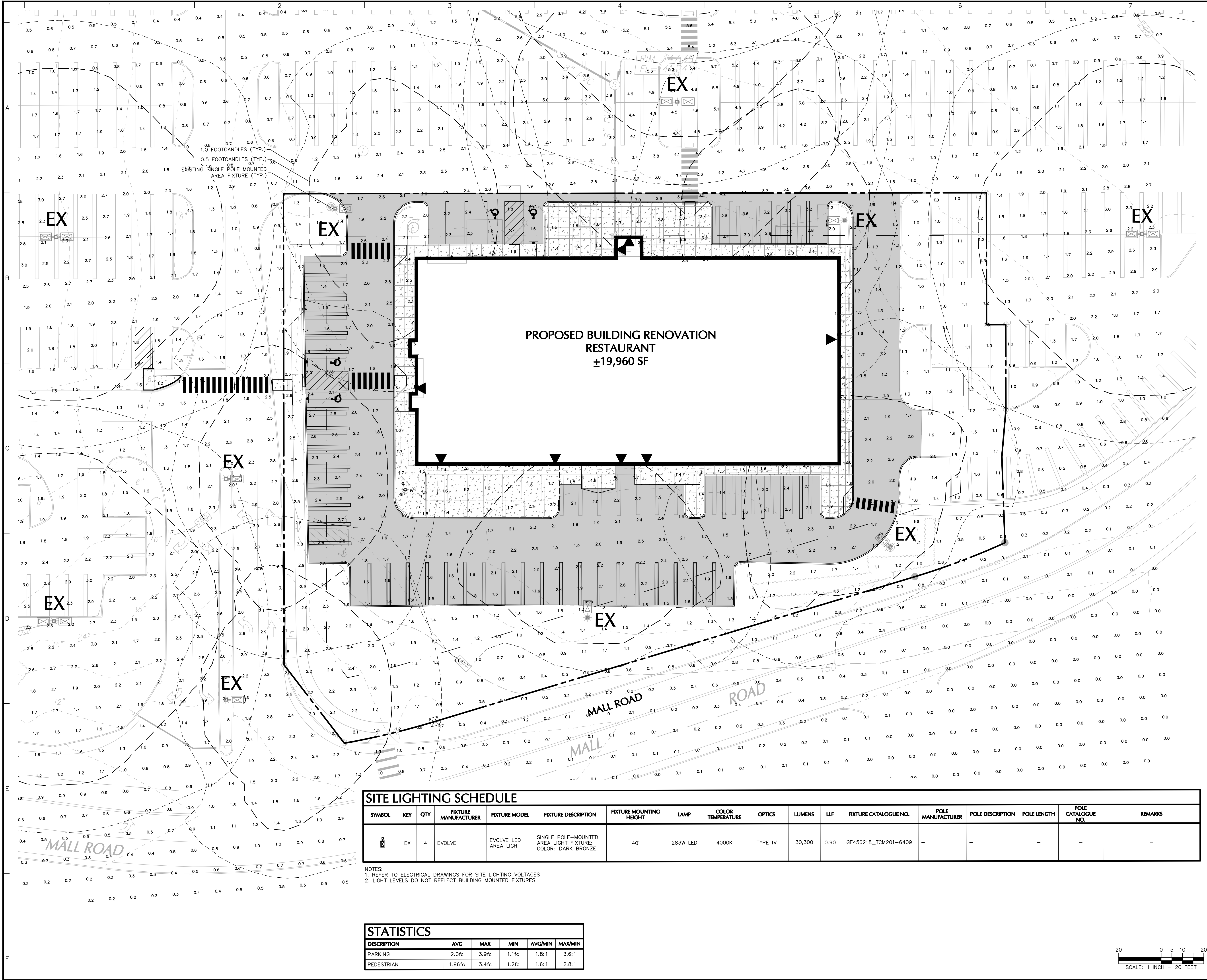
Salem Planning  
Board Approval

Zoning Classification:  
**COMMERCIAL-INDUSTRIAL DISTRICT,**  
**SUBDISTRICT C (C-C)**

20 0 5 10 20  
SCALE: 1 INCH = 20 FEET







| SITE LIGHTING SCHEDULE |     |     |                      |                       |                                                            |                         |          |                   |         |        |      |                       |                   |                  |
|------------------------|-----|-----|----------------------|-----------------------|------------------------------------------------------------|-------------------------|----------|-------------------|---------|--------|------|-----------------------|-------------------|------------------|
| SYMBOL                 | KEY | QTY | FIXTURE MANUFACTURER | FIXTURE MODEL         | FIXTURE DESCRIPTION                                        | FIXTURE MOUNTING HEIGHT | LAMP     | COLOR TEMPERATURE | OPTICS  | LUMENS | LLF  | FIXTURE CATALOGUE NO. | POLE MANUFACTURER | POLE DESCRIPTION |
|                        | EX  | 4   | EVOLVE               | EVOLVE LED AREA LIGHT | SINGLE POLE-MOUNTED AREA LIGHT FIXTURE, COLOR: DARK BRONZE | 40'                     | 283W LED | 4000K             | TYPE IV | 30,300 | 0.90 | GE456218_TCM201-6409  | -                 | -                |

NOTES:  
1. REFER TO ELECTRICAL DRAWINGS FOR SITE LIGHTING VOLTAGES  
2. LIGHT LEVELS DO NOT REFLECT BUILDING MOUNTED FIXTURES

| STATISTICS  |        |       |       |         |         |
|-------------|--------|-------|-------|---------|---------|
| DESCRIPTION | AVG    | MAX   | MIN   | AVG/MIN | MAX/MIN |
| PARKING     | 2.0fc  | 3.9fc | 1.1fc | 1.8:1   | 3.6:1   |
| PEDESTRIAN  | 1.96fc | 3.4fc | 1.2fc | 1.6:1   | 2.8:1   |

LL101 SITE LIGHTING PLAN

Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
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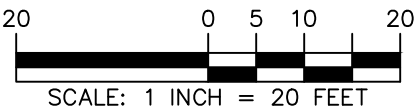
October 30, 2025



Owner: SALEM NIM, LLC  
C/O LYONS GROUP  
355 CONGRESS ST  
BOSTON, MA 02210

Salem Planning Board Approval

Zoning Classification:  
COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (C-C)





SITE LIGHTING NOTES:

GENERAL

1. POINT-BY-POINT CALCULATIONS PROVIDED WITHIN HAVE BEEN PREPARED IN ACCORDANCE TO IESNA STANDARDS AND IN CONSIDERATION OF THE VARIABLES WITHIN THESE NOTES AND SITE LIGHTING SCHEDULE. THE VALUES SHOWN ON THE PLANS ARE NOT AN INDICATION OF THE INITIAL LIGHT INTENSITIES OF THE LAMPS. THESE VALUES ARE AN APPROXIMATION OF THE MAINTAINED INTENSITIES DELIVERED TO THE GROUND PLANE USING INDUSTRY STANDARD LIGHT LOSS FACTORS (LLF) WHICH COVER LAMP DEGRADATION AND NATURAL BUILDUP / DIRT DEGRADATION ON THE FIXTURE LENS. THE LIGHTING PLAN IS DESIGNED WITH AN INDUSTRY STANDARD LLF IN ACCORDANCE WITH GUIDANCE AS PROVIDED BY IESNA. MINOR VARIATIONS IN TOPOGRAPHY, PHYSICAL OBSTRUCTIONS, AMBIENT OR ADJACENT LIGHT SOURCES AND/OR OTHER POTENTIAL IMPACTS HAVE NOT BEEN INCLUDED IN THESE CALCULATIONS. THEREFORE, AS-BUILT LIGHT INTENSITIES MAY VARY, IN EITHER DIRECTION, FROM WHAT IS EXPLICITLY PORTRAYED WITHIN THESE DRAWINGS.NO GUARANTEE OF LIGHT LEVELS IS EXPRESSED OR IMPLIED BY THE POINT BY POINT CALCULATIONS SHOWN ON THESE PLANS.
2. LIGHT LEVEL POINT SPACING IS 10 FT. LEFT TO RIGHT AND 10 FT. TOP TO BOTTOM. POINT BY POINT CALCULATIONS ARE BASED ON THE LIGHT LOSS FACTOR AS STATED IN THE LIGHTING SCHEDULE.

COMPLIANCE

3. ALL SITE LIGHTING RELATED WORK AND MATERIALS SHALL COMPLY WITH CITY, COUNTY, AND OTHER APPLICABLE GOVERNING AUTHORITY REQUIREMENTS.
4. LIGHTING LAYOUT COMPLIES WITH THE ILLUMINATING ENGINEERING SOCIETY OF NORTH AMERICA (IESNA) SAFETY STANDARDS FOR LIGHT LEVELS.

COORDINATION

5. CONTRACTOR TO COORDINATE POWER SOURCE WITH LIGHT FIXTURES TO ENSURE ALL SITE LIGHTING IS OPERATING EFFECTIVELY, EFFICIENTLY AND SAFELY.
6. REFER TO ELECTRIFICATION PLAN FOR PROVIDING ADEQUATE POWER FOR SITE LIGHTING.
7. CONTRACTOR TO COORDINATE LOCATION OF EASEMENTS, UNDERGROUND UTILITIES AND DRAINAGE BEFORE DRILLING POLE BASES.
8. INSTALLATION OF ALL LIGHTING FIXTURES, POLES, FOOTINGS, AND FEEDER CABLE TO BE COORDINATED WITH ALL SITE WORK TRADES TO AVOID CONFLICT WITH FINISHED AND PROPOSED WORK.
9. CONTRACTOR TO COORDINATE INSTALLATION OF UNDERGROUND FEEDER CABLE FOR EXTERIOR LIGHTING WITH EXISTING UTILITIES, SITE DRAINAGE SYSTEMS, AND PAVING. CONTRACTOR SHALL PROMPTLY NOTIFY THE OWNER'S REPRESENTATIVE SHOULD ANY UTILITIES, NOT SHOWN ON THE PLANS, BE FOUND DURING EXCAVATIONS.

POLES AND FOOTINGS

10. PROVIDE A CONCRETE BASE FOR EACH LIGHT POLE AT THE LOCATIONS INDICATED ON THE CONSTRUCTION DRAWINGS AND/OR IN ACCORDANCE WITH PROJECT PLANS AND SPECIFICATIONS RELATING DIRECTLY TO CAST-IN-PLACE CONCRETE. THE USE OF ALTERNATE LIGHTING FOUNDATIONS, SUCH AS PRECAST, MAY CHANGE THE SIZING AND REINFORCEMENT REQUIREMENTS FROM THOSE SHOWN ON THESE PLANS. CONTRACTOR TO SUBMIT SHOP DRAWINGS FOR REVIEW PRIOR TO ORDERING ANY SUBSTITUTED PRODUCTS.
11. CONTRACTOR SHALL EXAMINE AND VERIFY THAT SOIL CONDITIONS ARE SUITABLE TO SUPPORT LOADS EXERTED UPON THE FOUNDATIONS DURING EXCAVATION. CONTRACTOR SHALL NOTIFY ENGINEER OF ANY UNSATISFACTORY CONDITIONS.
12. POLE FOUNDATIONS SHALL NOT BE POURED IF FREE STANDING WATER IS PRESENT IN EXCAVATED AREA.
13. ALL POLES 25 FT OR GREATER SHALL BE EQUIPPED WITH FACTORY INSTALLED VIBRATION DAMPENERS.

WALL MOUNTED FIXTURES

14. CONTRACTOR TO COORDINATE INSTALLATION OF ALL THE WALL MOUNTED FIXTURES AND ELECTRICAL CONNECTIONS TO SITE STRUCTURE(S) WITH BUILDING MEP, ARCHITECT, AND/OR OWNER.
15. INSTALLATION AND ELECTRICAL CONNECTIONS FOR WALL MOUNTED FIXTURES TO BE COORDINATED WITH ARCHITECTURAL, STRUCTURAL, UTILITY AND SITE PLANS AND TO BE IN ACCORDANCE WITH ALL APPLICABLE CODES.

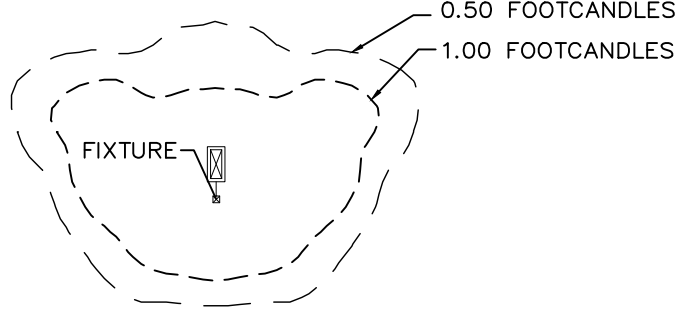
ADJUSTMENT AND INSPECTION

16. CONTRACTOR TO OPERATE EACH LUMINAIRE AFTER INSTALLATION AND CONNECTION. INSPECT FOR IMPROPER CONNECTIONS AND OPERATION.
17. CONTRACTOR TO AIM AND ADJUST ALL LUMINAIRES TO PROVIDE ILLUMINATION LEVELS AND DISTRIBUTION AS INDICATED ON THE CONSTRATION DRAWINGS OR AS DIRECTED BY THE LANDSCAPE ARCHITECT AND/OR OWNER.
18. CONTRACTOR TO CONFIRM THAT LIGHT FIXTURES, TILT ANGLE AND AIMING MATCH SPECIFICATIONS ON THE PLANS.

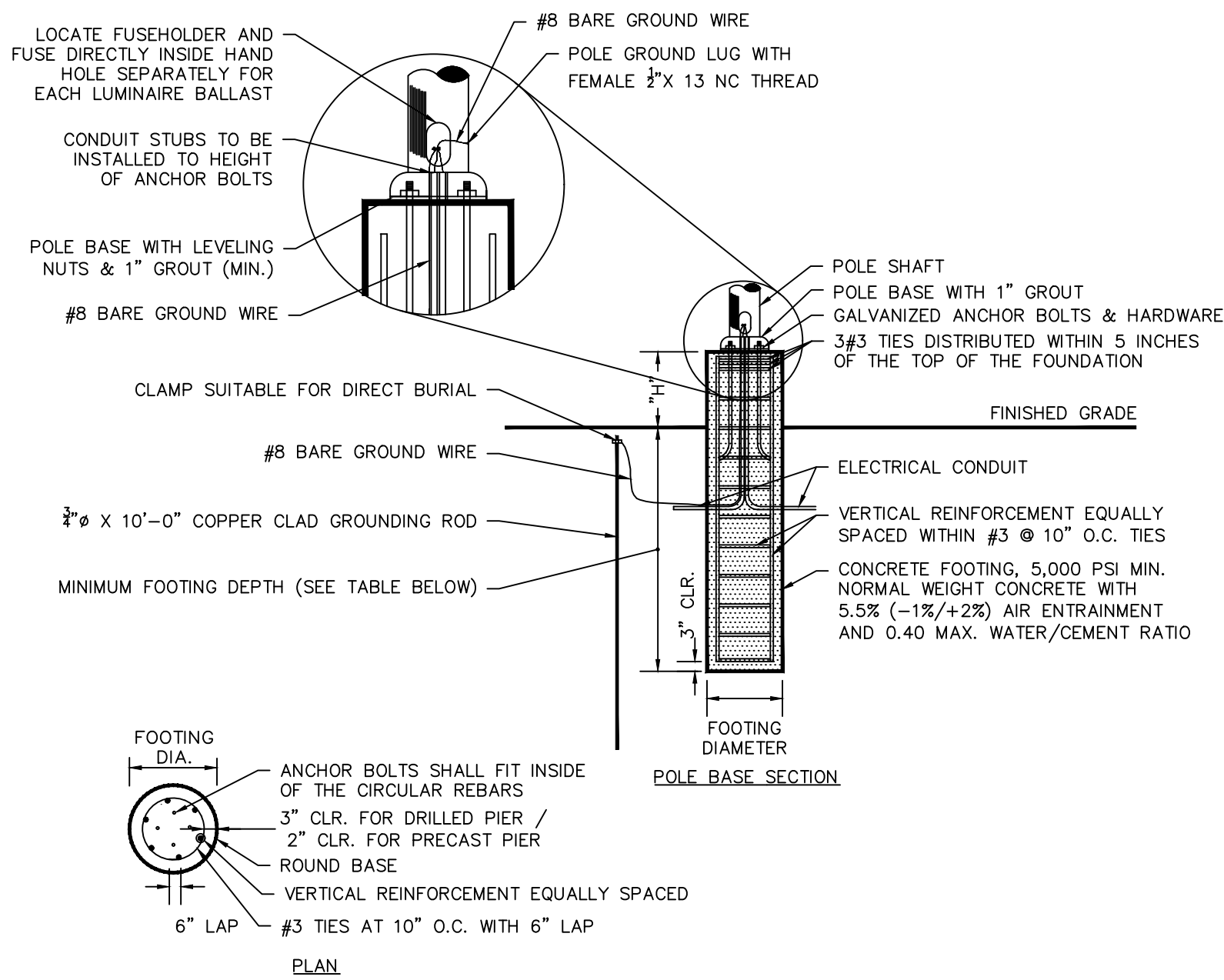
REQUIREMENTS FOR ALTERNATES

19. ALL LIGHTING SUBSTITUTIONS MUST BE MADE WITHIN 14 DAYS PRIOR TO THE BID DATE TO PROVIDE AMPLE TIME FOR REVIEW AND TO ISSUE AN ADDENDUM INCORPORATING THE SUBSTITUTION WITH THE FOLLOWING REQUIREMENTS:
- A. ANY SUBSTITUTION TO LIGHTING FIXTURES, POLES, ETC. MUST BE APPROVED BY THE OWNER, ENGINEER AND TENANTS. ANY COST ASSOCIATED WITH REVIEW AND/OR APPROVAL OF THE SUBSTITUTIONS SHALL BE ENTIRELY BORNE BY THE CONTRACTOR
- B. COMPUTER PREPARED PHOTOMETRIC LAYOUT OF THE PROPOSED LIGHTED AREA WHICH INDICATES, BY ISOFOOTCANDLE, THE SYSTEM'S PERFORMANCE.
- C. A PHOTOMETRIC REPORT FROM A NATIONAL INDEPENDENT TESTING LABORATORY WITH REPORT NUMBER, DATE, FIXTURE CATALOG NUMBER, LUMINAIRE AND LAMP SPECIFICATIONS; IES CALCULATIONS, POINT BY POINT FOOT CANDLE PLAN, STATISTIC ZONES SHOWING AVERAGE, MAXIMUM, MINIMUM AND UNIFORMITY RATIOS, SUMMARY, ISOLUX PLOT, AND CATALOGUE CUTS. CATALOGUE CUTS MUST IDENTIFY OPTICS, LAMP TYPE, DISTRIBUTION OPTICS, REFLECTOR, LENS, BALLASTS, WATTAGE, VOLTAGE, FINISH HOUSING DESCRIPTION AND ALL OTHER PERTINENT INFORMATION.
- D. POLE MANUFACTURER ASHTO CALCULATIONS INDICATING THE POLE AND ANCHOR BOLTS BEING SUBMITTED ARE CAPABLE OF SUPPORTING THE POLE AND FIXTURE SYSTEMS BEING UTILIZED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
- E. THE UNDERWRITERS LABORATORY LISTING AND FILE NUMBER FOR THE SPECIFIC FIXTURE(S) TO BE UTILIZED.
- F. A COLOR PHOTOGRAPH THAT CLEARLY SHOWS THE REPLACEMENT FIXTURE POLE MOUNTED, THE FIXTURE'S COLOR, FINISH, AND PHYSICAL CHARACTERISTICS.

PHOTOMETRIC LIGHTING LEGEND:



NOTE: THE PHOTOMETRIC TEMPLATE REPRESENTS LIGHT THROW FOR EACH INDIVIDUAL FIXTURE AND DOES NOT REPRESENT LIGHT COMING FROM OTHER SOURCES.



| FIXTURE TYPE | MOUNTING HEIGHT | FOOTING DEPTH | FOOTING DIAMETER | EXPOSED HEIGHT *H* | VERTICAL REINFORCEMENT |
|--------------|-----------------|---------------|------------------|--------------------|------------------------|
| A            | 40'             | 4'-6"         | 2'-0"            | 3'-0" BASE         | 6#5 BARS               |

1 LIGHT FIXTURE BASE

N.T.S

Ordering Number Logic  
Evolve™ LED Scalable Area Light (EASA)



EASA

| PROD. ID                                                           | VOLTAGE                                                                 | OPTICAL CODE                                             | DISTRIBUTION                                           | DRIVE CURRENT            | LED COLOR                                                                                                                                                                   | PE FUNCTION                                                                                                                                                                                                                                                                                      | MOUNTING                                                                                                                        | COLOR                                                                                   | OPTIONS                                                                                                                   |
|--------------------------------------------------------------------|-------------------------------------------------------------------------|----------------------------------------------------------|--------------------------------------------------------|--------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------|
| E = Evolve<br>A = Area<br>S = Scalable<br>A = Photometric Series A | 120-277<br>1 = 120"<br>2 = 208"<br>3 = 240"<br>4 = 277"<br>H = 347-480V | F = Front<br>L = Left<br>R = Right<br>N = Not Applicable | 5 = S250NA<br>(433W only)<br>7 = 7000NA<br>(633W only) | 40 = 4000K<br>57 = 5700K | A = 30° Beam for Square Pole supplied with leads<br>B = 30° Beam for Round Pole supplied with leads<br>C = 30° Beam for 2" Pipe Control<br>N = 34° Beam supplied with leads | 1 = None<br>2 = PE Rec. supplied with leads<br>3 = PE Rec. with Shorting Cap<br>4 = PE Rec. with Control<br>5 = Dimming PE<br>6 = PE Rec. with Shorting Cap<br>7 = PE Rec. with Shorting Cap<br>8 = PE Rec. with Shorting Cap<br>9 = PE Rec. with Shorting Cap<br>10 = PE Rec. with Shorting Cap | A = 30° Arm for Square Pole<br>B = 30° Arm for Round Pole<br>C = 30° Arm for 2" Pipe Control<br>N = 34° Arm supplied with leads | BLACK - Black<br>BRZ - Dark Bronze<br>F = Fairly<br>T = Fairly<br>XXX = Special Options | DX = Dimmable<br>10-10 Volt Input<br>+ Fairly<br>+ Extra Surge Protection<br>* Contact manufacturer for Tolerant mounting |

Light pattern shown in direction specified in relation to Pole and Fixture

| OPTICAL CODE | TYPE               | TYPICAL INITIAL LUMENS |       | TYPICAL SYSTEM WATTAGE |      | DISTRIBUTION ORIENTATION AVAILABLE |   |   | BUG RATINGS* |   |   |      |        |        | IES FILE NUMBER |  |
|--------------|--------------------|------------------------|-------|------------------------|------|------------------------------------|---|---|--------------|---|---|------|--------|--------|-----------------|--|
|              |                    | 4000K                  | 5000K | 300W                   | 350W | B                                  | U | G | B            | U | G | 300K | 4000K  | 5000K  |                 |  |
| D5           | Symmetric Medium   | 9400                   | 10200 | 120                    | 120  | 320                                | N | 3 | 0            | 2 | 3 | 0    | 2      | 455198 | 455199          |  |
| D6           | Symmetric Medium   | 12400                  | 13600 | 160                    | 160  | 320                                | N | 4 | 0            | 2 | 4 | 0    | 2      | 455197 | 455198          |  |
| D7           | Symmetric Medium   | 15300                  | 16500 | 199                    | 199  | 320                                | N | 4 | 0            | 2 | 4 | 0    | 2      | 455196 | 455197          |  |
| D8           | Symmetric Medium   | 18100                  | 19600 | 236                    | 236  | 320                                | N | 4 | 0            | 2 | 4 | 0    | 2      | 455195 | 455196          |  |
| D9           | Symmetric Medium   | 21800                  | 23600 | 285                    | 285  | 320                                | N | 5 | 0            | 3 | 5 | 0    | 3      | 455194 | 455195          |  |
| D10          | Symmetric Medium   | 25300                  | 27400 | 343                    | 343  | 320                                | N | 5 | 0            | 3 | 5 | 0    | 3      | 455193 | 455194          |  |
| D11          | Symmetric Short    | 10000                  | 11000 | 100                    | 100  | 320                                | N | 3 | 0            | 1 | 3 | 0    | 1      | 455192 | 455193          |  |
| D12          | Symmetric Short    | 13100                  | 14400 | 162                    | 162  | 320                                | N | 3 | 0            | 2 | 3 | 0    | 2      | 455191 | 455192          |  |
| D13          | Symmetric Short    | 16500                  | 17800 | 199                    | 199  | 320                                | N | 4 | 0            | 2 | 4 | 0    | 2      | 455190 | 455191          |  |
| D14          | Symmetric Short    | 19900                  | 21100 | 236                    | 236  | 320                                | N | 4 | 0            | 2 | 4 | 0    | 2      | 455189 | 455190          |  |
| D15          | Symmetric Short    | 23600                  | 25500 | 285                    | 285  | 320                                | N | 4 | 0            | 2 | 4 | 0    | 2      | 455188 | 455189          |  |
| D16          | Symmetric Short    | 27400                  | 29600 | 343                    | 343  | 320                                | N | 5 | 0            | 3 | 5 | 0    | 3      | 455187 | 455188          |  |
| D17          | Asymmetric Forward | 4800                   | 5000  | 62                     | 67   | F.L.R                              | 1 | 0 | 2            | 1 | 0 | 2    | 455186 | 455187 |                 |  |
| D18          | Asymmetric Forward | 6300                   | 6800  | 82                     | 82   | F.L.R                              | 1 | 0 | 2            | 1 | 0 | 2    | 455185 | 455186 |                 |  |
| D19          | Asymmetric Forward | 7800                   | 8400  | 101                    | 101  | F.L.R                              | 1 | 0 | 2            | 1 | 0 | 2    | 455184 | 455185 |                 |  |
| D20          | Asymmetric Forward | 9300                   | 10000 | 120                    | 120  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455183 | 455184 |                 |  |
| D21          | Asymmetric Forward | 11200                  | 12100 | 145                    | 145  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455182 | 455183 |                 |  |
| D22          | Asymmetric Forward | 13100                  | 14000 | 170                    | 170  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455181 | 455182 |                 |  |
| D23          | Asymmetric Forward | 15100                  | 16000 | 199                    | 199  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455180 | 455181 |                 |  |
| D24          | Asymmetric Forward | 17100                  | 18000 | 236                    | 236  | F.L.R                              | 3 | 0 | 3            | 3 | 0 | 3    | 455179 | 455180 |                 |  |
| D25          | Asymmetric Forward | 19100                  | 20000 | 285                    | 285  | F.L.R                              | 3 | 0 | 3            | 3 | 0 | 3    | 455178 | 455179 |                 |  |
| D26          | Asymmetric Forward | 21100                  | 22000 | 343                    | 343  | F.L.R                              | 3 | 0 | 3            | 3 | 0 | 3    | 455177 | 455178 |                 |  |
| D27          | Asymmetric Wide    | 5100                   | 5500  | 62                     | 67   | F.L.R                              | 1 | 0 | 1            | 1 | 0 | 1    | 455176 | 455177 |                 |  |
| D28          | Asymmetric Wide    | 6800                   | 7300  | 82                     | 82   | F.L.R                              | 1 | 0 | 1            | 1 | 0 | 1    | 455175 | 455176 |                 |  |
| D29          | Asymmetric Wide    | 8300                   | 9000  | 101                    | 101  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455174 | 455175 |                 |  |
| D30          | Asymmetric Wide    | 9900                   | 10700 | 120                    | 120  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455173 | 455174 |                 |  |
| D31          | Asymmetric Wide    | 11900                  | 12800 | 145                    | 145  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455172 | 455173 |                 |  |
| D32          | Asymmetric Wide    | 13900                  | 14800 | 170                    | 170  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455171 | 455172 |                 |  |
| D33          | Asymmetric Wide    | 15900                  | 16800 | 199                    | 199  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455170 | 455171 |                 |  |
| D34          | Asymmetric Wide    | 17900                  | 18800 | 236                    | 236  | F.L.R                              | 2 | 0 | 2            | 2 | 0 | 2    | 455169 | 455170 |                 |  |
| D35          | Asymmetric Wide    | 19900                  | 20800 | 285                    | 285  | F.L.R                              | 3 | 0 | 3            | 3 | 0 | 3    | 455168 | 455169 |                 |  |
| D36          | Asymmetric Wide    | 21900                  | 22800 | 343                    | 343  | F.L.R                              | 3 | 0 | 3            | 3 | 0 | 3    | 455167 | 455168 |                 |  |
| D37          | Asymmetric Narrow  | 32400                  | 35100 | 633                    | 633  | F.L.R                              | 3 | 0 | 3            | 3 | 0 | 3    | 455222 | 455223 |                 |  |

\*Rating values for B and G are based on rated lumens and may vary due to flux tolerances.

2 FIXTURES 'A'

N.T.S

NOTES:

1. SHAFT CAP, ARMS, BASE FLANGE, ANCHOR BOLTS, LEVELING NUTS, CONNECTION HARDWARE, BOLT COVERS, HANDHOLE COVER, AND BOLT CIRCLE TEMPLATE SHALL BE FURNISHED BY POLE MANUFACTURER.
2. EACH STANDARD TO BE PROTECTED AGAINST LIGHTNING WITH AN INTERCONNECTED GROUND ROD. THIS ROD SHALL BE BONDED PER SECTION NUMBER 250-86, N.E.C.
3. THE CONCRETE WORK SHALL COMPLY WITH THE REQUIREMENT OF ACI 318. CAST-IN-PLACE SHALL HAVE UNCONFINED COMPRESSIVE STRENGTH OF AT LEAST 5,000 PSI AT 28-DAYS. DEFORMED REINFORCEMENT BARS SHALL CONFORM TO ASTM A615, GRADE 60. CONTRACTOR TO ENSURE CONCRETE POLE BASES ARE POURED / PLACED ABSOLUTELY VERTICAL & LEVEL.
4. IF POLE BASE IS CAST-IN-PLACE, POLE BASE SHALL BE ONE CONTINUOUS POUR. EXPOSED PORTION OF BASE SHALL BE HAND-RUBBED SMOOTH.
5. CONTRACTOR TO COMPACT SUBGRADE AROUND POLE BASE PER EARTHWORK SPECIFICATIONS / GEOTECH REPORT.
6. FOOTING SHOP DRAWINGS TO BE SUBMITTED TO PROJECT CIVIL ENGINEER PRIOR TO INSTALLATION.
7. CONTRACTOR TO CONFIRM GROUNDING DESIGN WITH MEP.

LL501 SITE LIGHTING NOTES AND DETAILS

Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

Scale: As noted

October 30, 2025



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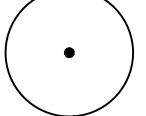




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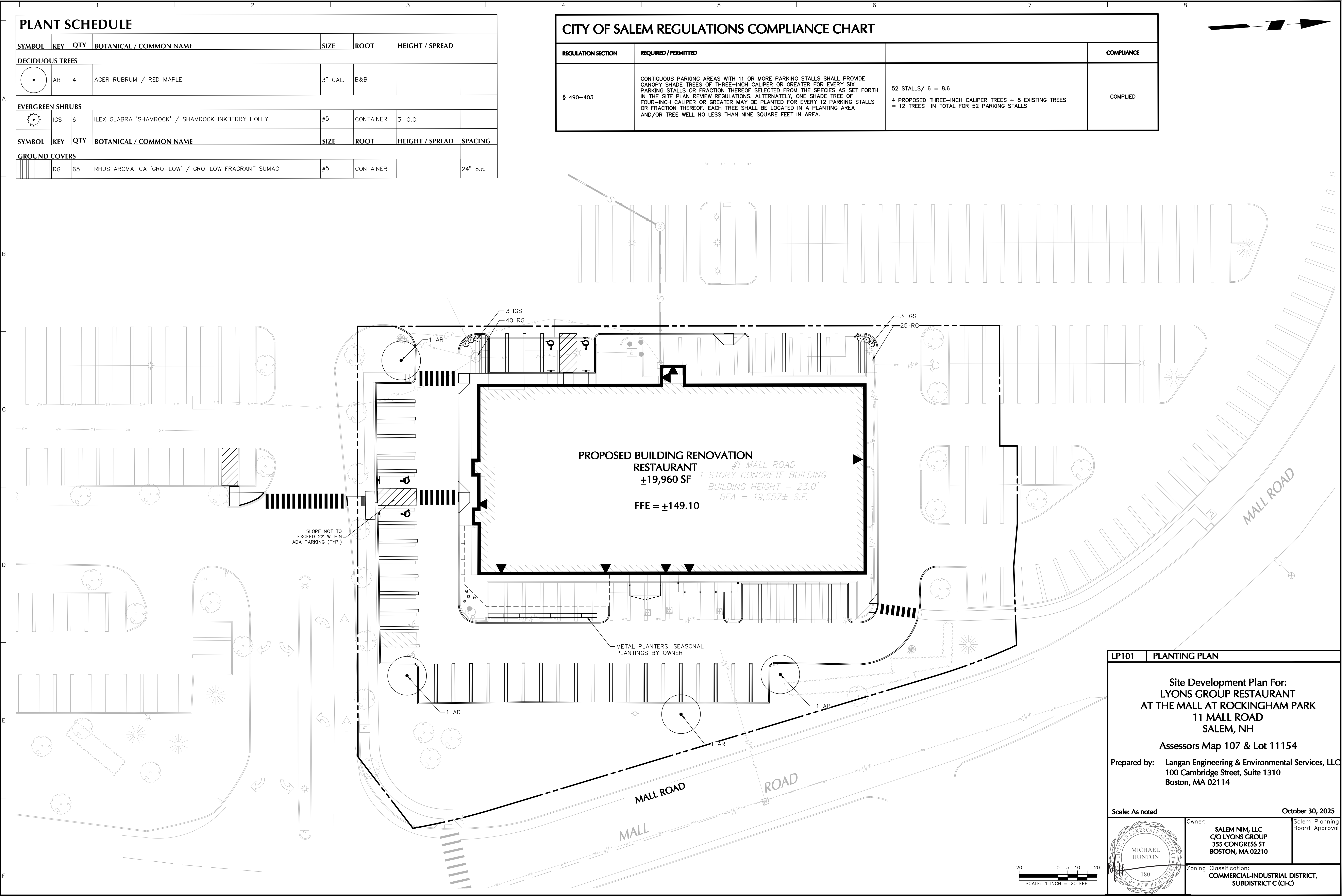
2

3

PLANT SCHEDULE

| SYMBOL                                                                            | KEY | QTY | BOTANICAL / COMMON NAME                           | SIZE    | ROOT      | HEIGHT / SPREAD |          |
|-----------------------------------------------------------------------------------|-----|-----|---------------------------------------------------|---------|-----------|-----------------|----------|
| DECIDUOUS TREES                                                                   |     |     |                                                   |         |           |                 |          |
|  | AR  | 4   | ACER RUBRUM / RED MAPLE                           | 3" CAL. | B&B       |                 |          |
| EVERGREEN SHRUBS                                                                  |     |     |                                                   |         |           |                 |          |
|  | IGS | 6   | ILEX GLABRA 'SHAMROCK' / SHAMROCK INKBERRY HOLLY  | #5      | CONTAINER | 3' O.C.         |          |
| SYMBOL                                                                            | KEY | QTY | BOTANICAL / COMMON NAME                           | SIZE    | ROOT      | HEIGHT / SPREAD | SPACING  |
| GROUND COVERS                                                                     |     |     |                                                   |         |           |                 |          |
|  | RG  | 65  | RHUS AROMATICA 'GRO-LOW' / GRO-LOW FRAGRANT SUMAC | #5      | CONTAINER |                 | 24" o.c. |

| CITY OF SALEM REGULATIONS COMPLIANCE CHART |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          |  |                                                                                                                                            |
|--------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--|--------------------------------------------------------------------------------------------------------------------------------------------|
| REGULATION SECTION                         | REQUIRED / PERMITTED                                                                                                                                                                                                                                                                                                                                                                                                                                                                     |  | COMPLIANCE                                                                                                                                 |
| § 490-403                                  | CONTIGUOUS PARKING AREAS WITH 11 OR MORE PARKING STALLS SHALL PROVIDE CANOPY SHADE TREES OF THREE-INCH CALIPER OR GREATER FOR EVERY SIX PARKING STALLS OR FRACTION THEREOF SELECTED FROM THE SPECIES AS SET FORTH IN THE SITE PLAN REVIEW REGULATIONS. ALTERNATELY, ONE SHADE TREE OF FOUR-INCH CALIPER OR GREATER MAY BE PLANTED FOR EVERY 12 PARKING STALLS OR FRACTION THEREOF. EACH TREE SHALL BE LOCATED IN A PLANTING AREA AND/OR TREE WELL NO LESS THAN NINE SQUARE FEET IN AREA. |  | 52 STALLS/ 6 = 8.6<br><br>4 PROPOSED THREE-INCH CALIPER TREES + 8 EXISTING TREES = 12 TREES IN TOTAL FOR 52 PARKING STALLS<br><br>COMPLIED |



LP101

PLANTING PLAN

Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH  
  
Assessors Map 107 & Lot 11154

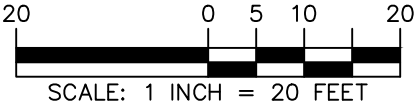
Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

Scale: As noted  
October 30, 2025

Owner:  
SALEM NIM, LLC  
C/O LYONS GROUP  
355 CONGRESS ST  
BOSTON, MA 02210

Salem Planning Board Approval

Zoning Classification:  
COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (CI-C)





## GENERAL LANDSCAPE PLANTING NOTES

- INVOICES FOR ALL PLANTING WITHIN WETLAND AND WETLAND BUFFER SHALL BE PROVIDED AND APPROVED BY CONSERVATION COMMISSION PRIOR TO INSTALLATION.
- NAMES OF PLANTS AS DESCRIBED ON THIS PLAN CONFORM TO THOSE GIVEN IN "STANDARDIZED PLANT NAMES", 1942 EDITION, PREPARED BY THE AMERICAN JOINT COMMITTEE ON HORTICULTURAL NOMENCLATURE. NAMES OF PLANT VARIETIES NOT INCLUDED THEREIN CONFORM TO NAMES GENERALLY ACCEPTED IN NURSERY TRADE.
- ALL EXPOSED GROUND SURFACES THAT ARE NOT PAVED WITHIN THE CONTRACT LIMIT LINE, AND THAT ARE NOT COVERED BY LANDSCAPE PLANTING OR SEEDING AS SPECIFIED, SHALL BE COVERED BY A NATURAL MULCH THAT WILL PREVENT SOIL EROSION AND THE EMANATION OF DUST.
- NO PLANT SHALL BE PUT INTO THE GROUND BEFORE ROUGH GRADING HAS BEEN COMPLETED AND APPROVED BY THE PROJECT LANDSCAPE ARCHITECT OR PROJECT ENGINEER.
- STANDARDS FOR TYPE, SPREAD, HEIGHT, ROOT BALL AND QUALITY OF NEW PLANT MATERIAL SHALL BE IN ACCORDANCE WITH GUIDELINES AS SET FORTH IN THE "AMERICAN STANDARD FOR NURSERY STOCK", PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSESMEN. PLANT MATERIAL SHALL HAVE NORMAL HABIT OF GROWTH AND BE HEALTHY, VIGOROUS, AND FREE FROM DISEASES AND INSECT INFESTATION.
- NEW PLANT MATERIAL SHALL BE NURSERY GROWN UNLESS SPECIFIED OTHERWISE. ALL PLANTS SHALL BE SET PLUMB AND SHALL BEAR THE SAME RELATIONSHIP TO FINISHED GRADE AS THE PLANT'S ORIGINAL GRADE BEFORE DIGGING. PLANT MATERIAL OF THE SAME SPECIES AND SPECIFIED AS THE SAME SIZE SHOULD BE SIMILAR IN SHAPE, COLOR, AND HABIT. THE LANDSCAPE ARCHITECT HAS THE RIGHT TO REJECT PLANT MATERIAL THAT DOES NOT CONFORM TO THE TYPICAL OR SPECIFIED HABIT OF THAT SPECIES.
- THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING UNDERGROUND UTILITY AND SEWER LINES PRIOR TO THE START OF EXCAVATION ACTIVITIES. NOTIFY THE PROJECT ENGINEER AND OWNER IMMEDIATELY OF ANY CONFLICTS WITH PROPOSED PLANTING LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE.
- THE CONTRACTOR SHALL NOT MAKE SUBSTITUTIONS. IF THE SPECIFIED LANDSCAPE MATERIAL IS NOT OBTAINABLE, THE CONTRACTOR SHALL SUBMIT PROOF OF NON-AVAILABILITY TO THE LANDSCAPE ARCHITECT AND OWNER, TOGETHER WITH A WRITTEN PROPOSAL FOR USE OF AN EQUIVALENT MATERIAL.
- LANDSCAPE CONTRACTOR TO STAKE OUT PLANTING LOCATIONS, FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT AND/OR OWNER BEFORE PLANTING WORK BEGINS. THE LANDSCAPE ARCHITECT AND/OR OWNER SHALL DIRECT THE CONTRACTOR IN THE FINAL PLACEMENT OF ALL PLANT MATERIAL AND LOCATION OF PLANTING BEDS TO ENSURE COMPLIANCE WITH DESIGN INTENT UNLESS OTHERWISE INSTRUCTED.
- THE LANDSCAPE ARCHITECT MAY REVIEW PLANT MATERIALS AT THE SITE, BEFORE PLANTING, FOR COMPLIANCE WITH REQUIREMENTS FOR GENUS, SPECIES, VARIETY, SIZE, AND QUALITY. THE LANDSCAPE ARCHITECT RETAINS THE RIGHT TO FURTHER REVIEW PLANT MATERIALS FOR SIZE AND CONDITION OF BALLS AND ROOT SYSTEM, INSECTS, INJURIES, AND LATENT DEFECTS, AND TO REJECT UNSATISFACTORY OR DEFECTIVE MATERIAL AT ANY TIME DURING PROGRESS OF WORK. THE CONTRACTOR SHALL REMOVE REJECTED PLANT MATERIALS IMMEDIATELY FROM PROJECT SITE AS DIRECTED BY THE LANDSCAPE ARCHITECT OR OWNER.
- DELIVERY, STORAGE, AND HANDLING
  - PACKAGED MATERIALS: PACKAGED MATERIALS SHALL BE DELIVERED IN CONTAINERS SHOWING WEIGHT, ANALYSIS, AND NAME OF MANUFACTURER. MATERIALS SHALL BE PROTECTED FROM DETERIORATION DURING DELIVERY, AND WHILE STORED AT SITE.
  - TREES AND SHRUBS: THE CONTRACTOR SHALL PROVIDE TREES AND SHRUBS DUG FOR THE GROWING SEASON FOR WHICH THEY WILL BE PLANTED. DO NOT PRUNE PRIOR TO DELIVERY UNLESS OTHERWISE DIRECTED BY THE LANDSCAPE ARCHITECT. DO NOT BEND OR BIND-TIE TREES OR SHRUBS IN SUCH A MANNER AS TO DAMAGE BARK, BREAK BRANCHES, OR DESTROY NATURAL SHAPE. PROVIDE PROTECTIVE COVERING DURING TRANSIT. DO NOT DROP BALLED AND BURLAPPED STOCK DURING DELIVERY OR HANDLING.
  - ALL PLANTS SHALL BE BALLED AND BURLAPPED OR CONTAINER GROWN AS SPECIFIED. NO CONTAINER GROWN STOCK WILL BE ACCEPTED IF IT IS ROOT BOUND. ALL ROOTBALL WRAPPING AND BINDING MATERIAL MADE OF SYNTHETICS OR PLASTICS SHALL BE REMOVED FROM THE TOP OF THE BALL AT THE TIME OF PLANTING. IF THE PLANT IS SHIPPED WITH A WIRE BASKET AROUND THE ROOT BALL, THE WIRE BASKET SHALL BE CUT AND FOLDED DOWN 8 INCHES INTO THE PLANTING HOLE. WITH CONTAINER GROWN STOCK, THE CONTAINER SHALL BE REMOVED AND THE ROOT BALL SHALL BE CUT THROUGH THE SURFACE IN TWO LOCATIONS.
  - THE CONTRACTOR SHALL HAVE TREES AND SHRUBS DELIVERED TO SITE AFTER PREPARATIONS FOR PLANTING HAVE BEEN COMPLETED AND PLANT IMMEDIATELY. IF PLANTING IS DELAYED MORE THAN 6 HOURS AFTER DELIVERY, THE CONTRACTOR SHALL SET TREES AND SHRUBS IN SHADE, PROTECT FROM WEATHER AND MECHANICAL DAMAGE AND KEEP ROOTS MOIST BY COVERING WITH MULCH, BURLAP OR OTHER ACCEPTABLE MEANS OF RETAINING MOISTURE.
- ALL LANDSCAPED AREAS TO BE CLEARED OF ROCKS, STUMPS, TRASH AND OTHER UNSIGHTLY DEBRIS. ALL FINE GRADED AREAS SHOULD BE HAND RAKED SMOOTH ELIMINATING ANY CLUMPS AND AND UNEVEN SURFACES PRIOR TO PLANTING OR MULCHING.
- ALL PLANT MATERIAL SHALL BE INSTALLED AS PER DETAILS, NOTES AND CONTRACT SPECIFICATIONS. THE LANDSCAPE ARCHITECT MAY REVIEW INSTALLATION AND MAINTENANCE PROCEDURES.
- NEW PLANT MATERIAL SHALL BE GUARANTEED TO BE ALIVE AND IN VIGOROUS GROWING CONDITION FOR A PERIOD OF TWO YEARS FOLLOWING ACCEPTANCE BY THE OWNER. PLANT MATERIAL FOUND TO BE UNHEALTHY, DYING OR DEAD DURING THIS PERIOD, SHALL BE REMOVED AND REPLACED IN KIND BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER.
- THE CONTRACTOR SHALL KEEP AREA CLEAN DURING DELIVERY AND INSTALLATION OF PLANT MATERIALS. REMOVE AND DISPOSE OF OFF-SITE ANY ACCUMULATED DEBRIS OR UNUSED MATERIALS. REPAIR DAMAGE TO ADJACENT AREAS CAUSED BY LANDSCAPE INSTALLATION OPERATIONS.
- ALL PLANTS SHALL BE WATERED THOROUGHLY TWICE DURING THE FIRST 24-HOUR PERIOD AFTER PLANTING. ALL PLANTS SHALL THEN BE WATERED WEEKLY OR AS REQUIRED BY SITE AND WEATHER CONDITIONS TO MAINTAIN VIGOROUS AND HEALTHY PLANT GROWTH.
- THE BACKFILL MIXTURE AND SOIL MIXES TO BE INSTALLED PER THE SPECIFICATIONS.
- AFTER PLANT IS PLACED IN TREE PIT LOCATION, ALL TWINE HOLDING ROOT BALL TOGETHER SHOULD BE COMPLETELY REMOVED AND THE BURLAP SHOULD BE PULLED DOWN SO 1/3 OF THE ROOT BALL IS EXPOSED. SYNTHETIC BURLAP SHOULD BE COMPLETELY REMOVED AFTER INSTALLATION.
- MULCH SHOULD NOT BE PILED UP AROUND THE TRUNK OF ANY PLANT MATERIAL. NO MULCH OR TOPSOIL SHOULD BE TOUCHING THE BASE OF THE TRUNK ABOVE THE ROOT COLLAR.
- ALL FENCE INSTALLATION SHALL BE COMPLETED PRIOR TO COMMENCEMENT OF ANY LANDSCAPE PLANTING, LAWN AND GRASSES, OR IRRIGATION WORK.
- FOR ANY DISCREPANCIES BETWEEN THE PLANT SCHEDULE AND PLANTING PLAN THE GRAPHIC QUANTITY SHOWN SHALL GOVERN.
- PLANT MATERIALS SHALL NOT BE PLANTED UNTIL THE FINISHED GRADING HAS BEEN COMPLETED.
- ALL PLANT INSTALLATIONS SHALL BE COMPLETED EITHER BETWEEN APRIL 1 – JUNE 15 OR AUGUST 15 – NOVEMBER 1, UNLESS OTHERWISE DIRECTED BY THE PROJECT LANDSCAPE ARCHITECT. SEE LAWN SEEDING DATES IN SEEDING NOTES.

## LANDSCAPE MAINTENANCE NOTES

- MAINTENANCE OPERATIONS BEFORE APPROVAL.**
  - PLANT CARE SHALL BEGIN IMMEDIATELY AFTER EACH PLANT IS SATISFACTORILY INSTALLED AND SHALL CONTINUE THROUGHOUT THE LIFE OF THE CONTRACT UNTIL FINAL ACCEPTANCE OF THE PROJECT.
  - CARE SHALL INCLUDE, BUT NOT BE LIMITED TO, REPLACING MULCH THAT HAS BEEN DISPLACED BY EROSION OR OTHER MEANS, REPAIRING AND RESHAPING WATER RINGS OR SAUCERS, MAINTAINING STAKES AND GUYS AS ORIGINALLY INSTALLED, WATERING WHEN NEEDED OR DIRECTED, AND PERFORMING ANY OTHER WORK REQUIRED TO KEEP THE PLANTS IN A HEALTHY CONDITION.
  - CONTRACTOR SHALL REMOVE AND REPLACE ALL DEAD, DEFECTIVE AND/OR REJECTED PLANTS AS REQUIRED BEFORE FINAL ACCEPTANCE.
- MAINTENANCE DURING CONSTRUCTION.**
  - MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER PLANTING. PLANTS SHALL BE WATERED, MULCHED, WEEDED, PRUNED, SPRAYED, FERTILIZED, CULTIVATED, AND OTHERWISE MAINTAINED AND PROTECTED UNTIL PROVISIONAL ACCEPTANCE. SETTLED PLANTS SHALL BE RESET TO PROPER GRADE AND POSITION, PLANTING SAUCER RESTORED AND DEAD MATERIAL REMOVED. STAKES AND WIRES SHALL BE TIGHTENED AND REPAIRED. DEFECTIVE WORK SHALL BE CORRECTED AS SOON AS POSSIBLE AFTER IT BECOMES APPARENT AND WEATHER AND SEASON PERMIT.
  - IF A SUBSTANTIAL NUMBER OF PLANTS ARE SICKLY OR DEAD AT THE TIME OF INSPECTION, ACCEPTANCE SHALL NOT BE GRANTED AND THE CONTRACTOR'S RESPONSIBILITY FOR MAINTENANCE OF ALL PLANTS SHALL BE EXTENDED FROM THE TIME REPLACEMENTS ARE MADE OR EXISTING PLANTS ARE DEEMED ACCEPTABLE BY THE LANDSCAPE ARCHITECT.
  - ALL REPLACEMENTS SHALL BE PLANTS OF THE SAME KIND AND SIZE SPECIFIED ON THE PLANT LIST OR THAT WHICH WAS TO REMAIN OR BE RELOCATED. THEY SHALL BE FURNISHED AND PLANTED AS SPECIFIED. THE COST SHALL BE BORNE BY THE CONTRACTOR. REPLACEMENTS RESULTING FROM REMOVAL, LOSS, OR DAMAGE DUE TO OCCUPANCY OF THE PROJECT IN ANY PART, VANDALISM, PHYSICAL DAMAGE BY ANIMALS, VEHICLES, ETC., AND LOSSES DUE TO CURTAILMENT OF WATER BY LOCAL AUTHORITIES SHALL BE APPROVED AND PAID FOR BY THE OWNER.
  - PLANTS SHALL BE GUARANTEED FOR A PERIOD OF TWO YEARS AFTER INSPECTION AND PROVISIONAL ACCEPTANCE.
  - AT THE END OF THE ESTABLISHMENT PERIOD, INSPECTION SHALL BE MADE AGAIN. ANY PLANT REQUIRED UNDER THIS CONTRACT THAT IS DEAD OR UNSATISFACTORY TO THE LANDSCAPE ARCHITECT OR OWNER SHALL BE REMOVED FROM THE SITE AND REPLACED DURING THE NORMAL PLANTING SEASON.
- LAWN MAINTENANCE.**
  - BEGIN MAINTENANCE IMMEDIATELY AFTER EACH PORTION OF LAWN IS PLANTED AND CONTINUE FOR 8 WEEKS AFTER ALL LAWN PLANTING IS COMPLETED.
  - WATER TO KEEP SURFACE SOIL MOIST, REPAIR WASHED OUT AREAS BY FILLING WITH TOPSOIL, LIMING, FERTILIZING AND RE-SEEDING; MOW TO 2 1/2 – 3 INCHES AFTER GRASS REACHES 3 1/2 INCHES IN HEIGHT, AND MOW FREQUENTLY ENOUGH TO KEEP GRASS FROM EXCEEDING 3 1/2 INCHES. WEEED BY LOCAL SPOT APPLICATION OF SELECTIVE HERBICIDE ONLY AFTER GRASS IS WELL-ESTABLISHED.
  - IN ORDER TO REMOVE WEEDS FROM THE LAWN AREAS, APPLY PRE-EMERGENT HERBICIDE ONCE GRASS IS WELL ESTABLISHED. APPLY POST-EMERGENT HERBICIDE AS NEEDED, IF WEEDS ARE STILL PRESENT, PER SUPPLIER.

## PLANTING SOIL SPECIFICATIONS

- PLANTING SOIL, ALTERNATELY MAY BE REFERRED TO AS TOPSOIL, SHOULD BE FRIABLE, FERTILE, WELL DRAINED, FREE OF DEBRIS, TOXINS, TRASH AND STONES OVER 1/2" DIA., IT SHOULD HAVE A HIGH ORGANIC CONTENT SUITABLE TO SUSTAIN HEALTHY PLANT GROWTH AND SHOULD LOOK AESTHETICALLY PLEASING HAVING NO NOXIOUS ODOORS.
- PLANTING SOIL.**

REUSE SURFACE SOILS STOCKPILED ON SITE, VERIFYING COMPLIANCE WITH PLANTING SOIL AND TOPSOIL CRITERIA IN THIS SPECIFICATION THROUGH TESTING. CLEAN SURFACE SOIL OF ALL ROOTS, PLANTS, SOD, AND GRAVEL OVER 1" IN DIAMETER AND DELETERIOUS MATERIALS. IF ON-SITE SOILS ARE TO BE USED FOR PROPOSED PLANTING, THE CONTRACTOR SHALL DEMONSTRATE, THROUGH SOIL TESTING, THAT ON-SITE SOILS MEET THE SAME CRITERIA AS INDICATED IN NOTES PLANS AND SPECIFICATIONS.

SUPPLEMENT WITH IMPORTED OR MANUFACTURED TOPSOIL FROM OFF SITE SOURCES WHEN TOPSOIL AND PLANTING SOIL QUANTITIES ARE INSUFFICIENT. OBTAIN SOIL DISPLACED FROM NATURALLY WELL-DRAINED SITES WHERE TOPSOIL OCCURS AT LEAST 4" DEEP. DO NOT OBTAIN FROM AGRICULTURAL LAND, BOGS, MARSHES OR CONTAMINATED SITES.

CONTRACTOR SHALL TEST SOILS AND FURNISH SAMPLES UPON REQUEST. PACKAGED MATERIALS SHALL BE UNOPENED BAGS OR CONTAINERS, EACH BEARING A NAME, GUARANTEE, AND TRADEMARK OF THE PRODUCER, MATERIAL COMPOSITION, MANUFACTURER'S CERTIFIED ANALYSIS, AND THE WEIGHT OF THE MATERIALS. SOIL OR AMENDMENT MATERIALS SHALL BE STORED ON SITE TEMPORARILY IN STOCKPILES PRIOR TO PLACEMENT AND SHALL BE PROTECTED FROM INTRUSION OF CONTAMINANTS AND EROSION. AFTER MIXING, SOIL MATERIALS SHALL BE COVERED WITH A TARPULIN UNTIL TIME OF ACTUAL USE.

ALL PLANTING SOILS SHALL BE SUBMITTED FOR TESTING TO THE STATE COOPERATIVE EXTENSION SERVICE, OR APPROVED EQUAL, PRIOR TO DELIVERY TO THE SITE. CONTRACTOR SHALL FURNISH SOIL SAMPLES AND SOIL TEST RESULTS TO LANDSCAPE ARCHITECT OR OWNER AT A RATE OF ONE SAMPLE PER 500 CUBIC YARDS TO ENSURE CONSISTENCY ACROSS THE TOTAL VOLUME OF PLANTING SOIL REQUIRED. TEST RESULTS SHALL EVALUATE FOR ALL CRITERIA LISTED IN THIS SPECIFICATION. IF TESTING AGENCY DETERMINES THAT THE SOILS ARE DEFICIENT IN ANY MANNER AND MAY BE CORRECTED BY ADDING AMENDMENTS, THE CONTRACTOR SHALL FOLLOW STATED RECOMMENDATIONS FOR SOIL IMPROVEMENT AND FURNISH SUBMITTALS FOR ALL AMENDMENTS PRIOR TO DELIVERY OF SOIL TO THE PROJECT SITE.

  - THE FOLLOWING TESTING SHOULD BE PERFORMED AND RESULTS GIVEN TO THE LANDSCAPE ARCHITECT FOR APPROVAL BEFORE INSTALLATION:
    - PARTICLE SIZE ANALYSIS – LOAMY SAND: 60–75% SAND, 25–40% SILT, AND 5–15% CLAY.
    - FERTILITY ANALYSIS: pH (5.5–6.5), SOLUBLE SALTS (LESS THAN 2 MMHO/CM), NITRATE, PHOSPHATE, POTASSIUM, CALCIUM AND MAGNESIUM
    - ORGANIC MATTER CONTENT: 2.5–5% IN NATIVE SOILS; UP TO 10% IN AMENDED SOILS
    - TOXIC SUBSTANCE ANALYSIS
    - MATERIAL DRAINAGE RATE: 60% PASSING IN 2 MINUTES, 40% RETAINED
    - NOT MORE THAN 1% OF MATERIAL SHALL BE RETAINED BY A #4 SIEVE
  - SOIL AMENDMENT FOR PLANT MATERIAL:

IF SOIL ORGANIC CONTENT IS INADEQUATE, SOIL SHALL BE AMENDED WITH COMPOST OR ACCEPTABLE, WEED FREE, ORGANIC MATTER. ORGANIC AMENDMENT SHALL BE WELL COMPOSTED, PH RANGE OF 6–8; MOISTURE CONTENT 35–55% BY WEIGHT 100% PASSING THROUGH 1" SIEVE; SOLUBLE SALT CONTENT LESS THAN 0.5 MM HOS/CM; MEETING ALL APPLICABLE ENVIRONMENTAL CRITERIA FOR CLEAN FILL.

    - ORGANIC MATTER AS A SOIL AMENDMENT: LEAF MOLD WITH 60–90% ORGANIC CONTENT BY WEIGHT. SHREDDED LEAF LITTER, COMPOSTED FOR A MINIMUM OF 1 YR. SHOULD BE FREE OF DEBRIS, STONES OVER 1/2", WOOD CHIPS OVER 1".
    - SOIL IN BEDS AND PLANTING ISLANDS OTHER THAN BACKFILL MATERIAL AND TOPSOIL, SHOULD BE FRIABLE, WELL DRAINED, AND FREE OF DEBRIS, INCLUDING STONES AND TRASH.
    - AMENDMENTS FOR BACK FILL IN TREE AND SHRUB PITS:
      - GROUND LIMESTONE (WITH A MIN. OF 88% OF CALCIUM AND MAGNESIUM CARBONATES) USED PENDING RESULTS OF SOIL ANALYSIS.

BRING pH LEVELS TO 5.5 MIN. TO 6.5 FOR NON-ERICACEOUS PLANTS  
BRING pH LEVELS TO 4.5 MIN. TO 5.5 FOR ERICACEOUS PLANTS
      - TERRA-SORB BY 'PLANT HEALTH CARE' 800–421–9051 (SEE MANUFACTURER RECOMMENDATIONS) USED IN PLANTER BACKFILL MIXTURE WITH TREES AND SHRUBS.
      - MYCOR-RSOT SAVER BY 'PLANT HEALTH CARE' 800–421–9051 (SEE MANUFACTURER RECOMMENDATIONS) USED IN BACKFILL MIXTURE WITH TREES.

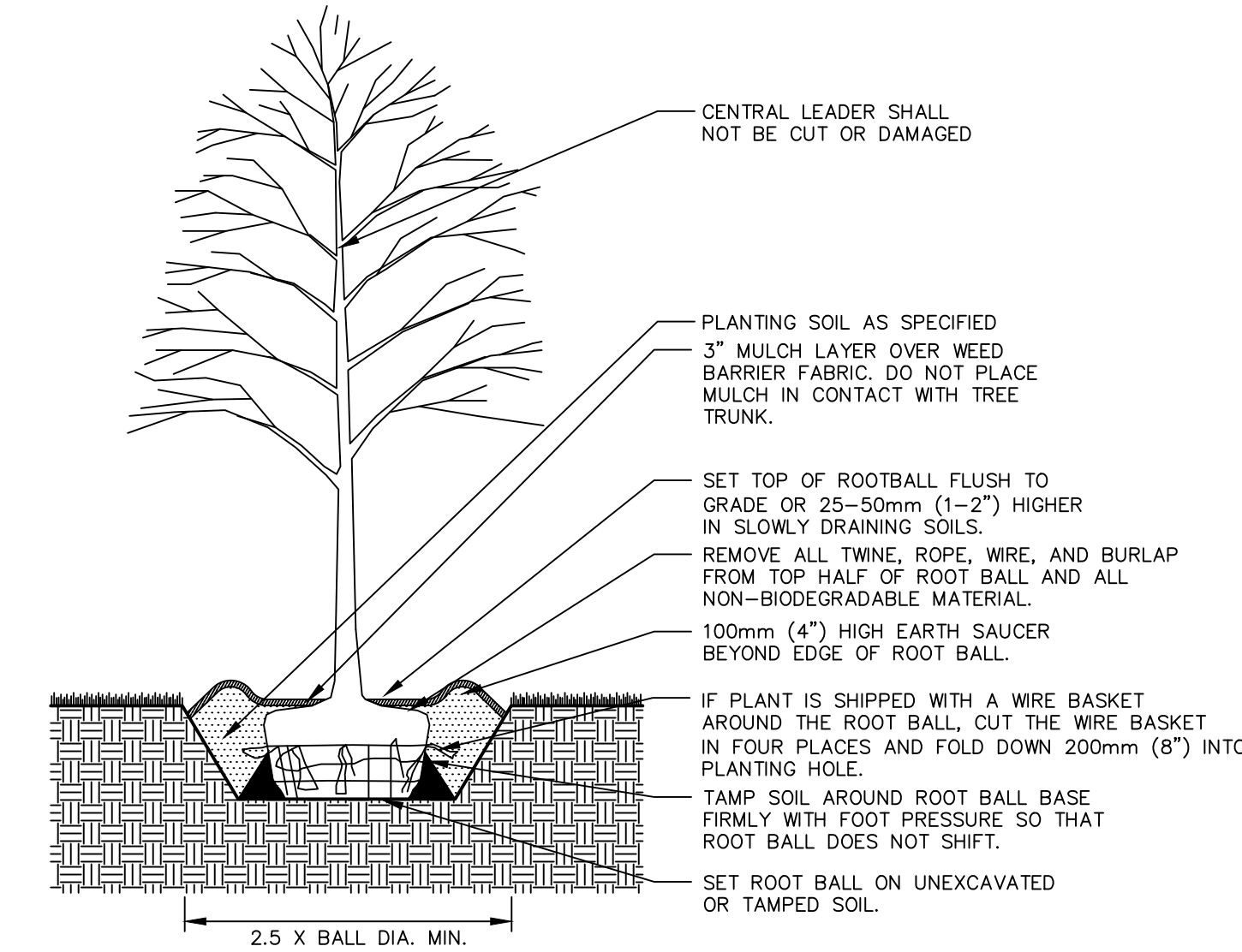
- WHERE PLANTING AREAS ARE PROPOSED FOR FORMER PAVED OR GRAVEL AREAS, BEDS SHALL BE EXCAVATED TO A MINIMUM 30" DEPTH AND, AT A MINIMUM, BE BACKFILLED WITH BOTTOM LAYER OF SANDY LOAM (ORGANIC CONTENT LESS THAN 2%) OVER WHICH TOPSOIL AND PLANTING SOILS WILL BE PLACED AT DEPTHS INDICATED IN PLANS, DETAILS AND NOTES.
- INFILTRATION/ DETENTION BASIN SOIL MIX
  - SPECIFIED SOIL MIX IS TO BE USED IN ALL DETENTION AND INFILTRATION BASINS.
  - TOPSOIL/HORTICULTURAL SOIL MIX: REFER TO SPECIFICATIONS LISTED IN SECTION ABOVE
    - CHEMICAL ANALYSIS  
PH: LOWER THAN 7.0  
TOXIC SUBSTANCE ANALYSIS
    - FINAL BIORETENTION MIX
      - CHEMICAL ANALYSIS
        - PH – 5.5–6.5
        - SOLUBLE SALTS: LESS THAN 2 MMHO/CM

- CLEAN SOIL FILL IN LANDSCAPE AREAS:**

LANDSCAPE FILL MATERIAL, BELOW PLANTING SOILS, SHALL HAVE THE PHYSICAL PROPERTIES OF A SANDY LOAM WITH AN ORGANIC CONTENT OF LESS THAN 2% AND A PH BETWEEN 5 – 7.
- SOIL PLANTING:**
  - CONTRACTOR TO PROVIDE SIX INCHES (6") MINIMUM DEPTH PLANTING SOIL LAYER IN LAWN AREAS, TWELVE INCHES (12") MINIMUM DEPTH PLANTING SOIL LAYER IN GROUNDCOVER AND PERENNIAL AREAS, EIGHTEEN INCHES (18") MINIMUM DEPTH PLANTING SOIL LAYER IN SHRUB AREAS, AND THIRTY-SIX INCHES (36") MINIMUM DEPTH PLANTING SOIL LAYER IN TREE PLANTING AREAS.
  - SCARIFY AND/OR TILL COMPACTED SUBSOILS TO A MINIMUM DEPTH OF 6 INCHES. THOROUGHLY MIX A 6 INCH DEPTH LAYER OF PLANTING SOIL INTO THE SUBSOIL PRIOR TO PLACING PLANTING SOIL AT THE DEPTHS INDICATED ABOVE. PLANTING SOIL SHALL BE PLACED IN 12–18" LIFTS AND WATER THOROUGHLY BEFORE INSTALLING NEXT LIFT. REPEAT UNTIL DEPTHS AND FINISH GRADES HAVE BEEN ACHIEVED. NO SOILS SHALL BE PLACED IN A FROZEN OR MUDDY CONDITION.
  - PLANTING SOIL PRESENT AT THE SITE, IF ANY, MAY BE USED TO SUPPLEMENT TOTAL AMOUNT REQUIRED. CONTRACTOR TO FURNISH AN ANALYSIS OF ON-SITE PLANTING SOIL UTILIZED IN ALL PLANTING AREAS.
- SOIL CONDITIONING:**
  - ADJUST pH AND NUTRIENT LEVELS AS REQUIRED TO ENSURE AN ACCEPTABLE GROWING MEDIUM. LOWER pH USING ELEMENTAL SULFUR ONLY. PEAT MOSS OR COPPER SULFATE MAY NOT BE USED. GROUND LIMESTONE AS A SOIL AMENDMENT MATERIAL WILL ONLY BE USED PENDING RESULTS OF SOIL ANALYSIS. PROVIDE WITH MINIMUM 88% CALCIUM AND MAGNESIUM CARBONATES AND SHALL HAVE TOTAL 100% PASSING THE 10 MESH SIEVE, MINIMUM 90% PASSING 20 MESH SIEVE, AND MINIMUM 60% PASSING 100 MESH SIEVE.
  - ALL DEBRIS EXPOSED FROM EXCAVATION AND CULTIVATION SHALL BE DISPOSED OF AT THE CONTRACTOR'S EXPENSE.
  - SOIL MODIFICATIONS (PENDING RESULTS OF SOIL ANALYSIS):**
    - THOROUGHLY TILL ORGANIC MATTER (LEAF COMPOST) INTO THE TOP 6 TO 12 IN. OF MOST PLANTING SOILS TO IMPROVE THE SOIL'S ABILITY TO RETAIN WATER AND NUTRIENTS. ALL PRODUCTS SHOULD BE COMPOSTED TO A DARK COLOR AND BE FREE OF PIECES WITH IDENTIFIABLE LEAF OR WOOD STRUCTURE. AVOID MATERIAL WITH A pH HIGHER THAN 7.0. PEAT MOSS MAY NOT BE USED AS ORGANIC MATTER AMENDMENT.
    - MODIFY HEAVY CLAY OR SILT (MORE THAN 40% CLAY OR SILT) BY ADDING COMPOSTED PINE BARK (UP TO 30% BY VOLUME) AND/OR GYPSUM. COARSE SAND MAY BE USED IF ENOUGH IS ADDED TO BRING THE SAND CONTENT TO MORE THAN 60% OF THE TOTAL MIX. IMPROVE DRAINAGE IN HEAVY SOILS BY PLANTING ON RAISED MOUNDS OR BEDS INCLUDING SUBSURFACE DRAINAGE LINES.
    - MODIFY EXTREMELY SANDY SOILS (MORE THAN 85% SAND) BY ADDING ORGANIC MATTER AND/OR DRY, SHREDDED CLAY LOAM UP TO 30% OF THE TOTAL MIX.

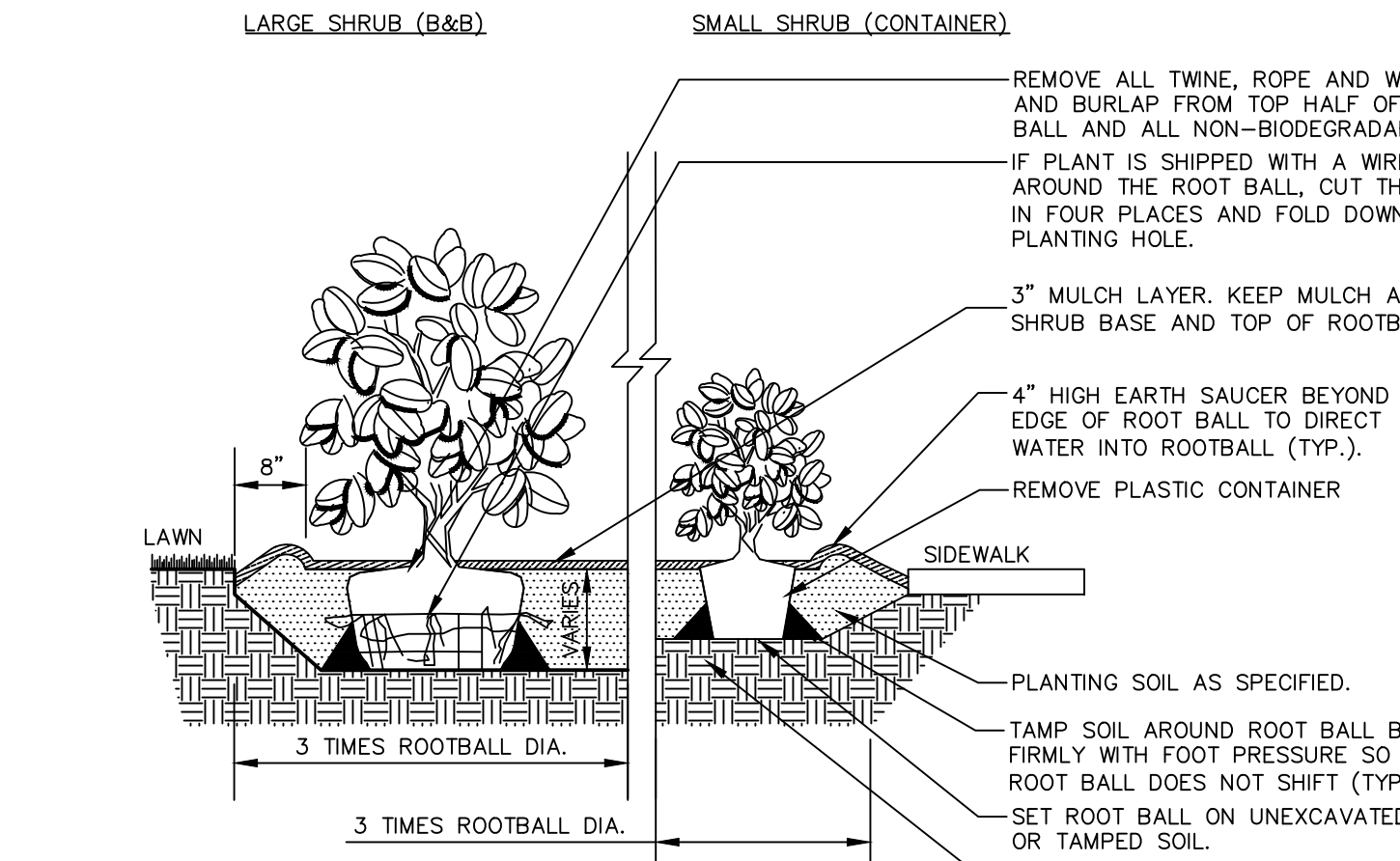
## IRRIGATION NOTES:

- THE IRRIGATION CONTRACTOR SHALL PROVIDE SHOP DRAWINGS OF THE IRRIGATION INSTALLATION PLAN AND OUT-SHEETS FOR ALL COMPONENTS FOR REVIEW AND APPROVAL BY THE PROJECT LANDSCAPE ARCHITECT OR OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION. THE IRRIGATION INSTALLATION PLAN SHALL BE COMPLETE WITH ZONE DESIGNATIONS AND WATER USAGE IN GALLONS PER MINUTE PER ZONE, RUN TIME SCHEDULE, LEGEND OF COMPONENTS AND PLAN GRAPHICS WITH QUANTITIES. MINIMUM SYSTEM REQUIREMENTS INCLUDING STATIC PRESSURE AT THE WATER CONNECTION POINT, ESTIMATED WATER BUDGET, CONSTRUCTION DETAILS AND IRRIGATION NOTES. THE PLAN SHALL ALSO INCLUDE LOCATIONS OF ALL PROPOSED SLEEVES AND THEIR SIZES, LOCATIONS OF ALL LATERAL LINE SIZE STEP-DOWNS WITH SIZE INDICATIONS, LOCATION OF ALL SOIL MOISTURE SENSORS, CONTROLLER, VALVES AND ALL OTHER COMPONENTS NECESSARY FOR THE SYSTEMS OPERATION.
- LANDSCAPE AREAS SHALL BE IRRIGATED WITH POP-UP SPRAY AND ROTARY IRRIGATION HEADS IN SUFFICIENT DENSITY TO COVER THE ENTIRE AREA.
- CONTRACTOR TO AVOID DISTURBANCE OF EXISTING PLANT MATERIAL WHEN LOCATING VALVES AND PIPE LINES. ANY PLANT MATERIAL DAMAGED AS A RESULT OF IRRIGATION INSTALLATION SHALL BE REPLACED AT NO ADDITIONAL COST TO THE OWNER.
- ALL EXCAVATION MATERIAL SHALL BE PLACED BACK IN TRENCHES.
- ALL DISTURBED LANDSCAPE AND PAVED AREAS SHALL BE RESTORED TO THE CONDITION FOUND PRIOR TO START OF INSTALLATION.
- DEPTH OF TRENCHES SHALL BE SUFFICIENT OR PROVIDE A MINIMUM COVER ABOVE THE TOP OF PIPE AS FOLLOWS:
  - 12" OVER NON-PRESSURE LATERAL LINES
  - 18" OVER NON-PRESSURE LATERAL LINES UNDER PAVING
  - 18" OVER CONTROL WIRES
  - 18" OVER MAIN LINE
  - 24" OVER MAIN LINE UNDER PAVING
- THE IRRIGATION CONTRACTOR IS RESPONSIBLE FOR COORDINATING THE LOCATION OF THE PLUMBING TIE-INS, SLEEVES UNDER PAVEMENTS (AS NECESSARY), AND CONTROL DEVICES WITH THE GENERAL CONTRACTOR, OWNER, AND OWNER'S REPRESENTATIVE.
- CONTRACTOR TO COORDINATE INSTALLATION OF IRRIGATION SYSTEM WITH EXISTING AND PROPOSED UTILITIES, SITE DRAINAGE SYSTEMS, AND PAVING.
- CONTRACTOR SHALL PROMPTLY NOTIFY THE OWNER'S REPRESENTATIVE SHOULD ANY UTILITIES, NOT SHOWN ON THE PLANS, BE FOUND DURING INSTALLATION WORK.
- WATERPROOF ALL WIRE CONNECTORS USING 3M "DBY" WATERPROOF CONNECTORS OR EQUIVALENT.
- DRAIN VALVES ARE TO BE PROVIDED AT SUFFICIENT INTERVALS TO PROVIDE COMPLETE DRAINAGE OF ALL PIPING.
- COORDINATE THE LOCATION OF CONTROLS, IRRIGATION CONTROLLER, AND SOIL MOISTURE SENSORS WITH THE PROJECT MEP AND OWNER PRIOR TO INSTALLATION.
- IRRIGATION CONTRACTOR SHALL MAKE FIELD ADJUSTMENTS TO IRRIGATION DESIGN WHERE REQUIRED TO PROVIDE 100% COVERAGE OF ALL LANDSCAPE AREAS, AS DESIGNATED ON THIS PLAN.
- INSTALLATION MUST COMPLY WITH ALL LOCAL CODES AND CONDITIONS.
- ALL IRRIGATION WORK SHALL BE GUARANTEED FOR 1 YEAR AFTER COMPLETION OF ALL WORK.
- CONTRACTOR TO PROVIDE THREE (3) COPIES OF AS-BUILTS, SERVICE MANUALS AND INSTRUCTIONS TO THE OWNER OR OWNER'S REPRESENTATIVE.
- ALL SPRINKLER HEADS SHALL BE SET BACK 4" MINIMUM FROM BACK OF ALL CURBS.
- CONTRACTOR MAY SUBMIT ALTERNATE EQUIVALENT MATERIALS FOR REVIEW AND APPROVAL BY OWNER'S REPRESENTATIVE OR PROJECT LANDSCAPE ARCHITECT.



## TREE PLANTING

NTS



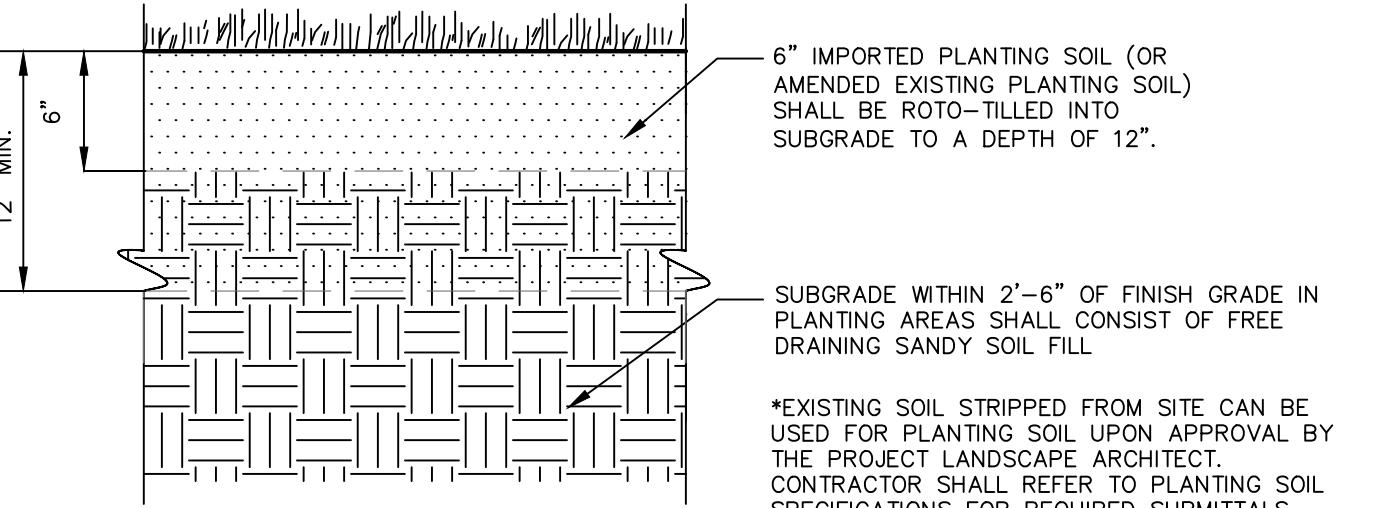
- NOTES:
- ALL SHRUBS TO BE SET PLUMB.
  - REFER TO LANDSCAPE PLAN FOR SPACING OF INDIVIDUAL PLANTS.
  - REMOVE ALL WIRE PLASTIC, TAGS OR SYNTHETIC MATERIAL FROM PLANTS PRIOR TO PLANTING.

## SHRUB AND ORNAMENTAL GRASS PLANTING

NTS

GENERAL NOTE:

DUE TO GENERAL CONSTRUCTION ACTIVITIES AND ADJACENT SITE COMPACTION REQUIREMENTS, SUBGRADE SOILS WITHIN PROPOSED PLANTING AREAS TEND TO BECOME HIGHLY COMPACTED. IN ORDER TO CREATE A HEALTHY GROWTH MEDIUM TO ALLOW PROPOSED PLANTINGS TO ESTABLISH A VIGOROUS ROOT MASS, THIS SUBGRADE SOIL MUST UNDERGO A RESTORATION PROCESS. IN ADDITION, IMPORTED OR AMENDED EXISTING SOILS SHALL BE MIXED WITH SUBGRADE SOILS WHERE THEY MEET IN ORDER TO CREATE A TRANSITIONAL GRADIENT TO ALLOW FOR PROPER DRAINAGE.



PLANTING SOIL WITHIN AREAS OF CUT OR RAISED GRADE

NOTES:

- CONTRACTOR IS RESPONSIBLE TO SEND SAMPLES OF EXISTING SOILS INTENDED FOR USE IN PLANTING AREAS (1 PER 500 CY.) TO TESTING LABORATORY OR UNIVERSITY COOPERATIVE EXTENSION FOR TESTING. ALL TESTING COSTS ARE AT THE CONTRACTOR'S EXPENSE.
- RECYCLED CRUSHED CONCRETE AND ASPHALT MILLINGS SHALL NOT BE PLACED WITHIN 2'–6" OF FINISH GRADE IN PROPOSED LANDSCAPE AREAS.
- IMPORTED FILL SHALL CONTAIN NO CONTAMINATION IN EXCEEDENCE OF THE APPLICABLE STATE ENVIRONMENTAL STANDARDS AND MEET THE ENVIRONMENTAL REQUIREMENTS FOR THE PROJECT. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION OF COMPLIANCE PRIOR TO DELIVERY OF ANY FILL TO THE SITE.
- CONTRACTOR TO LIGHTLY COMPACT ALL PLACED PLANTING SOILS AND RAISE GRADES ACCORDINGLY TO ALLOW FOR FUTURE SETTLEMENT OF PLANTING SOILS (TYP.)
- NO STONES, WOOD CHIPS, OR DEBRIS LARGER THAN 1/2" SHALL BE ACCEPTABLE WITHIN PLANTING AREAS.

## PLANTING SOIL

NTS

### LP501 PLANTING DETAILS

Site Development Plan For:  
**LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH**

Assessors Map 107 & Lot 11154

Prepared by: **Langan Engineering & Environmental Services, LLC**  
**100 Cambridge Street, Suite 1310  
Boston, MA 02114**

Scale: As noted

October 30, 2025



|                        |                                                                                    |                               |
|------------------------|------------------------------------------------------------------------------------|-------------------------------|
| Owner:                 | <b>SALEM NIM, LLC<br/>C/O LYONS GROUP<br/>355 CONGRESS ST<br/>BOSTON, MA 02210</b> | Salem Planning Board Approval |
| Zoning Classification: | <b>COMMERCIAL-INDUSTRIAL DISTRICT,<br/>SUBDISTRICT C (C-1C)</b>                    |                               |



27 August 2025, 18 November 2025  
Revised 29 December 2025

Jacob LaFontaine  
Planning Director  
Salem Planning Division  
33 Geremonty Drive  
Salem, NH 03079

Re: Traffic & Parking Utilization Study  
Proposed Reuse of Former Sears Auto Center  
The Mall at Rockingham Park  
Salem, NH  
Langan Project No.: 151064501

Dear Mr. LaFontaine:

Langan Engineering and Environmental Services, Inc. (Langan) has prepared this traffic and parking assessment for the proposed reuse of the former Sears Auto Center at the Mall at Rockingham Park ("the Mall") located at 1 Mall Road in Salem, New Hampshire. The Lyons Group proposes to convert the former auto center into a "Game On!" adult entertainment center and a "Loretta's Last Call", a country music themed restaurant, bar and dancing venue.

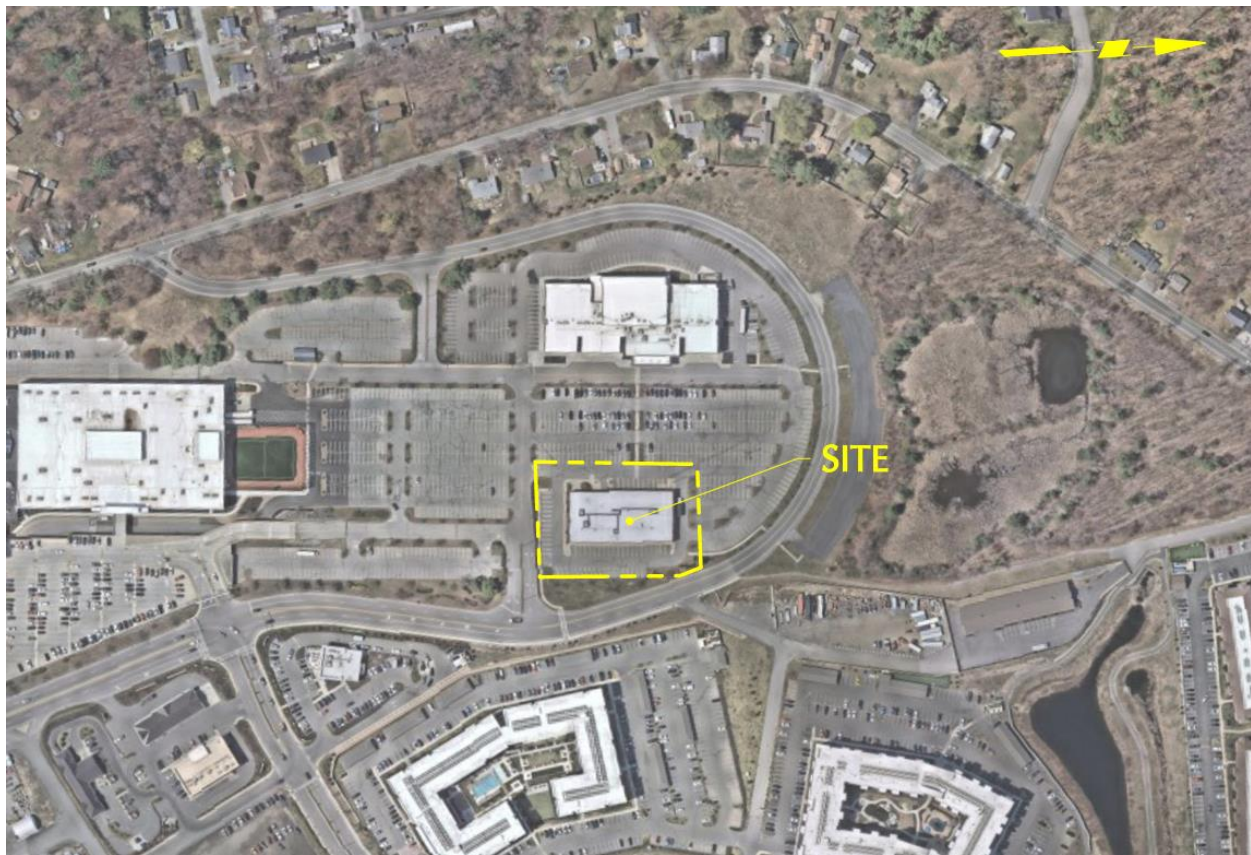
The analysis presented herein shows a reduction in expected traffic volumes when compared to the previously approved development plan. There is also ample parking capacity in the north lots to accommodate the planned development. Parking observations are consistent with those previously reported in the 2017 *Parking Utilization Study for the Redevelopment/Expansion of The Mall at Rockingham Park*<sup>1</sup> and the 2023 *Parking Utilization Study for the Dick's House of Sport at The Mall at Rockingham Park*<sup>2</sup>. This letter report presents the parking demand for the surface lots located at the north end of the mall and the anticipated changes in traffic compared to previous approvals.

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<sup>1</sup> Parking Utilization Study for Redevelopment/Expansion of The Mall at Rockingham Park, Rockingham Park Boulevard, Salem, New Hampshire, Langan Engineering and Environmental Services, May 2017

<sup>2</sup> Parking Utilization Study for Dick's House of Sport at The Mall at Rockingham Park, Rockingham Park Boulevard, Salem, New Hampshire, Langan Engineering and Environmental Services, May 2023, Revised July 2023





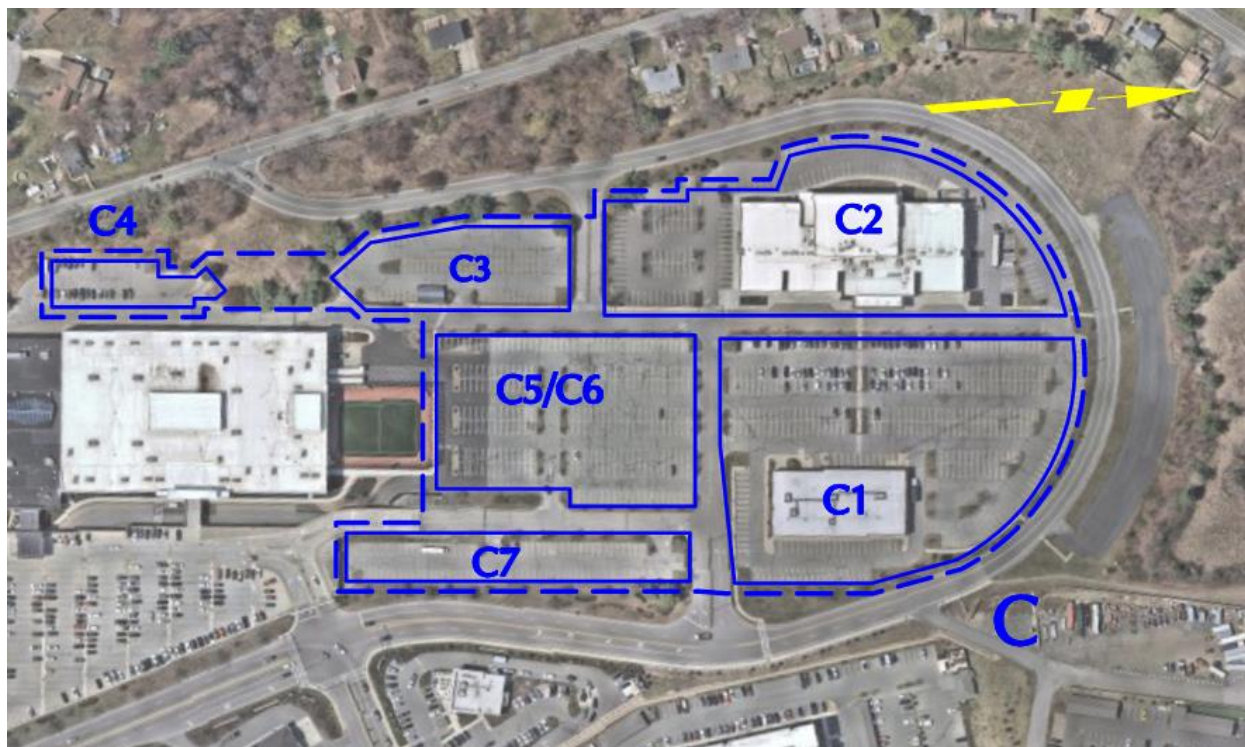
**Figure 1: Site Aerial Photograph**

## PROJECT DESCRIPTION

Lyons Group plans to repurpose and renovate the approximately 19,600 square foot former Sears Auto Center in the north parking lot the Mall to include a  $\pm 14,158$  SF, 143 seat "Game On!" adult entertainment center and a  $\pm 5,343$  SF, 194 seat (that includes 59 seasonal outdoor patio seats) "Loretta's Last Call" restaurant/bar/dance venue. This space was previously approved as 19,600 SF of restaurant and retail space as part of the Cinemark movie theater project in 2018. The proposed renovation of the Lyons Group includes full interior remodeling and exterior improvements such as landscaping and seasonal outdoor seating. The  $\pm 1.67$ -acre lot presently includes 84 parking spaces. The proposed site plan eliminates 16 parking spaces in lot C (See Figure 2) to accommodate the restaurant's entryway and patio seating, which would leave 1,070 parking spaces in lot C.

"Game On!" is expected to operate seven days a week and open from 11:30 AM to 2:00 AM on weekends and 11:30 AM to 12:00 AM on weekdays. "Loretta's Last Call" is expected to operate seven days a week and open from 4:00 PM to 2:00 AM on weekends and 4:00 PM to 1:00 AM on weekdays. "Loretta's" will likely open for brunch on Saturdays and Sundays from 10:30 AM to 4:00 PM. The project is expected to be completed by summer/fall 2026.

The overall mall provides approximately 4,563 spaces, which includes the proposed parking lot renovations. An additional 500 spaces are available during peak shopping periods under an easement with Tuscan Village bringing the overall total to 5,063 during peak parking demand periods.



**Figure 2: Parking Utilization Study Area**

## BACKGROUND

Langan previously prepared an overall parking utilization study entitled *Parking Utilization Study for Redevelopment/Expansion of The Mall at Rockingham Park* during the 2016 Christmas shopping season. The study evaluated the feasibility of adding retail space at the mall and collected parking counts for the entire mall during the peak holiday season on Black Friday, the busiest shopping day of the year, two Saturdays – one in early December and one in mid-January<sup>3</sup>. The study found that the mall has sufficient parking spaces to serve the parking demand with the proposed redevelopment with exception of Black Friday, a once-a-year event, when parking was at capacity. Table 2 from the original parking study showing both peak and non-peak parking supply and demand are provided in the Appendix. The addition of the cinema and retail uses resulted in the issuance of a Conditional Use Permit decreasing the required Mall parking from 5,123 spaces to 4,645 spaces.

<sup>3</sup> Parking utilization was recorded on Friday, November 25, (Black Friday), Saturday, December 10, 2016 and Saturday January 14, 2017

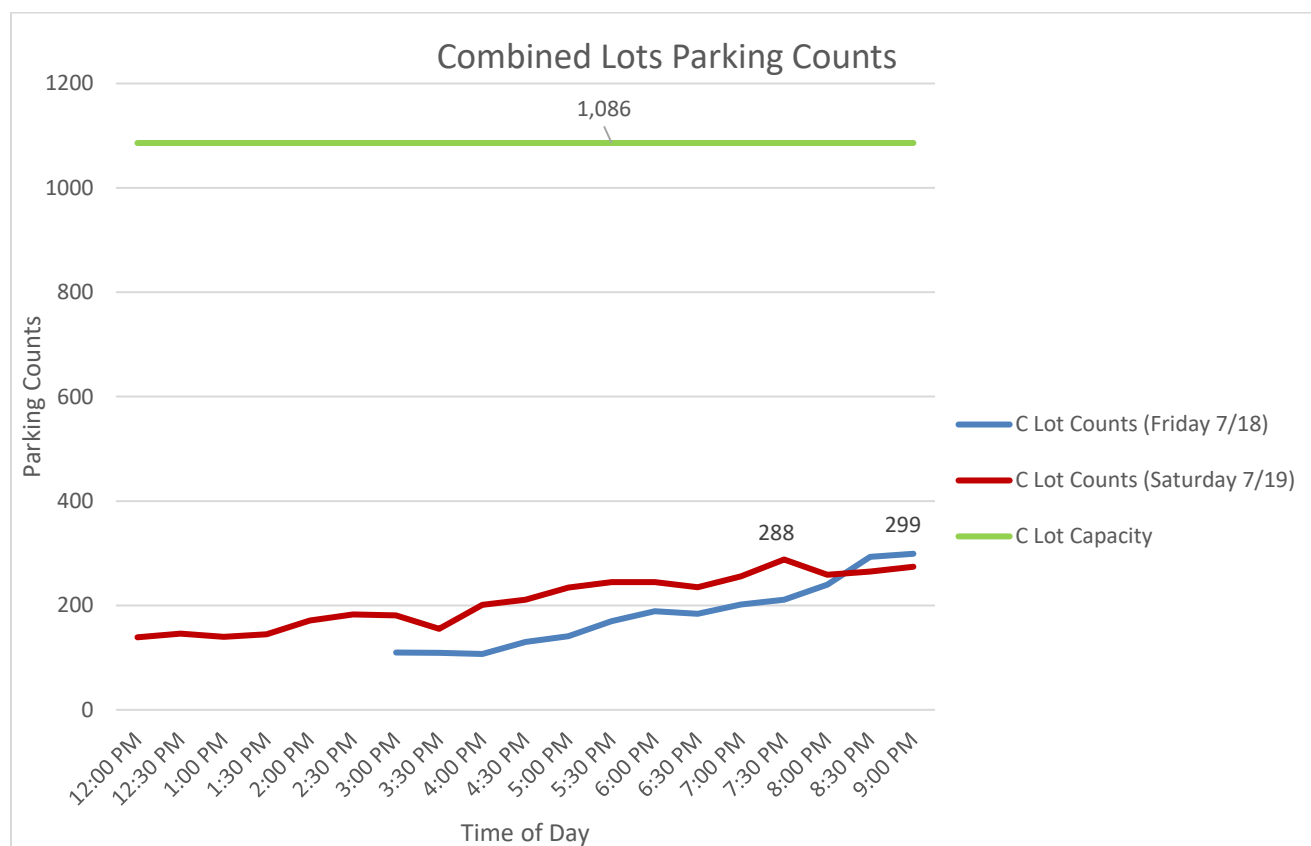
In 2023, Langan updated the 2017 study finding for Dick's Sporting Goods. Dick's renovated the former Sears store at the north end of the Mall and added an outdoor demonstration field. The addition of the field resulted in the loss of 58 spaces in Parking Lot C of the Mall, bringing the Lot C parking to 1,086. A Conditional Use Permit was granted by the Salem Planning Board further reducing the required mall parking from 4,645 to 4,587 overall spaces.

In 2024, Langan updated the 2017 and 2023 study findings for the Live! charitable gaming facility that was approved for the former Lord & Taylor store at the southwest end of the Mall. These renovations reduced the parking count by 8 spaces in Parking Lot A to add accessible spaces, bringing the Lot C parking to 1,486. A Conditional Use Permit was granted by the Salem Planning Board further reducing the required mall parking from 4,587 to 4,579.

## **PARKING UTILIZATION STUDY**

We performed manual parking counts every 30 minutes on Friday, July 18<sup>th</sup> between 3:00 PM and 9:00 PM and Saturday July 19<sup>th</sup> between 12:00 PM and 9:00 PM to document parking utilization. We performed parking counts in the northern C lots as the parking demand associated with the reuse will be isolated almost exclusively there. The C lots presently contain 1,086 striped spaces broken down into seven separate lots, C1 through C7, as shown in Figure 2. Demand in each of the sub lots was recorded separately. The raw parking counts for the two days surveyed are included in Appendix A.

Based on the data collected we found an average utilization rate of 17 percent on Friday July 18<sup>th</sup> and 19 percent on Saturday July 19<sup>th</sup>. The highest peak utilization on Friday occurred at 9:00 PM with a 28 percent occupancy (299 parked vehicles) with the final count of the evening. The highest peak on the Saturday counts occurred between 7:30 – 8:00 PM with a 27 percent occupancy (288 parked vehicles). Figure 3 below shows the parking utilization by time of day and compared that to the existing parking supply in the C lots.



**Figure 3: Parking Utilization by Time of Day**

### Anticipated Parking Demand

The 2017 study was conducted to demonstrate the feasibility of additional development at the north end of the mall that consisted of the Cinemark movie theater, 15,000 SF of restaurants and 4,500 SF of generic retail space. Only one of the proposed uses outlined in that study, the movie theatre, has been constructed since the parking study was submitted to the Town in 2017. Since then, Dick's House of Sport has occupied the Sears anchor space and constructed an outdoor turf field in lot C5.

The remaining development projected in the 2017 study was a mix of restaurant/retail in the former Sears Auto Center. The ITE *Parking Generation Manual*<sup>4</sup>, 6<sup>th</sup> Edition was used to estimate the proposed renovation's peak parking demand. Under the renovation as part of this current application, this building will be entirely a restaurant/entertainment use. Lyons Group proposed two restaurants within the existing structure, "Game On!" and "Loretta's Last Call". "Game On!" contains 10 bowling lanes, pool tables, arcade space and a bar area. "Loretta's Last Call" is a western-themed restaurant and bar with a dance floor.

<sup>4</sup> Parking Generation Manual, 6<sup>th</sup> Edition published by the Institution of Transportation Engineers (ITE)



## ITE Land Use Code

Choosing a representative ITE land use code (LUC) to predict parking and traffic demand included consideration of a few land uses in that data base. There are codes for uses such as bowling alley and bar/tavern uses (LUC 437 and LUC 975, respectively). Data points are limited in both cases where daily trip estimates or parking demand are not available. Bowling alley as a use also generated parking demand that is significantly lower than expected for this type of mixed use facility. Land Use Code (LUC) 932 – High-Turnover (Sit-Down) Restaurant was found to be a use that could capture parking demand for both “Game On!” and “Loretta’s Last Call,” as both developments will serve food and have table seating for patrons. Seats were chosen to be the most appropriate independent variable for LUC 932, as “Game On!” has a large floor area space dedicated to bowling lanes, which is likely not typical of the standard High-Turnover (Sit-Down) Restaurant use. As the space for bowling lanes are not occupiable, using square footage as an independent variable will produce a significantly inflated parking demand. Additionally, estimating parking demand for “Loretta’s Last Call” resulted in lower anticipated parking demand compared to seats. A full land use description, data graphs, and a table with anticipated average and peak parking demand for “Loretta’s Last Call” using square footage as the independent variable are included in the appendix.

## Traffic & Parking Generation

The parking demand generation for the proposed renovations was estimated using parking generation data contained in the ITE *Parking Generation Manual*, 6<sup>th</sup> Edition <sup>5</sup>. **Table 1** below shows the resulting average parking demand, and **Table 2** shows the peak parking demand estimates for the proposed restaurants.

| TABLE 1<br>ANTICIPATED AVERAGE PARKING DEMAND - SALEM NH LYONS |                            |                                   |                                    |                                   |                                    |                                   |                                    |
|----------------------------------------------------------------|----------------------------|-----------------------------------|------------------------------------|-----------------------------------|------------------------------------|-----------------------------------|------------------------------------|
| USE                                                            | LAND USE CODE <sup>1</sup> | WEEKDAY (MON - THURS)             |                                    | FRIDAY                            |                                    | SATURDAY                          |                                    |
|                                                                |                            | Average Parking Rate <sup>2</sup> | Anticipated Average Parking Demand | Average Parking Rate <sup>2</sup> | Anticipated Average Parking Demand | Average Parking Rate <sup>2</sup> | Anticipated Average Parking Demand |
| Loretta's Last Call (194 Seats)                                | 932                        | 0.28 spaces/seat                  | 54                                 | 0.47 spaces/seat                  | 91                                 | 0.38 spaces/seat                  | 74                                 |
| Game On! (143 Seats)                                           | 932                        | 0.28 spaces/seat                  | 40                                 | 0.47 spaces/seat                  | 67                                 | 0.38 spaces/seat                  | 54                                 |
| <b>Total Average Parking Demand</b>                            |                            | <b>0.28 spaces/seat</b>           | <b>94</b>                          | <b>0.47 spaces/seat</b>           | <b>158</b>                         | <b>0.38 spaces/seat</b>           | <b>128</b>                         |

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

<sup>5</sup> Parking Generation Manual, 6<sup>th</sup> Edition published by the Institution of Transportation Engineers (ITE).

**TABLE 2**  
**ANTICIPATED PEAK (85TH PERCENTILE) PARKING DEMAND - SALEM NH LYONS**

| USE                                 | LAND USE CODE <sup>1</sup> | WEEKDAY (MON - THURS)          |                                 | FRIDAY                         |                                 | SATURDAY                       |                                 |
|-------------------------------------|----------------------------|--------------------------------|---------------------------------|--------------------------------|---------------------------------|--------------------------------|---------------------------------|
|                                     |                            | Peak Parking Rate <sup>2</sup> | Anticipated Peak Parking Demand | Peak Parking Rate <sup>2</sup> | Anticipated Peak Parking Demand | Peak Parking Rate <sup>2</sup> | Anticipated Peak Parking Demand |
| Loretta's Last Call (194 Seats)     | 932                        | 0.48 spaces/seat               | 93                              | 0.79 spaces/seat               | 153                             | 0.56 spaces/seat               | 109                             |
| Game On! (143 Seats)                | 932                        | 0.48 spaces/seat               | 69                              | 0.79 spaces/seat               | 113                             | 0.56 spaces/seat               | 80                              |
| <b>Total Average Parking Demand</b> |                            | <b>0.48 spaces/seat</b>        | <b>162</b>                      | <b>0.79 spaces/seat</b>        | <b>266</b>                      | <b>0.56 spaces/seat</b>        | <b>189</b>                      |

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

The proposed renovations are expected to generate an average demand for 158 parking spaces on a Friday and 128 parking spaces on a Saturday, and a peak demand for 266 parking spaces on a Friday and 189 spaces on a Saturday during the off-peak season in the C parking section. The Parking Generation Manual does not provide a peak season/off-peak season parking generation differential for LUC 932.

This analysis simply overlays the estimated parking demand using ITE Parking Generation Manual with parking demands measured at the in the field at the Mall at Rockingham Park. We did not take into account shared usage with the traditional retail components of the mall. Demand at Cinemark and the proposed restaurants is lowest when the rest of the mall is most active and peaks later in the evening when retail traffic in the mall is diminishing. This combined with the fact that the existing parking utilization plus peak future restaurant parking demand in Lot C would result in 53% utilization in Lot C during off-peak seasons suggest that the results of this analysis are conservative and general day to day operations will be less impactful.

The trip generation for the proposed renovations was estimated using trip generation data contained in the ITE *Trip Generation Manual*, 11<sup>th</sup> Edition <sup>6</sup>. **Table 3** below shows the resulting trip generation estimates for the proposed restaurants using seats as the independent variable. The proposed renovation estimates 132 evening peak hour trips and 1,473 weekday daily trips.

<sup>6</sup> Trip Generation Manual, 11<sup>th</sup> Edition published by the Institution of Transportation Engineers (ITE).

**TABLE 3**  
**ANTICIPATED TRIP GENERATION - SALEM NH LYONS**

| USE                                                     | LAND<br>USE<br>CODE <sup>1</sup> | PM PEAK HOUR |           |            | WEEKDAY<br>DAILY<br>TOTAL | SAT PEAK HOUR |            |            | SATURDAY<br>DAILY<br>TOTAL |
|---------------------------------------------------------|----------------------------------|--------------|-----------|------------|---------------------------|---------------|------------|------------|----------------------------|
|                                                         |                                  | ENTER        | EXIT      | TOTAL      |                           | ENTER         | EXIT       | TOTAL      |                            |
| Loretta's Last Call<br>(194 Seats)                      | 932                              | 43           | 33        | 76         | 848                       | 54            | 49         | 103        | 1086                       |
| Game On! (143 Seats)                                    | 932                              | 32           | 24        | 56         | 625                       | 40            | 36         | 76         | 801                        |
| <b>Total New Trips</b>                                  |                                  | <b>75</b>    | <b>57</b> | <b>132</b> | <b>1473</b>               | <b>94</b>     | <b>85</b>  | <b>179</b> | <b>1887</b>                |
| Previously Approved<br>Restaurant/Retail<br>(19,600 SF) | -                                | 94           | 64        | 158        | 2022                      | 121           | 106        | 227        | 2508                       |
| <b>Net Change in Trips</b>                              |                                  | <b>-19</b>   | <b>-7</b> | <b>-26</b> | <b>-549</b>               | <b>-27</b>    | <b>-21</b> | <b>-48</b> | <b>-621</b>                |

<sup>1</sup> Land Use Codes based on ITE Trip Generation Manual 11th Edition

<sup>2</sup> Volume based on ITE Trip Generation Manual 11th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

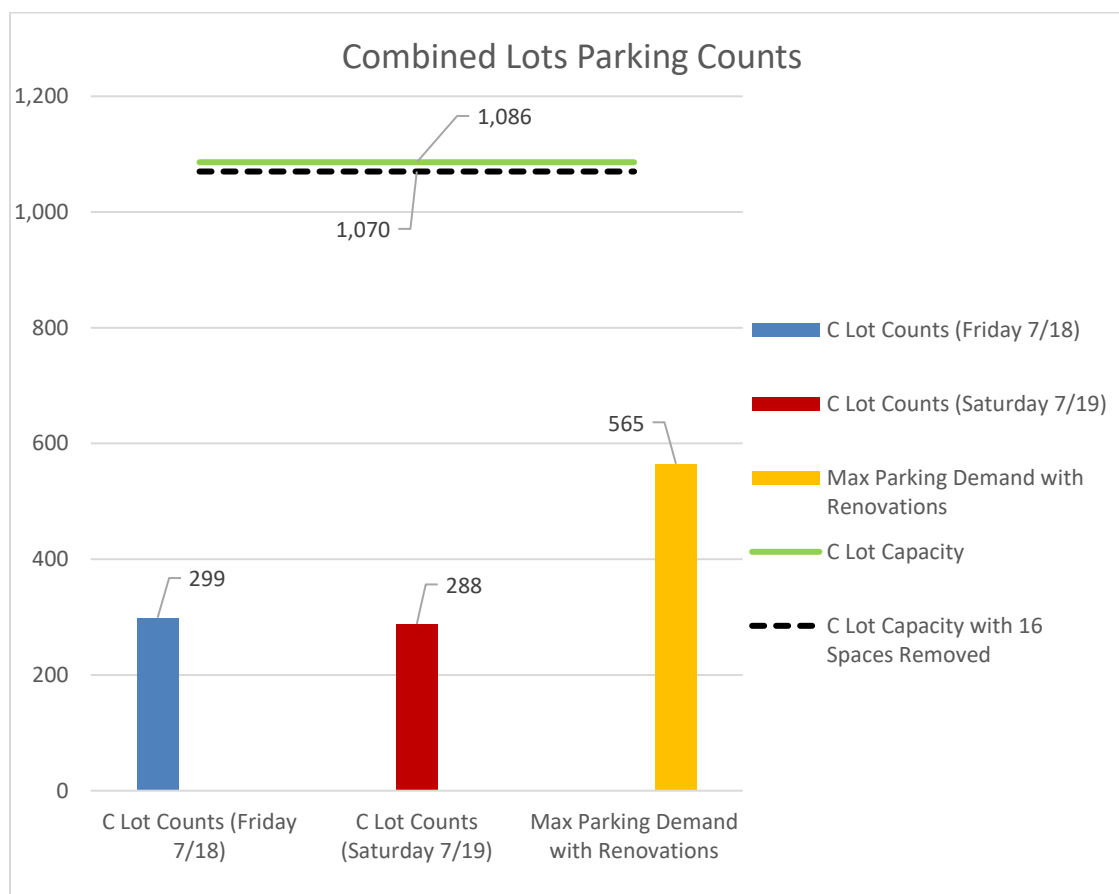
<sup>3</sup> Volume based on report titled "Traffic Impact Study - Redevelopment/Expansion of the Mall at Rockingham Park" by Langan dated June 2017.

The previously approved trip generation for this site (former Sears Auto Center) is also shown in **Table 3**. The proposed renovation proposes a reduction in 26 weekday and 48 Saturday evening peak hour trips, and 549 weekday and 621 Saturday daily trips than the previous approvals.

Similar to the parking demand generation, a trip generation estimate was created using square footage as the independent variable for "Loretta's Last Call," and it was found that this generates less traffic than using seats as the independent variable. A table with anticipated trip generation for "Loretta's Last Call" using square footage as the independent variable is included in the appendix.

## Peak Parking Demand

Figure 4 shows the 2025 observed parking counts with the addition of the max peak daily parking demand (266) for the renovated restaurants.



**Figure 4: Parking Utilization by Time of Day with Renovations Added**

As shown in Figure 4, there will be a surplus of 505 parking spaces during the off-peak season in parking lot C. Therefore, with the addition of the proposed restaurants and the existing parking demand, parking lot C can easily accommodate the new development. The graph also clearly shows that the parking demand for the uses in parking lot C is complimentary to each other and to the traditional retail uses in the mall. Demand at Cinemark and the proposed restaurants is lowest when the rest of the mall is most active and peaks later in the evening when retail traffic in the mall is diminishing.

Parking demand at the Mall at Rockingham Park increases during the holiday season in late November and December. The 2023 Dick's parking study data was collected in Lot C on Black Friday and the following Saturday, both considered to be peak dates for the Mall. Black Friday peak parking was 645 occupied spaces and Saturday peaked with 175 occupied spaces. The peak Lot C parking demand with the proposed restaurant renovations on a Friday during the holiday



season is expected to be 911 occupied spaces out of 1,070 available<sup>7</sup>. This peak parking demand assumes that the peak retail parking utilization overlaps with the peak restaurant utilization, which is unlikely to coincide. This data indicates that during the holiday season, a considerable number of vacant spaces will be available in the overall mall parking supply.

#### Additional Parking Capacity

The Mall at Rockingham Park has an easement for an additional 500 spaces available during peak shopping periods at the Tuscan Village. The analysis presented herein suggest that implementation of this additional capacity is not necessary, as the Mall will be available to increase the capacity of the overall mall parking supply if needed.

### **CONDITIONAL USE PERMIT**

The proponent will require an amended conditional use permit (CUP) for the overall mall parking requirement to account for the loss of 16 spaces around the former Sears Auto Center. The overall parking requirement under zoning for the Mall as per the restaurant renovations site plan submittal is 5,573 spaces. The estimated parking requirement under zoning in the November 2024 CUP was 5,123 spaces. The CUP must be amended to request a total 1,010 space reduction consisting of 478 (Cinemark), 58 (Dick's), 458 (Casino), and 16 (Lyons Group) for the proposed restaurant renovations. The total required parking will be reduced to 4,563 parking spaces. The analysis presented herein demonstrates that this small reduction in the overall parking count is inconsequential given the significant surplus in parking spaces in the north lots.

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<sup>7</sup> The addition of the restaurant's Friday parking demand (158) to Black Friday's peak peaking demand (645)

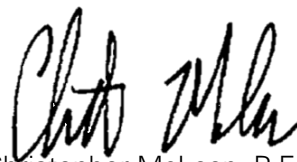
## CONCLUSION

Langan evaluated the parking impact and trip generation of the proposed renovations of the former Sears Auto Center to a restaurant use at the Mall at Rockingham Park. We combined field observations of parking in Area C adjacent to Cinemark and Dick's with ITE Parking Generation estimates. Key findings discussed herein are:

- Existing parking demand in Parking Lot C peaks at 28% utilization on Friday and 27% on Saturday in the evenings.
- The combination of existing parking utilization and anticipated parking demand would result in a 53% utilization rate of Parking Lot C during off-peak season.
- The Mall will require an amended Conditional Use Permit (CUP) to reduce the required parking to 4,563 to account for the removal of 16 parking spaces for the proposed restaurants.
- The estimated trip generation for the proposed restaurants is 26 less evening peak hour trips and 549 daily trips less than the previous approvals.

In conclusion, our analysis indicated that the proposed renovation/reuse of the former Sears Auto Center building by proposed restaurants can be accommodated under typical operating conditions based on the estimated parking requirement.

Sincerely,  
**Langan Engineering and Environmental Services, Inc.**



Christopher McLean, P.E.  
Project Engineer



Maximo Polanco, P.E.  
Senior Project Engineer

Appendix A - Parking Count Data

In-House

ROCKINGHAM PARK

PARKING COUNTS

FRIDAY 7/18/2025

| Time     | C1  | C2  | C3  | C4 | C5  | C6 | C7 | Total | Utilization |
|----------|-----|-----|-----|----|-----|----|----|-------|-------------|
| 3:00 PM  | 73  | 19  | 1   | 17 | 0   | 0  | 0  | 110   | 10%         |
| 3:30 PM  | 73  | 18  | 1   | 17 | 0   | 0  | 0  | 109   | 10%         |
| 4:00 PM  | 70  | 17  | 1   | 17 | 0   | 2  | 0  | 107   | 10%         |
| 4:30 PM  | 87  | 24  | 1   | 17 | 0   | 1  | 0  | 130   | 12%         |
| 5:00 PM  | 92  | 30  | 1   | 17 | 0   | 1  | 0  | 141   | 13%         |
| 5:30 PM  | 118 | 31  | 2   | 17 | 0   | 2  | 0  | 170   | 16%         |
| 6:00 PM  | 133 | 35  | 1   | 17 | 0   | 3  | 0  | 189   | 17%         |
| 6:30 PM  | 124 | 35  | 3   | 17 | 0   | 5  | 0  | 184   | 17%         |
| 7:00 PM  | 142 | 35  | 3   | 17 | 0   | 5  | 0  | 202   | 19%         |
| 7:30 PM  | 152 | 33  | 2   | 17 | 0   | 6  | 1  | 211   | 19%         |
| 8:00 PM  | 177 | 33  | 2   | 17 | 0   | 11 | 0  | 240   | 22%         |
| 8:30 PM  | 222 | 38  | 1   | 17 | 0   | 15 | 0  | 293   | 27%         |
| 9:00 PM  | 228 | 37  | 1   | 17 | 0   | 16 | 0  | 299   | 28%         |
| Capacity | 389 | 191 | 106 | 43 | 259 |    | 98 | 1086  |             |
| Average  | 130 | 30  | 2   | 17 | 5   |    | 0  | 184   | 17%         |

NDS

ROCKINGHAM PARK

PARKING COUNTS

SATURDAY 7/19/2025

| Time     | C1  | C2  | C3  | C4 | C5  | C6 | C7 | Total | Utilization |
|----------|-----|-----|-----|----|-----|----|----|-------|-------------|
| 12:00 PM | 95  | 22  | 0   | 17 | 3   |    | 2  | 139   | 13%         |
| 12:30 PM | 98  | 22  | 4   | 17 | 3   |    | 2  | 146   | 13%         |
| 1:00 PM  | 89  | 27  | 0   | 17 | 6   |    | 1  | 140   | 13%         |
| 1:30 PM  | 92  | 28  | 0   | 17 | 7   |    | 1  | 145   | 13%         |
| 2:00 PM  | 117 | 30  | 0   | 17 | 6   |    | 1  | 171   | 16%         |
| 2:30 PM  | 121 | 34  | 1   | 17 | 9   |    | 1  | 183   | 17%         |
| 3:00 PM  | 118 | 34  | 1   | 17 | 8   |    | 3  | 181   | 17%         |
| 3:30 PM  | 109 | 25  | 0   | 17 | 3   |    | 1  | 155   | 14%         |
| 4:00 PM  | 154 | 26  | 0   | 17 | 3   |    | 1  | 201   | 19%         |
| 4:30 PM  | 158 | 27  | 0   | 17 | 7   |    | 2  | 211   | 19%         |
| 5:00 PM  | 173 | 30  | 1   | 17 | 12  |    | 1  | 234   | 22%         |
| 5:30 PM  | 183 | 32  | 2   | 17 | 10  |    | 1  | 245   | 23%         |
| 6:00 PM  | 180 | 36  | 0   | 17 | 11  |    | 1  | 245   | 23%         |
| 6:30 PM  | 171 | 33  | 1   | 17 | 12  |    | 1  | 235   | 22%         |
| 7:00 PM  | 191 | 28  | 1   | 17 | 18  |    | 1  | 256   | 24%         |
| 7:30 PM  | 230 | 23  | 1   | 17 | 16  |    | 1  | 288   | 27%         |
| 8:00 PM  | 208 | 22  | 0   | 17 | 11  |    | 1  | 259   | 24%         |
| 8:30 PM  | 218 | 14  | 0   | 17 | 15  |    | 1  | 265   | 24%         |
| 9:00 PM  | 225 | 17  | 0   | 17 | 14  |    | 1  | 274   | 25%         |
| Capacity | 389 | 191 | 106 | 43 | 259 |    | 98 | 1086  |             |
| Average  | 154 | 27  | 1   | 17 | 9   |    | 1  | 209   | 19%         |

# Land Use: 932 High-Turnover (Sit-Down) Restaurant

## Description

A high-turnover (sit-down) restaurant is full-service eating establishment with a typical duration of stay of 60 minutes or less. This type of restaurant is usually moderately priced, frequently belongs to a restaurant chain, and is commonly referred to as casual dining. Generally, these restaurants serve lunch and dinner; they may also be open for breakfast and are sometimes open 24 hours a day. These restaurants typically do not accept reservations. A patron commonly waits to be seated, is served by wait staff, orders from a menu, and pays after the meal. Some facilities offer carry-out for a small proportion of their customers. Some facilities within this land use may also contain a bar area for serving food and alcoholic drinks.

## Land Use Subcategory

Data are separated into two subcategories for this land use:

- Restaurants that serve breakfast
- Restaurants that do not serve breakfast

The “serves breakfast” subcategory includes restaurants that serve customers during breakfast, lunch, and dinner; during breakfast and lunch; and during breakfast only. The “does not serve breakfast” subcategory includes restaurants that serve customers during lunch and dinner, during dinner only, and during lunch only.

## Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a weekday (Monday–Thursday) at restaurants that serve breakfast, lunch, and dinner (10 study sites) and at restaurants that serve only lunch and dinner (25 sites). The following table also presents a time-of-day distribution of parking demand on a Saturday at restaurants that serve breakfast, lunch, and dinner (nine study sites) and at restaurants that serve only lunch and dinner (six sites).

| Hour Beginning  | Percent of Monday–Thursday Peak Parking Demand |                          | Percent of Saturday Peak Parking Demand |                          |
|-----------------|------------------------------------------------|--------------------------|-----------------------------------------|--------------------------|
|                 | Serving Breakfast, Lunch, and Dinner           | Serving Lunch and Dinner | Serving Breakfast, Lunch, and Dinner    | Serving Lunch and Dinner |
| 12:00–4:00 a.m. | –                                              | –                        | –                                       | –                        |
| 5:00 a.m.       | –                                              | –                        | –                                       | –                        |
| 6:00 a.m.       | –                                              | –                        | –                                       | –                        |
| 7:00 a.m.       | –                                              | –                        | –                                       | –                        |
| 8:00 a.m.       | 64                                             | –                        | 55                                      | –                        |
| 9:00 a.m.       | 74                                             | –                        | 76                                      | –                        |
| 10:00 a.m.      | 82                                             | –                        | 91                                      | –                        |
| 11:00 a.m.      | 89                                             | 28                       | 100                                     | 33                       |
| 12:00 p.m.      | 100                                            | 96                       | 97                                      | 56                       |
| 1:00 p.m.       | 86                                             | 100                      | 91                                      | 69                       |
| 2:00 p.m.       | 57                                             | 51                       | 73                                      | 58                       |
| 3:00 p.m.       | 44                                             | 37                       | 51                                      | 49                       |
| 4:00 p.m.       | 39                                             | 34                       | 43                                      | 63                       |
| 5:00 p.m.       | 62                                             | 56                       | 57                                      | 77                       |
| 6:00 p.m.       | 73                                             | 87                       | 66                                      | 100                      |
| 7:00 p.m.       | 95                                             | 91                       | 80                                      | 100                      |
| 8:00 p.m.       | 76                                             | 73                       | 62                                      | 85                       |
| 9:00 p.m.       | –                                              | –                        | –                                       | 55                       |
| 10:00 p.m.      | –                                              | –                        | –                                       | 35                       |
| 11:00 p.m.      | –                                              | –                        | –                                       | –                        |

## **Additional Data**

If the restaurant has outdoor seating, its area is not included in the overall gross floor area. For a restaurant that has significant outdoor seating, the number of seats may have a more direct relationship to site-generated parking demand than GFA.

The average parking supply ratio for the 55 study sites with parking supply information in a general urban/suburban setting is 16 spaces per 1,000 square feet GFA. The average parking supply ratio for the five study sites with parking supply information in a dense multi-use urban setting is 11 spaces per 1,000 square feet GFA. The average peak parking occupancy is 74 percent at the general urban/suburban sites and 88 percent at the dense multi-use urban sites.

The sites were surveyed in the 1990s, the 2000s, the 2010s, and the 2020s in Arizona, British Columbia (CAN), California, Florida, Illinois, Indiana, Maine, Maryland, Massachusetts, Minnesota, New Jersey, New York, North Carolina, Oregon, Pennsylvania, Texas, Virginia, Washington, and Wisconsin.

## **Source Numbers**

168, 218, 274, 276, 299, 527, 531, 556, 557, 567, 568, 618, 619, 620, 622, 626, 628, 637



# High-Turnover (Sit Down) Restaurant Does Not Serve Breakfast (932)

Peak Period Parking Demand vs: Seats

On a: Weekday (Monday - Thursday)

Setting/Location: General Urban/Suburban

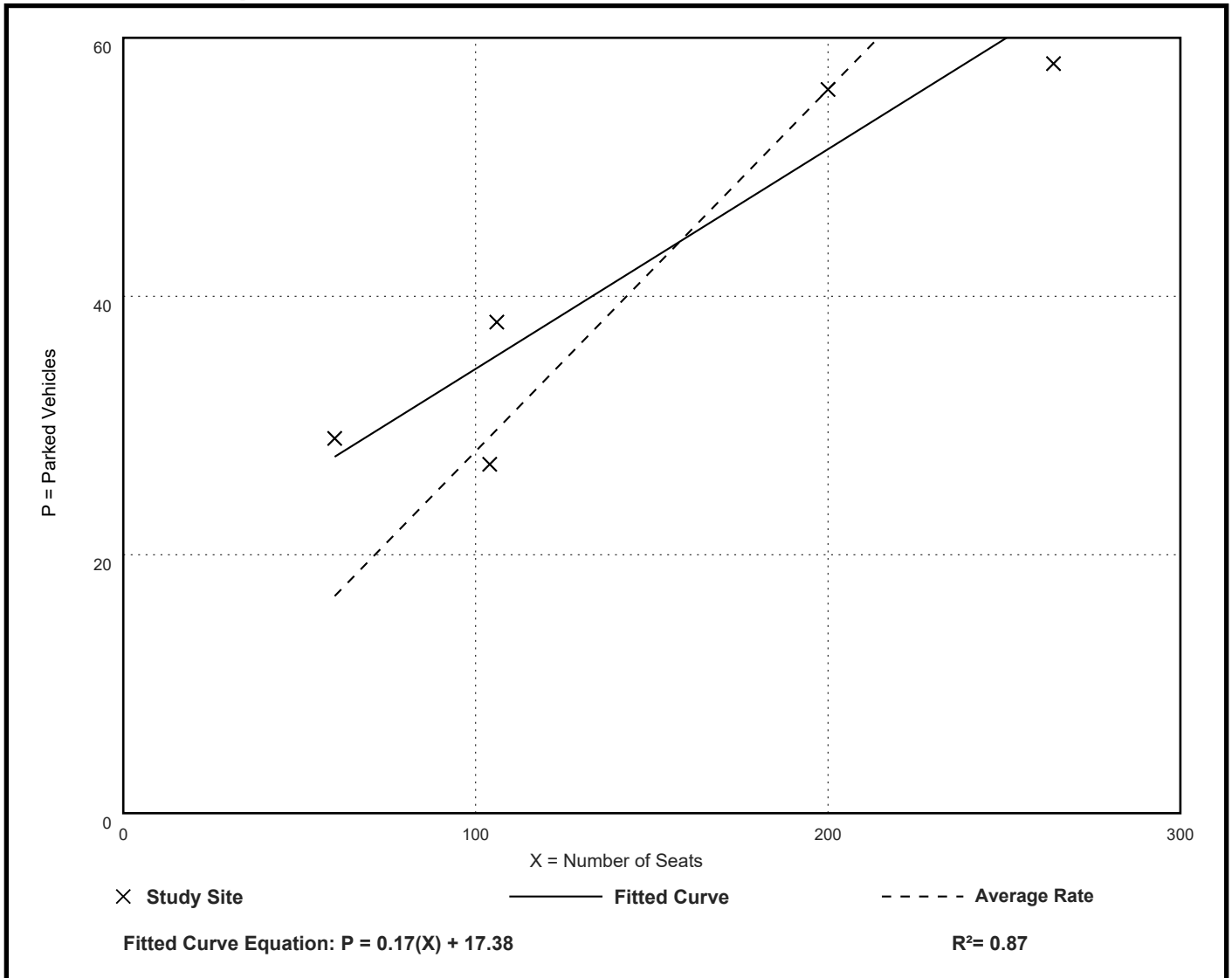
Number of Studies: 5

Avg. Num. of Seats: 147

## Peak Period Parking Demand per Seat

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|------------------------------------------|
| 0.28         | 0.22 - 0.48    | 0.26 / 0.48            | ***                     | 0.08 ( 29% )                             |

## Data Plot and Equation



# High-Turnover (Sit Down) Restaurant Does Not Serve Breakfast (932)

Peak Period Parking Demand vs: Seats

On a: Friday

Setting/Location: General Urban/Suburban

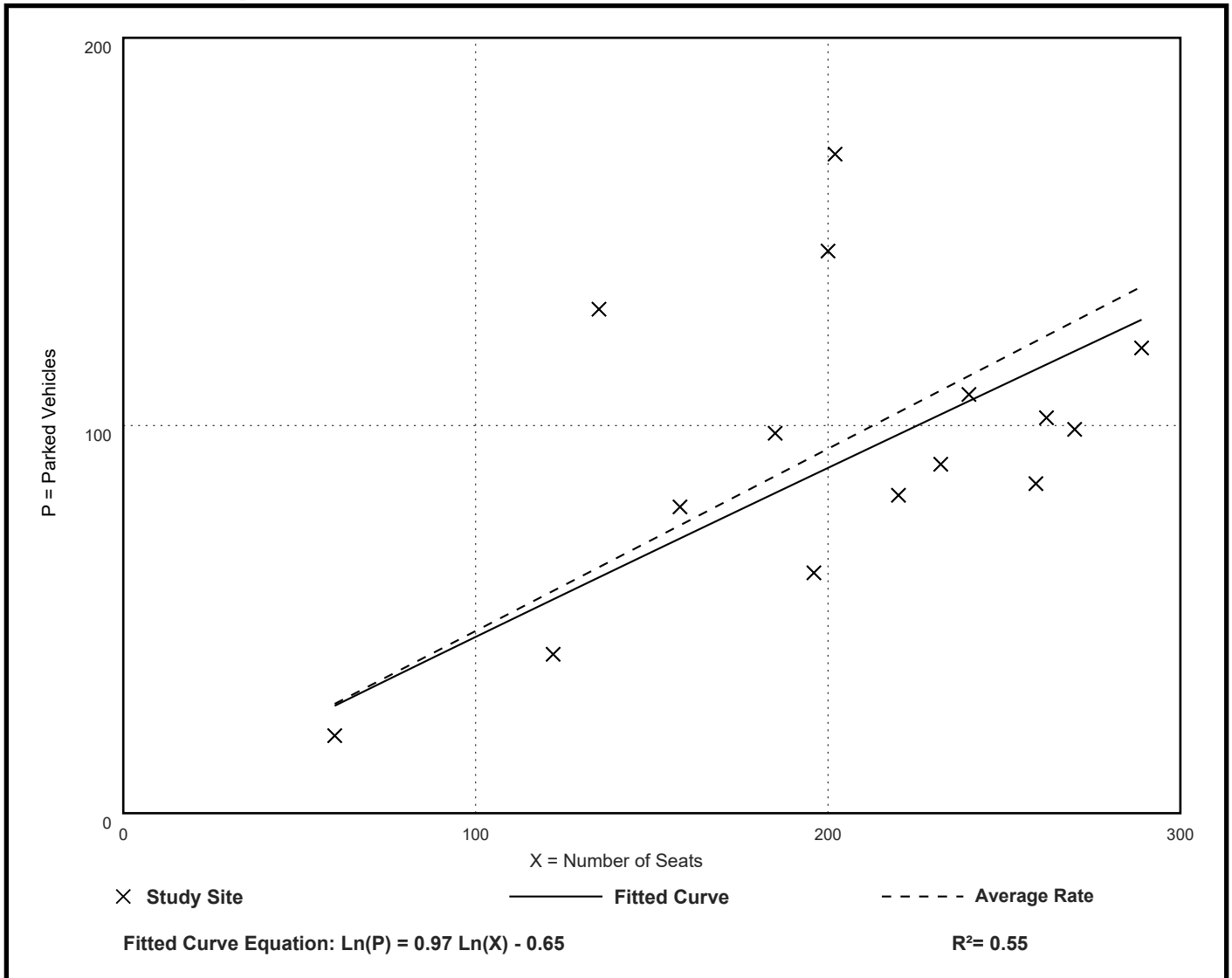
Number of Studies: 15

Avg. Num. of Seats: 202

## Peak Period Parking Demand per Seat

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|------------------------------------------|
| 0.47         | 0.32 - 0.96    | 0.37 / 0.79            | ***                     | 0.19 ( 40% )                             |

## Data Plot and Equation



# High-Turnover (Sit Down) Restaurant Does Not Serve Breakfast (932)

Peak Period Parking Demand vs: Seats

On a: Saturday

Setting/Location: General Urban/Suburban

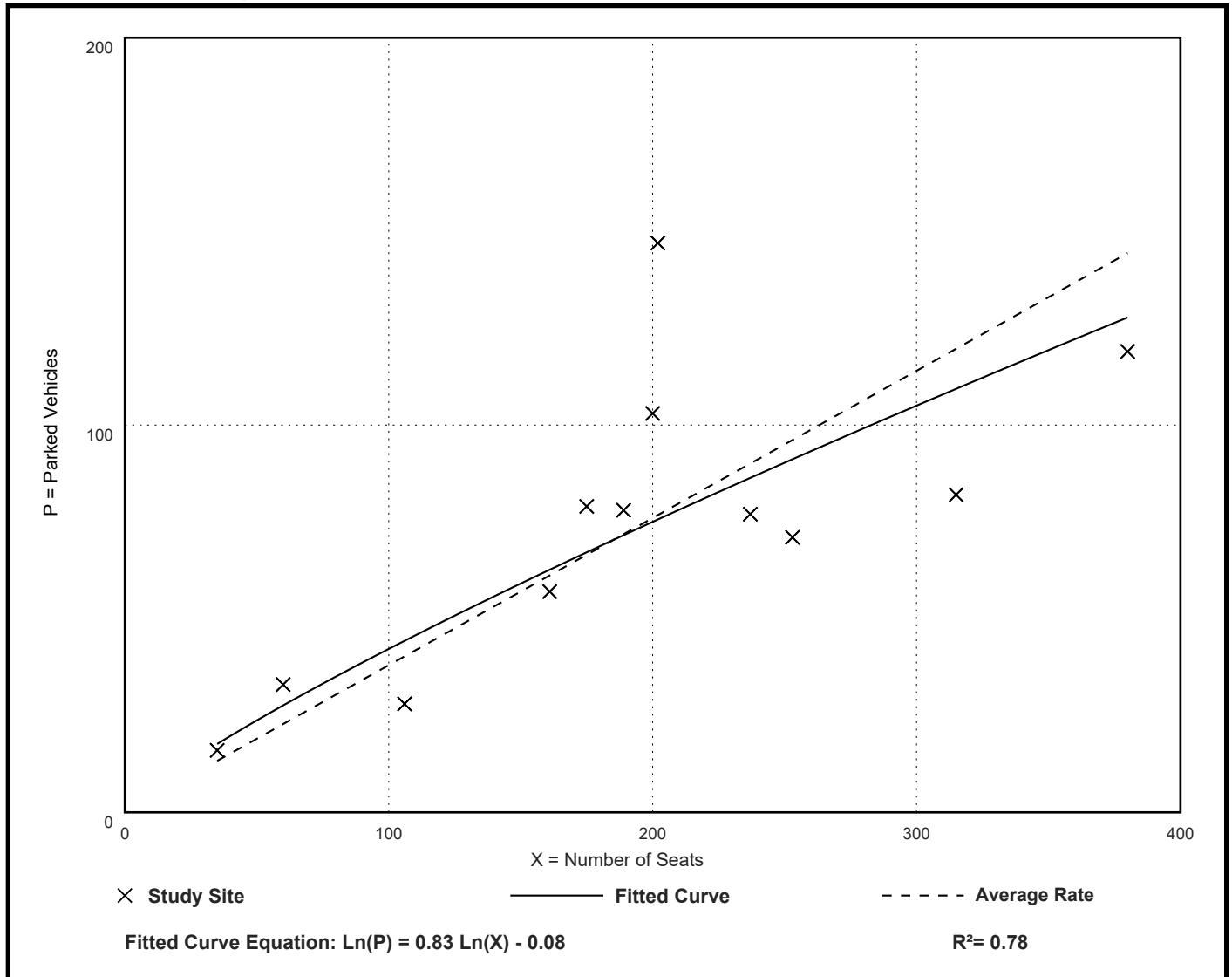
Number of Studies: 12

Avg. Num. of Seats: 193

## Peak Period Parking Demand per Seat

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|------------------------------------------|
| 0.38         | 0.26 - 0.73    | 0.32 / 0.56            | ***                     | 0.14 ( 37% )                             |

## Data Plot and Equation



# Land Use: 437 Bowling Alley

## Description

A bowling alley is a recreational facility that includes bowling lanes. A small lounge, restaurant and/or snack bar, video games, and billiard tables may also be available.

## Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a weekday at five study sites.

| Hour Beginning  | Percent of Weekday Peak Parking Demand |
|-----------------|----------------------------------------|
| 12:00–4:00 a.m. | —                                      |
| 5:00 a.m.       | —                                      |
| 6:00 a.m.       | —                                      |
| 7:00 a.m.       | —                                      |
| 8:00 a.m.       | —                                      |
| 9:00 a.m.       | —                                      |
| 10:00 a.m.      | —                                      |
| 11:00 a.m.      | —                                      |
| 12:00 p.m.      | —                                      |
| 1:00 p.m.       | —                                      |
| 2:00 p.m.       | —                                      |
| 3:00 p.m.       | —                                      |
| 4:00 p.m.       | —                                      |
| 5:00 p.m.       | 45                                     |
| 6:00 p.m.       | 87                                     |
| 7:00 p.m.       | 99                                     |
| 8:00 p.m.       | 100                                    |
| 9:00 p.m.       | 82                                     |
| 10:00 p.m.      | —                                      |
| 11:00 p.m.      | —                                      |

## **Additional Data**

Factors such as the extent of ancillary activities (for example, billiards, lounge) and transitions between activities (when one bowling league had not yet ended and the next was about to begin) may have affected the reported parking demand.

The average parking supply ratio for the four study sites with parking supply information is 5.6 spaces per lane. The average peak parking occupancy at these four sites is 56 percent.

The sites were surveyed in the 1990s in Oregon.

## **Source Number**

275

# Bowling Alley (437)

## Peak Period Parking Demand vs: Lanes

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

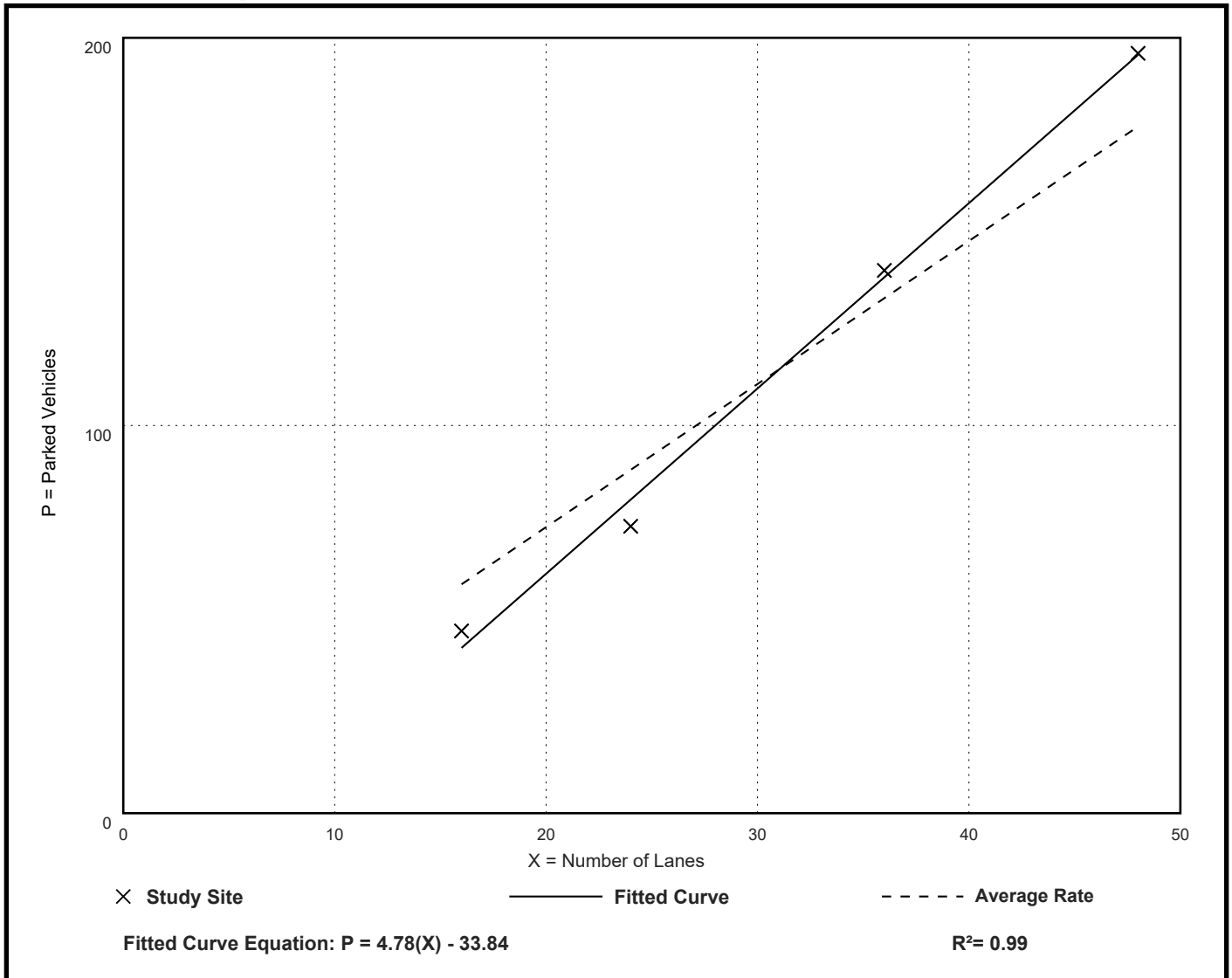
Number of Studies: 4

Avg. Num. of Lanes: 31

## Peak Period Parking Demand per Lane

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|------------------------------------------|
| 3.69         | 2.94 - 4.08    | 3.03 / 4.08            | ***                     | 0.54 ( 15% )                             |

## Data Plot and Equation



# Bowling Alley (437)

## Peak Period Parking Demand vs: Lanes

On a: Weekday (Monday - Friday)

Setting/Location: Dense Multi-Use Urban

Number of Studies: 1

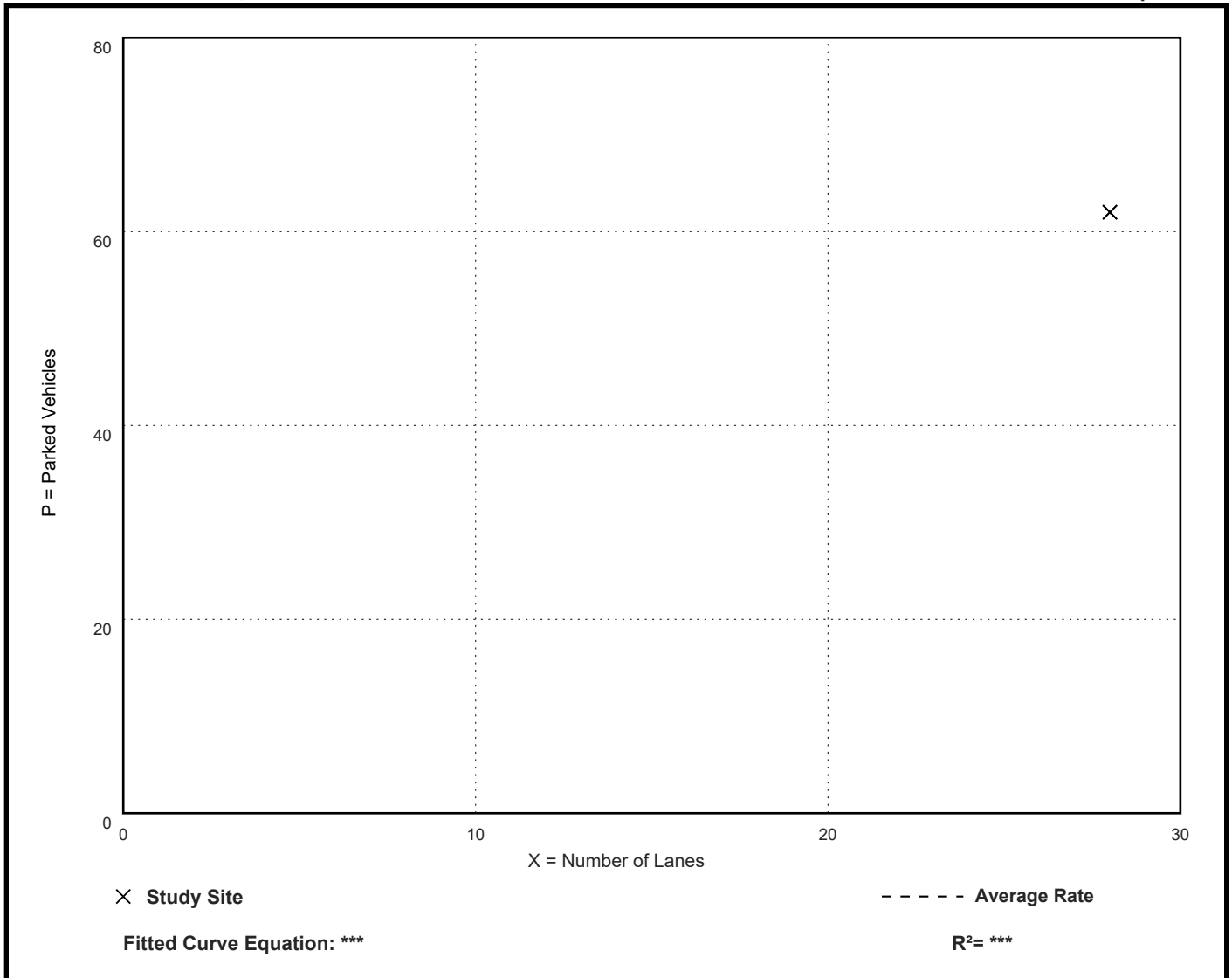
Avg. Num. of Lanes: 28

## Peak Period Parking Demand per Lane

| Average Rate | Range of Rates | 33rd / 85th Percentile | 95% Confidence Interval | Standard Deviation (Coeff. of Variation) |
|--------------|----------------|------------------------|-------------------------|------------------------------------------|
| 2.21         | 2.21 - 2.21    | *** / ***              | ***                     | *** ( *** )                              |

## Data Plot and Equation

Caution – Small Sample Size



# Land Use: 932

## High-Turnover (Sit-Down) Restaurant

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### Description

This land use consists of sit-down, full-service eating establishments with a typical duration of stay of 60 minutes or less. This type of restaurant is usually moderately priced, frequently belongs to a restaurant chain, and is commonly referred to as casual dining. Generally, these restaurants serve lunch and dinner; they may also be open for breakfast and are sometimes open 24 hours a day. These restaurants typically do not accept reservations. A patron commonly waits to be seated, is served by wait staff, orders from a menu, and pays after the meal.

Some facilities offer carry-out for a small proportion of its customers. Some facilities within this land use may also contain a bar area for serving food and alcoholic drinks.

Fast casual restaurant (Land Use 930), fine dining restaurant (Land Use 931), fast-food restaurant without drive-through window (Land Use 933), and fast-food restaurant with drive-through window (Land Use 934) are related uses.

### Additional Data

***Users should exercise caution when applying statistics during the AM peak periods, as the sites contained in the database for this land use may or may not be open for breakfast. In cases where it was confirmed that the sites were not open for breakfast, data for the AM peak hour of the adjacent street traffic were removed from the database.***

If the restaurant has outdoor seating, its area is not included in the overall gross floor area. For a restaurant that has significant outdoor seating, the number of seats may be more reliable than GFA as an independent variable on which to establish a trip generation rate.

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Florida, Georgia, Indiana, Kentucky, Massachusetts, Minnesota, New Hampshire, New Jersey, New York, Ohio, Oklahoma, Oregon, Pennsylvania, South Carolina, South Dakota, Texas, Vermont, and Wisconsin.

### Source Numbers

126, 269, 275, 280, 300, 301, 305, 338, 340, 341, 358, 384, 424, 432, 437, 438, 444, 507, 555, 577, 589, 617, 618, 728, 868, 884, 885, 903, 927, 939, 944, 961, 962, 977, 1048



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: Seats

On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Seats: 148

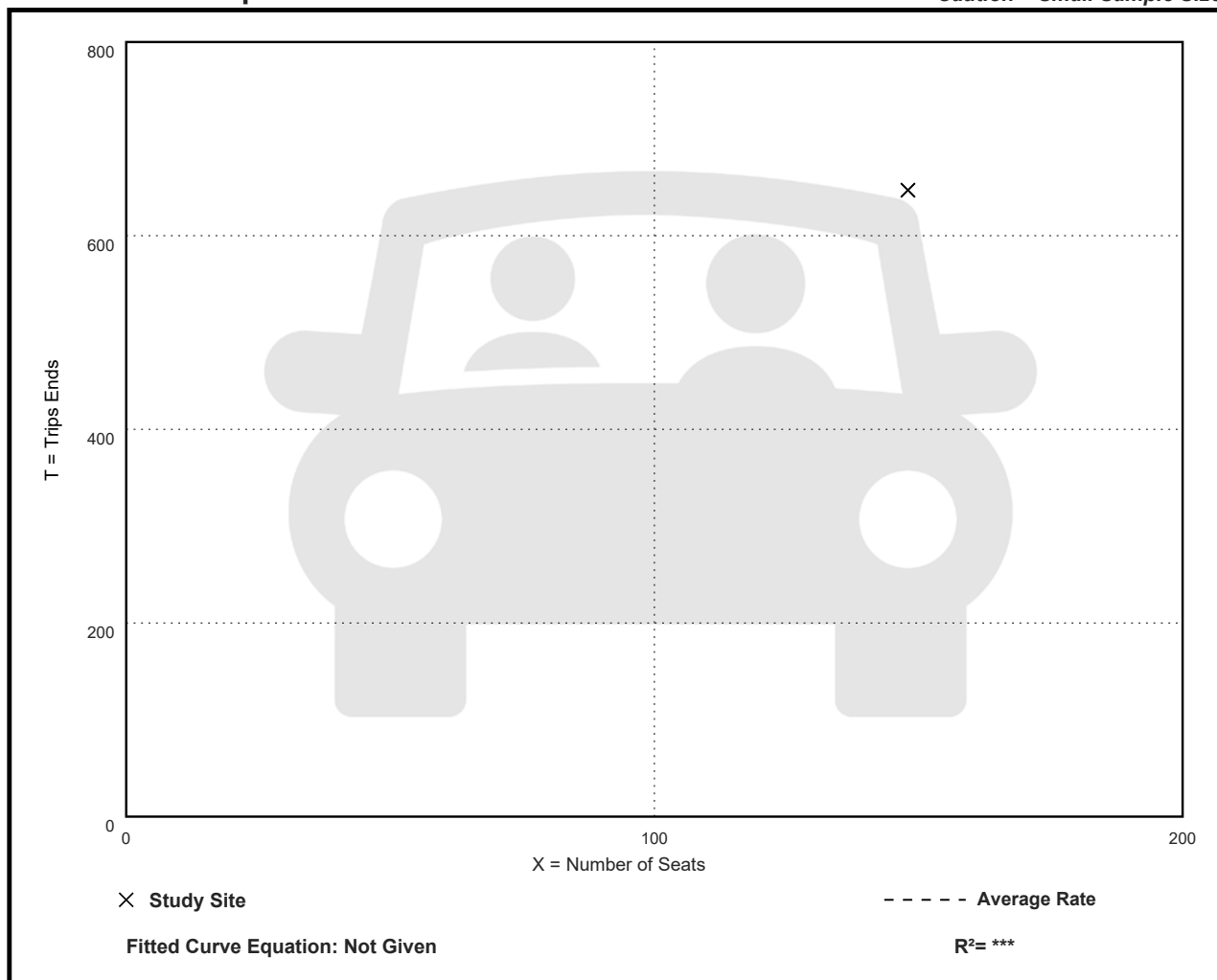
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Seat

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 4.37         | 4.37 - 4.37    | ***                |

## Data Plot and Equation

Caution – Small Sample Size



# High-Turnover (Sit-Down) Restaurant (932)

## Vehicle Trip Ends vs: Seats

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 7

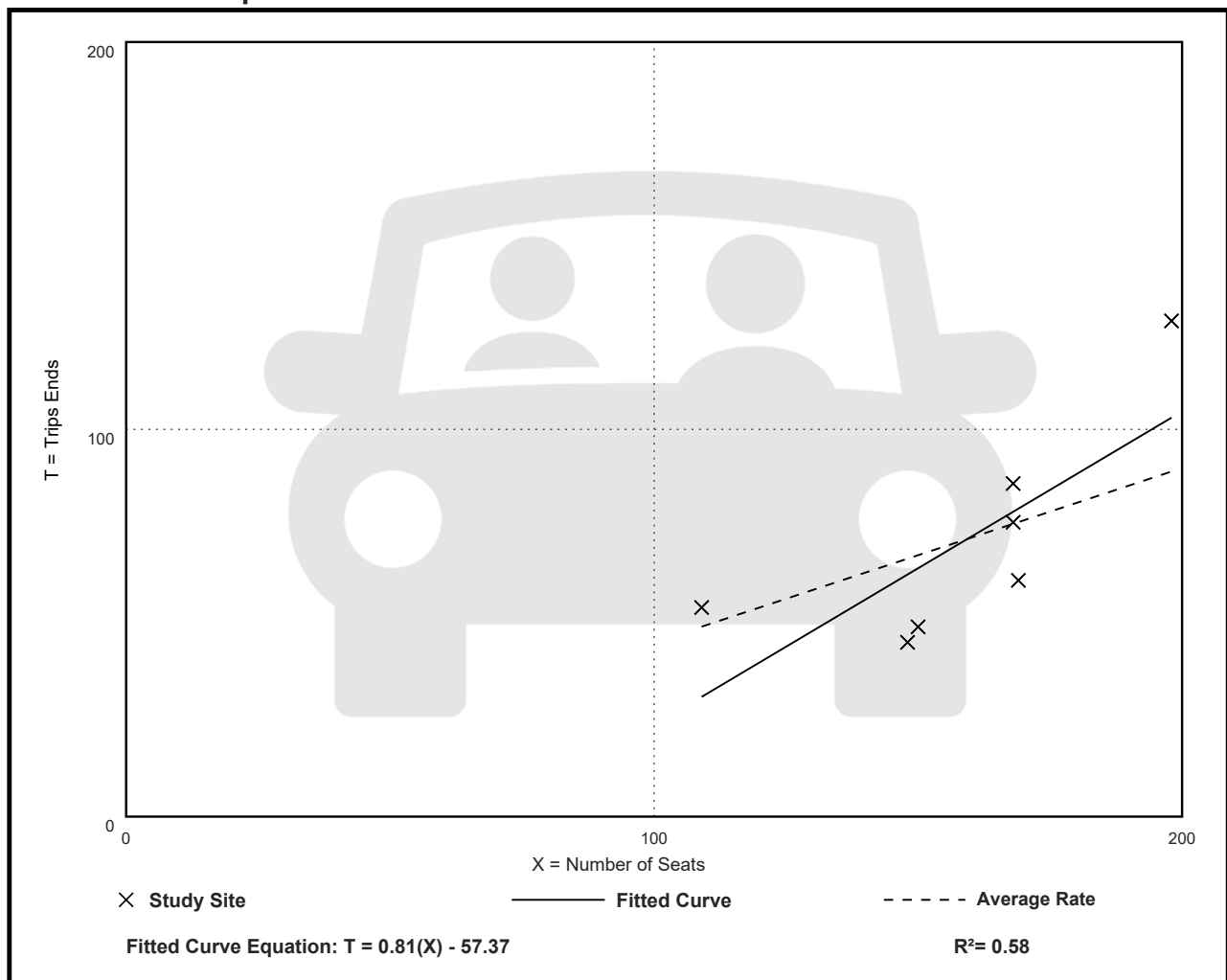
Avg. Num. of Seats: 159

Directional Distribution: 52% entering, 48% exiting

## Vehicle Trip Generation per Seat

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 0.45         | 0.30 - 0.65    | 0.13               |

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

## Vehicle Trip Ends vs: Seats

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 14

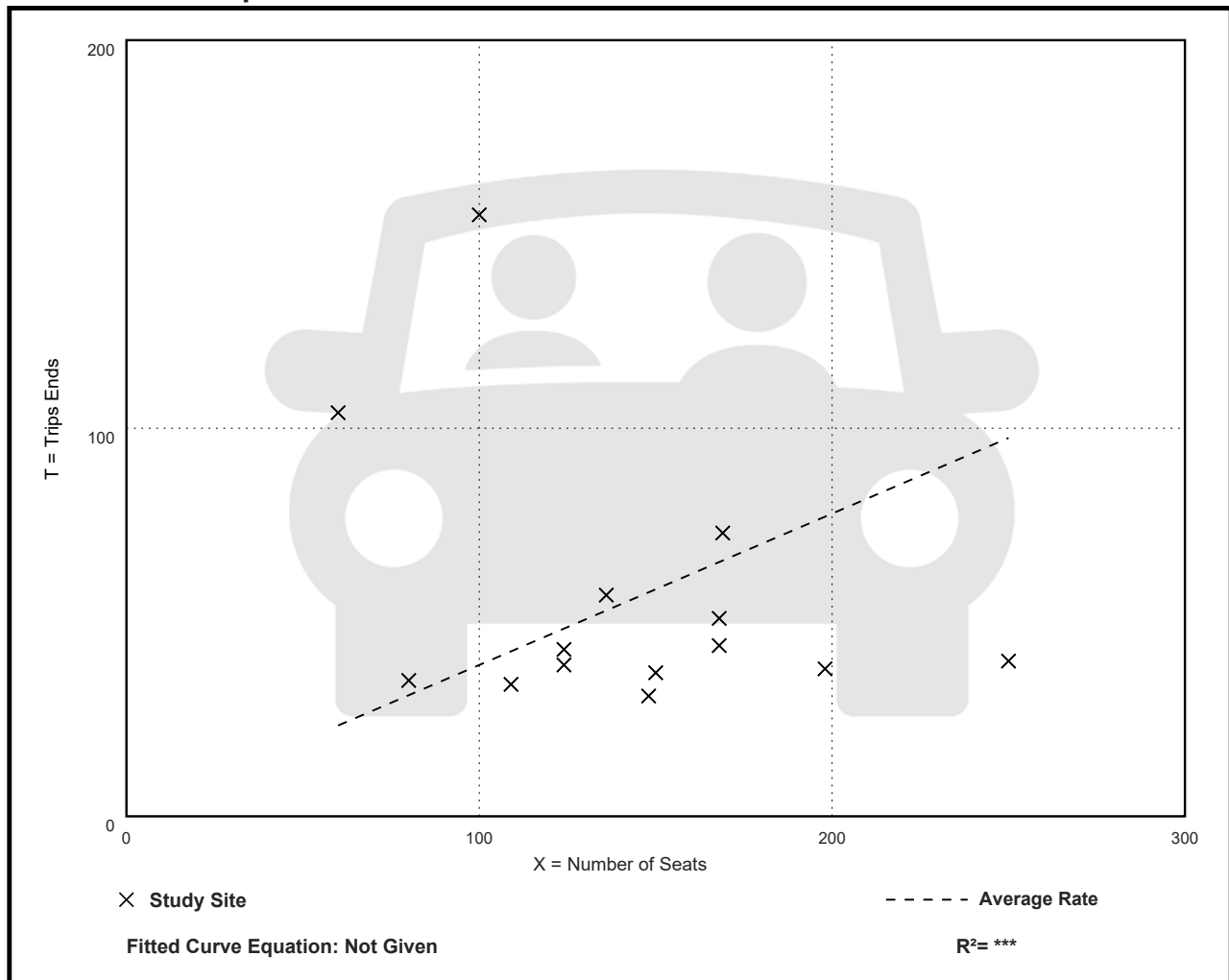
Avg. Num. of Seats: 142

Directional Distribution: 57% entering, 43% exiting

## Vehicle Trip Generation per Seat

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 0.39         | 0.16 - 1.73    | 0.39               |

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

## Vehicle Trip Ends vs: Seats

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

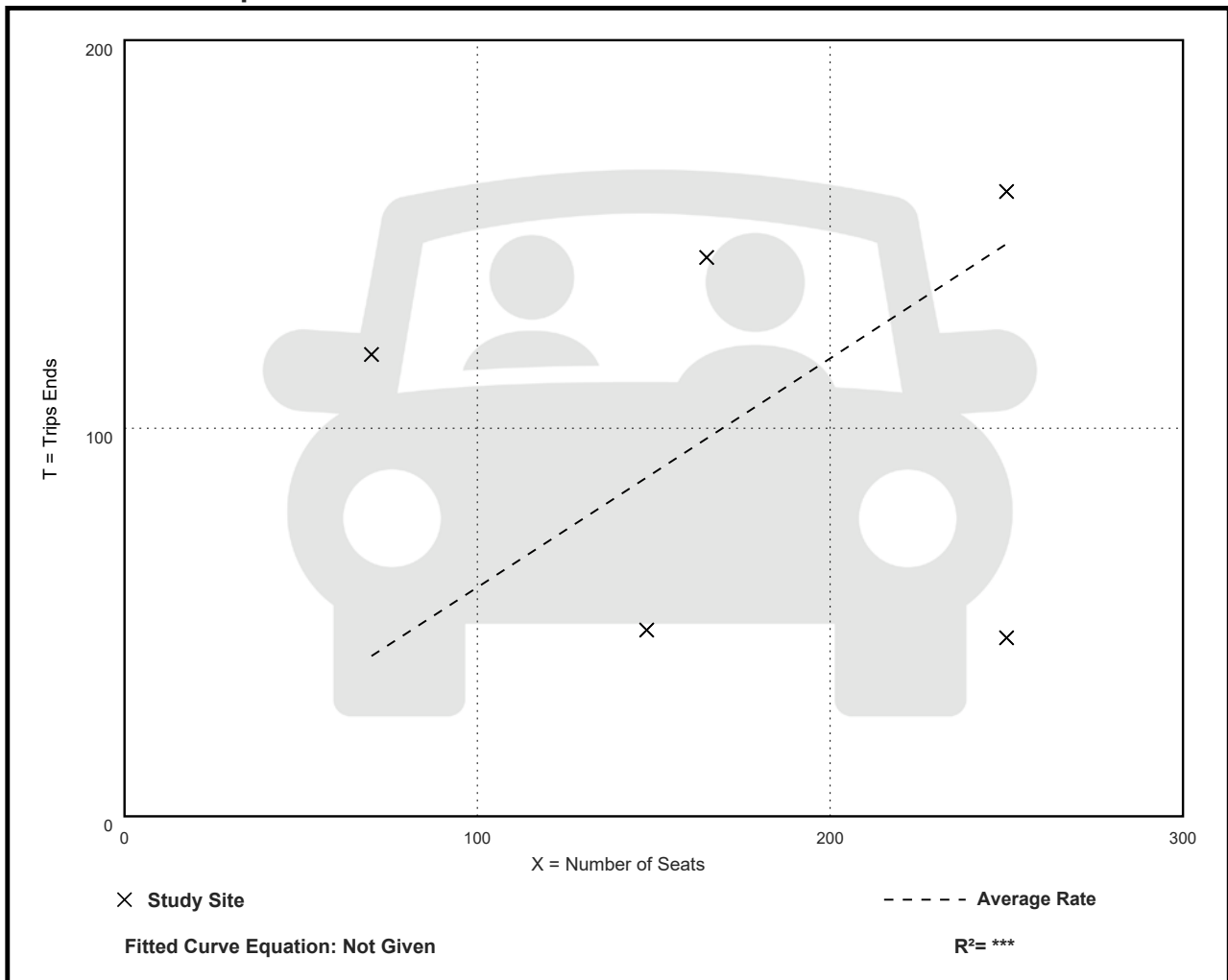
Avg. Num. of Seats: 177

Directional Distribution: 60% entering, 40% exiting

## Vehicle Trip Generation per Seat

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 0.59         | 0.18 - 1.70    | 0.46               |

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

## Vehicle Trip Ends vs: Seats

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 10

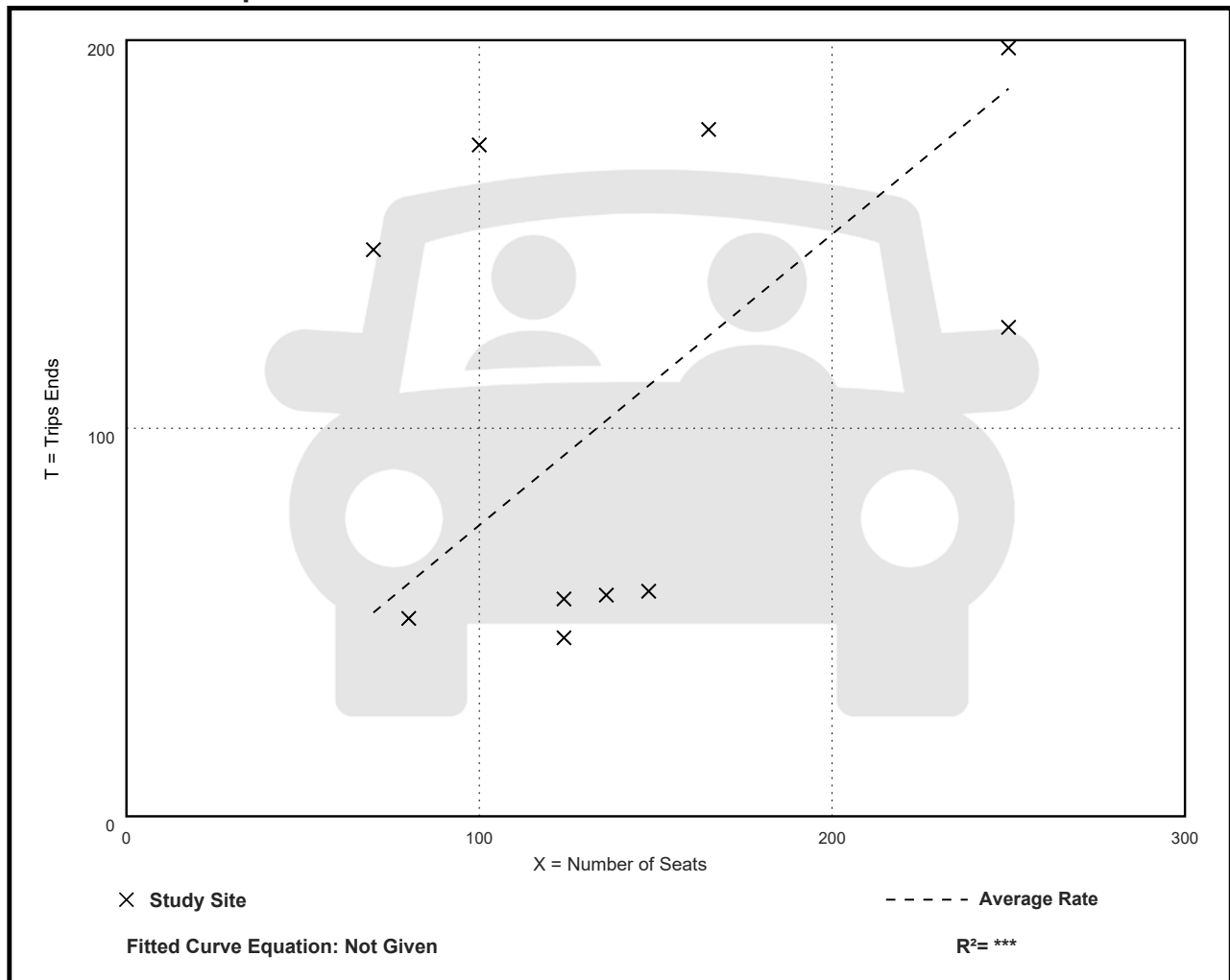
Avg. Num. of Seats: 145

Directional Distribution: 52% entering, 48% exiting

## Vehicle Trip Generation per Seat

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 0.75         | 0.37 - 2.09    | 0.49               |

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: Seats  
On a: Saturday

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Seats: 148

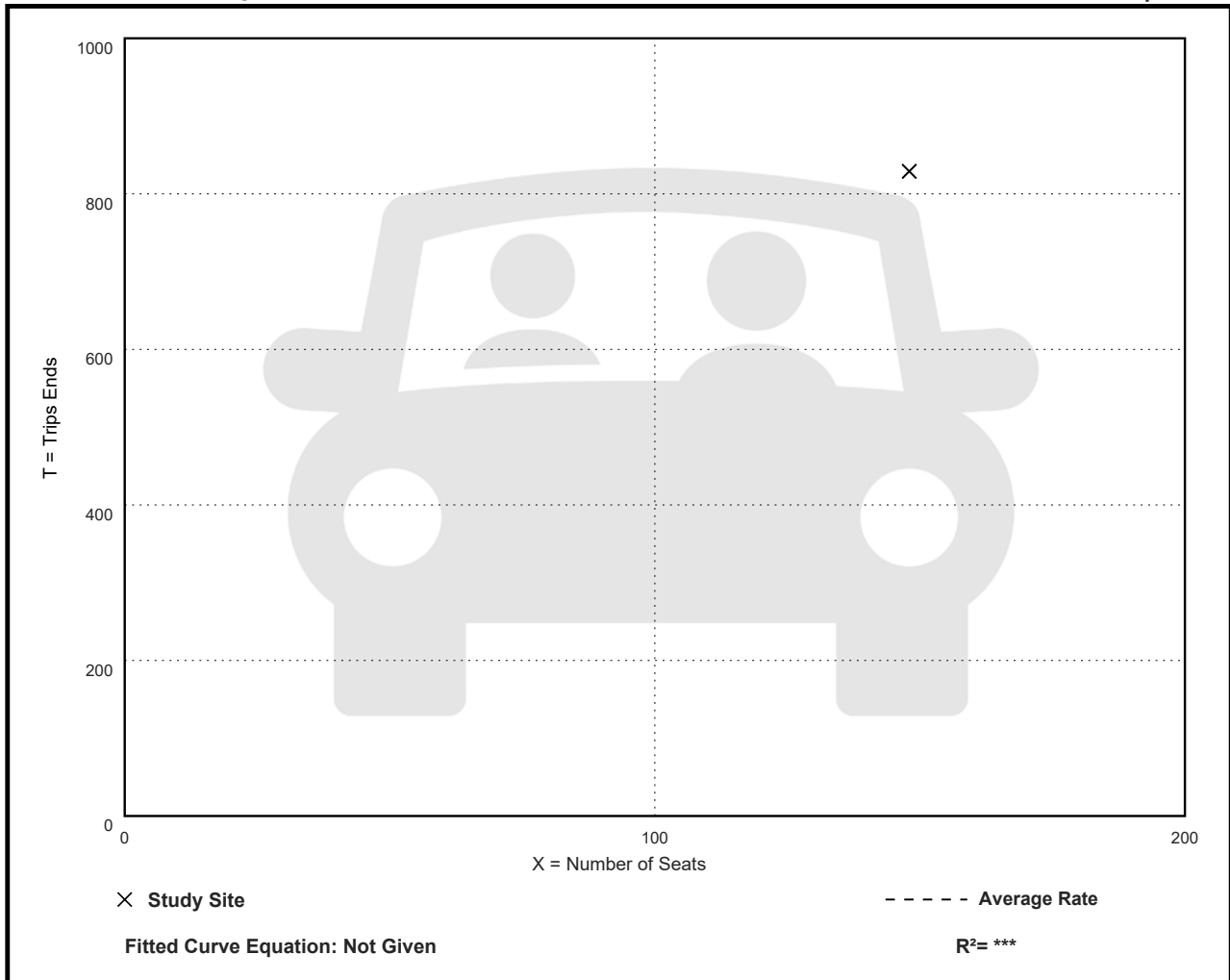
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Seat

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 5.60         | 5.60 - 5.60    | ***                |

## Data Plot and Equation

Caution – Small Sample Size



# High-Turnover (Sit-Down) Restaurant (932)

## Vehicle Trip Ends vs: Seats

On a: Saturday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 8

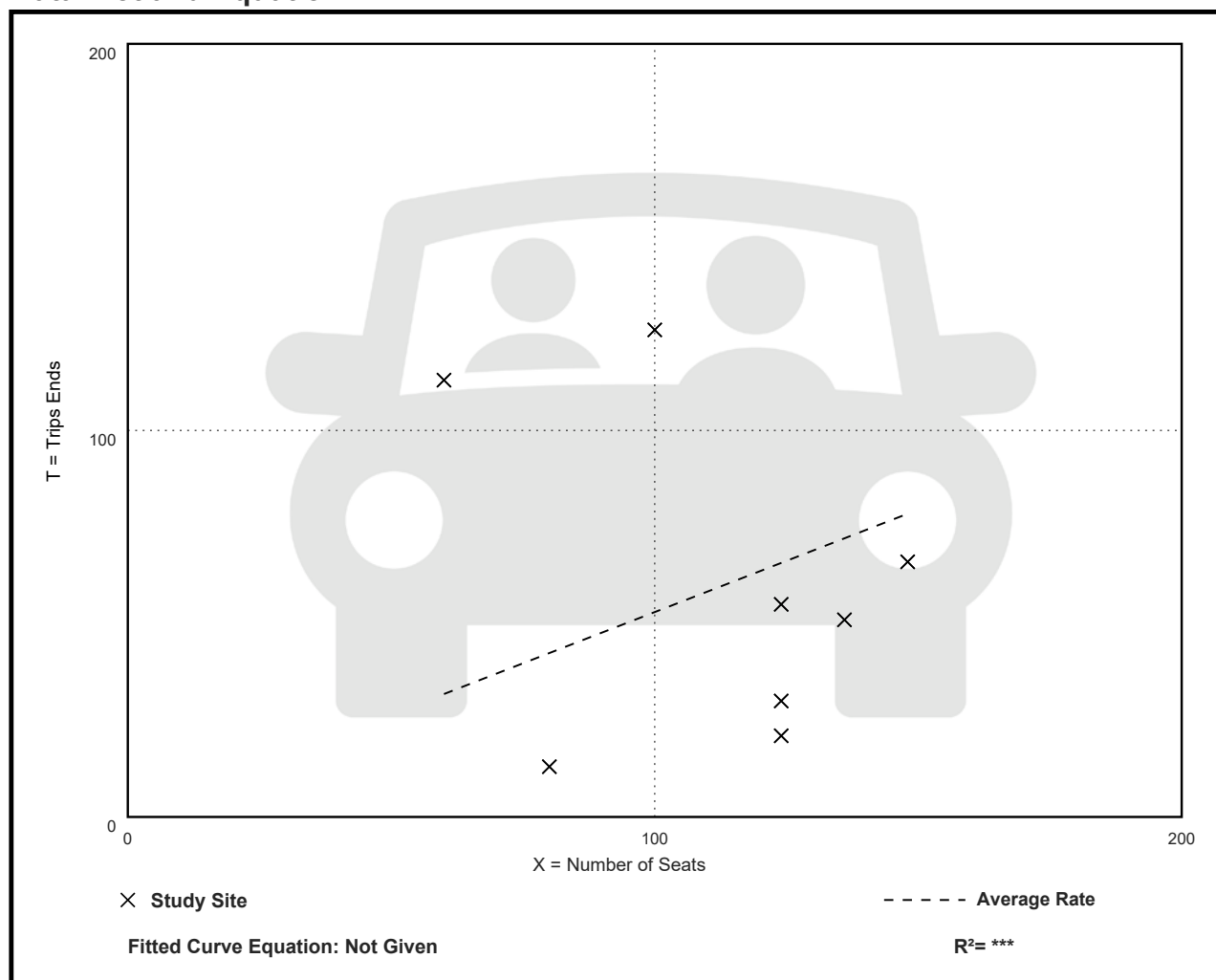
Avg. Num. of Seats: 112

Directional Distribution: 53% entering, 47% exiting

## Vehicle Trip Generation per Seat

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 0.53         | 0.16 - 1.88    | 0.51               |

## Data Plot and Equation



# Land Use: 437

## Bowling Alley

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### Description

A bowling alley is a recreational facility that includes bowling lanes. A small lounge, restaurant and/or snack bar, video games, and pool tables may also be available.

### Additional Data

The sites were surveyed in the 1990s, the 2000s, and the 2010s in Connecticut, Florida, and Texas.

### Source Numbers

400, 721, 945



# Bowling Alley (437)

## Vehicle Trip Ends vs: Bowling Lanes

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Bowling Lanes: 40

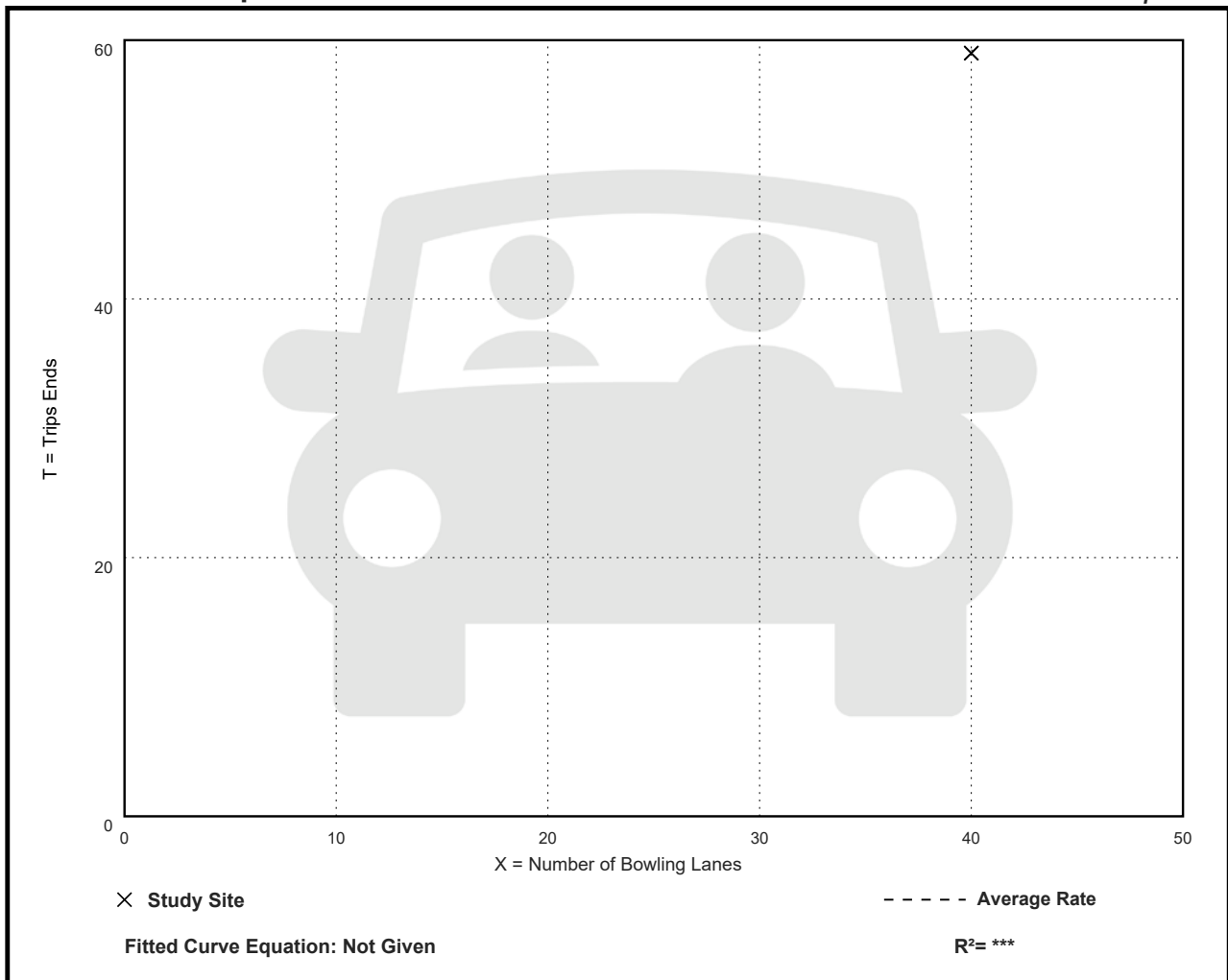
Directional Distribution: 95% entering, 5% exiting

## Vehicle Trip Generation per Bowling Lane

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 1.48         | 1.48 - 1.48    | ***                |

## Data Plot and Equation

Caution – Small Sample Size



# Bowling Alley (437)

## Vehicle Trip Ends vs: Bowling Lanes

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

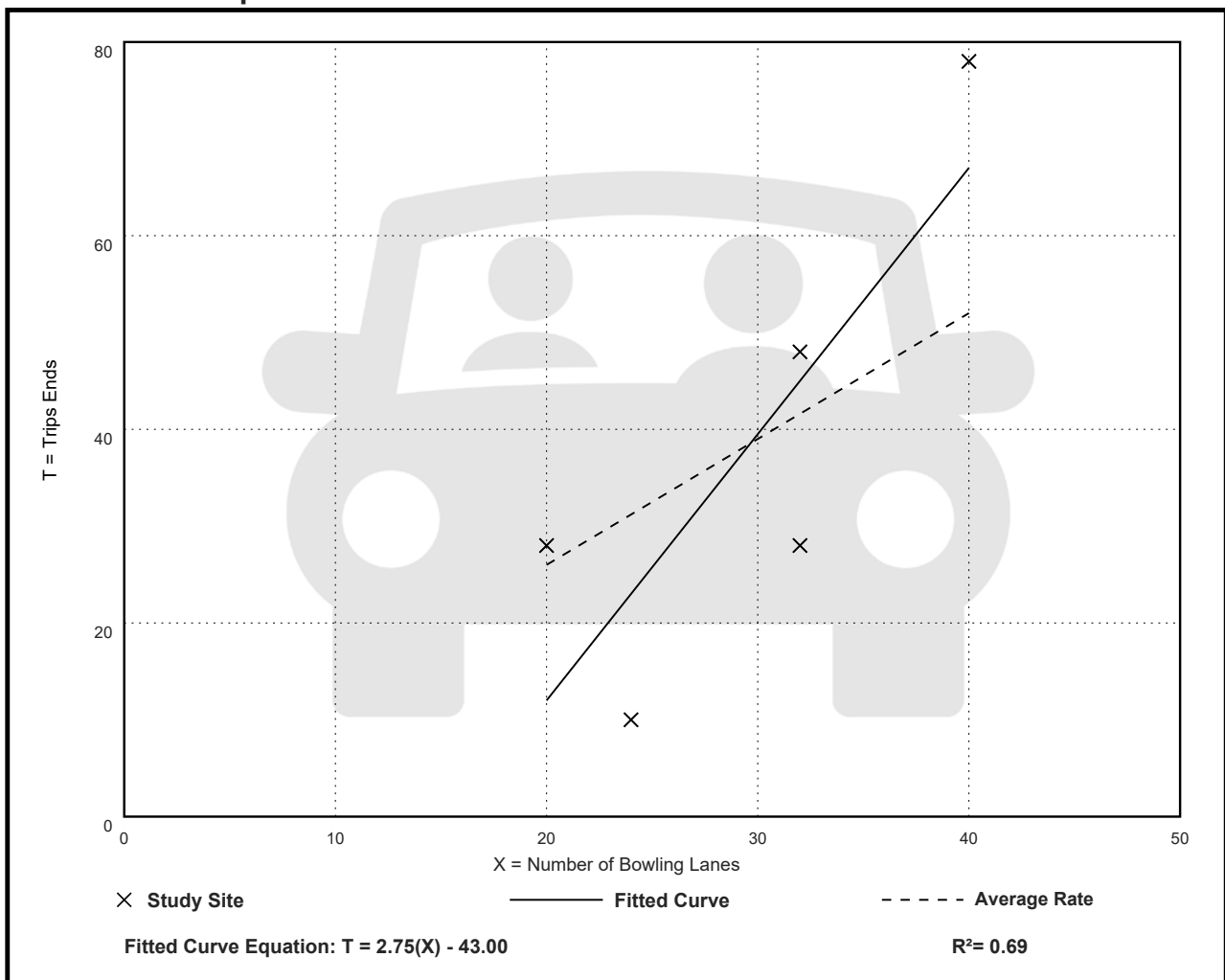
Avg. Num. of Bowling Lanes: 30

Directional Distribution: 65% entering, 35% exiting

## Vehicle Trip Generation per Bowling Lane

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 1.30         | 0.42 - 1.95    | 0.60               |

## Data Plot and Equation



# Bowling Alley (437)

## Vehicle Trip Ends vs: Bowling Lanes

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Bowling Lanes: 32

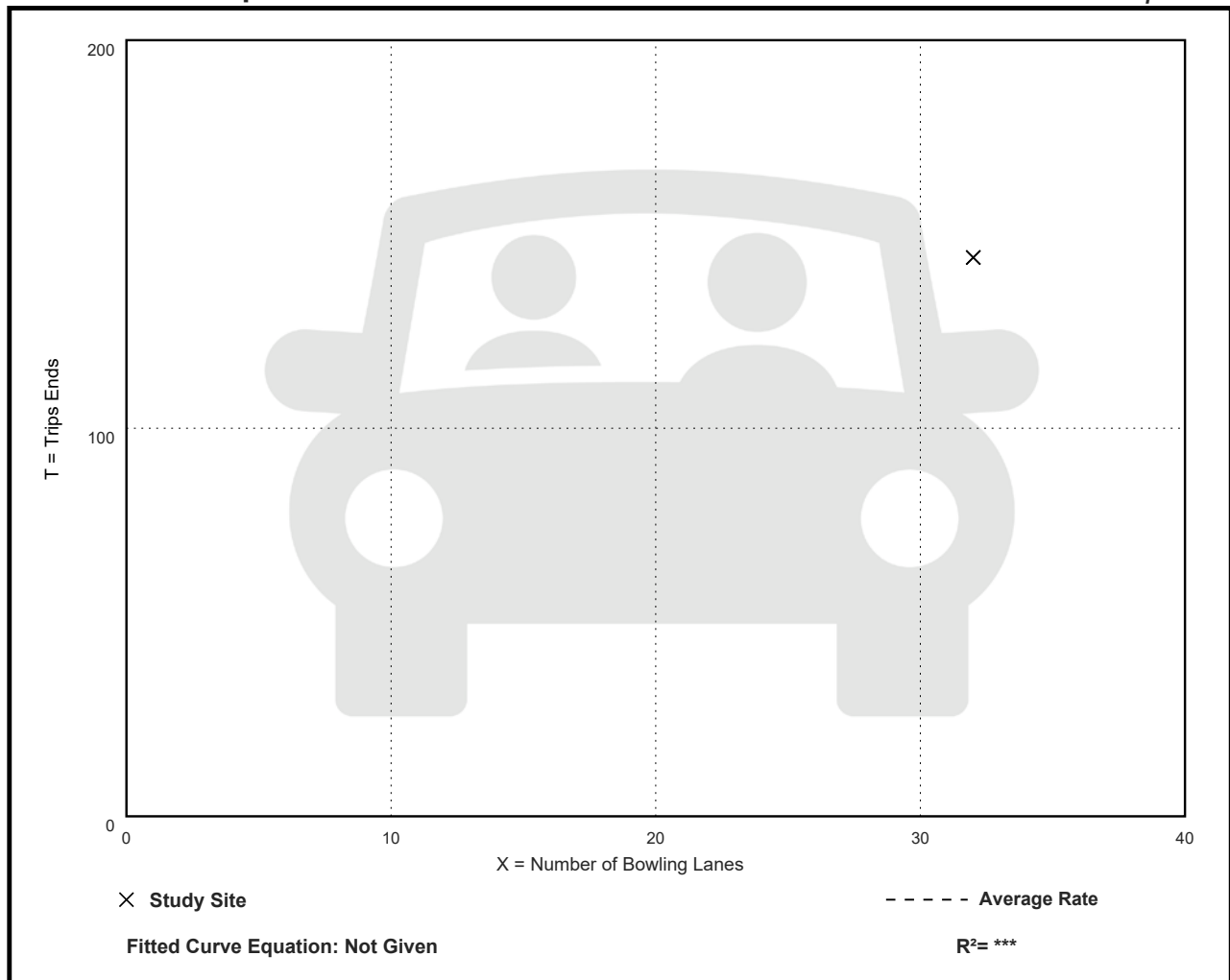
Directional Distribution: 83% entering, 17% exiting

## Vehicle Trip Generation per Bowling Lane

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 4.50         | 4.50 - 4.50    | ***                |

## Data Plot and Equation

Caution – Small Sample Size



# Bowling Alley (437)

## Vehicle Trip Ends vs: Bowling Lanes

On a: Saturday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Bowling Lanes: 32

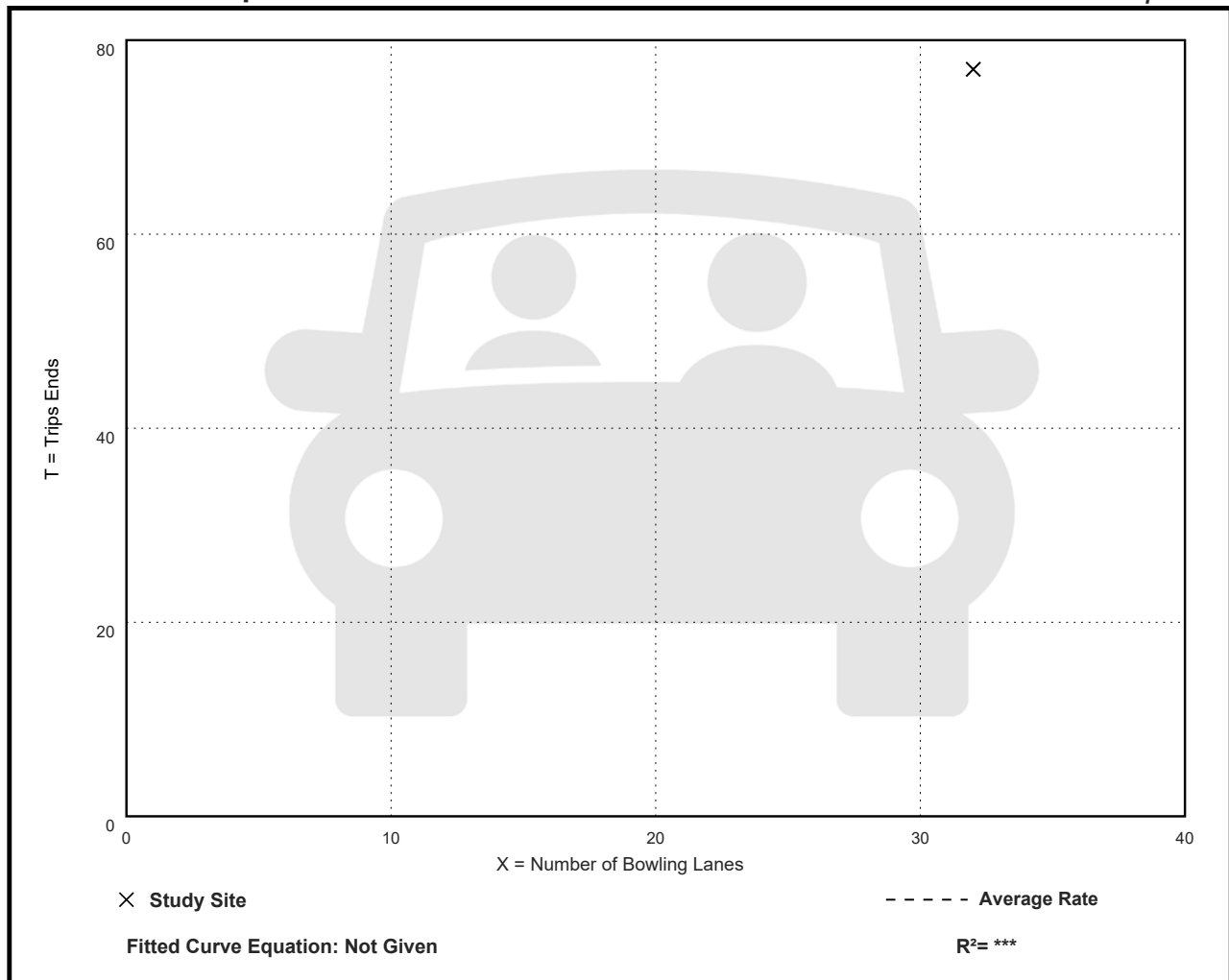
Directional Distribution: 39% entering, 61% exiting

## Vehicle Trip Generation per Bowling Lane

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 2.41         | 2.41 - 2.41    | ***                |

## Data Plot and Equation

Caution – Small Sample Size



# Bowling Alley (437)

## Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. 1000 Sq. Ft. GFA: 73

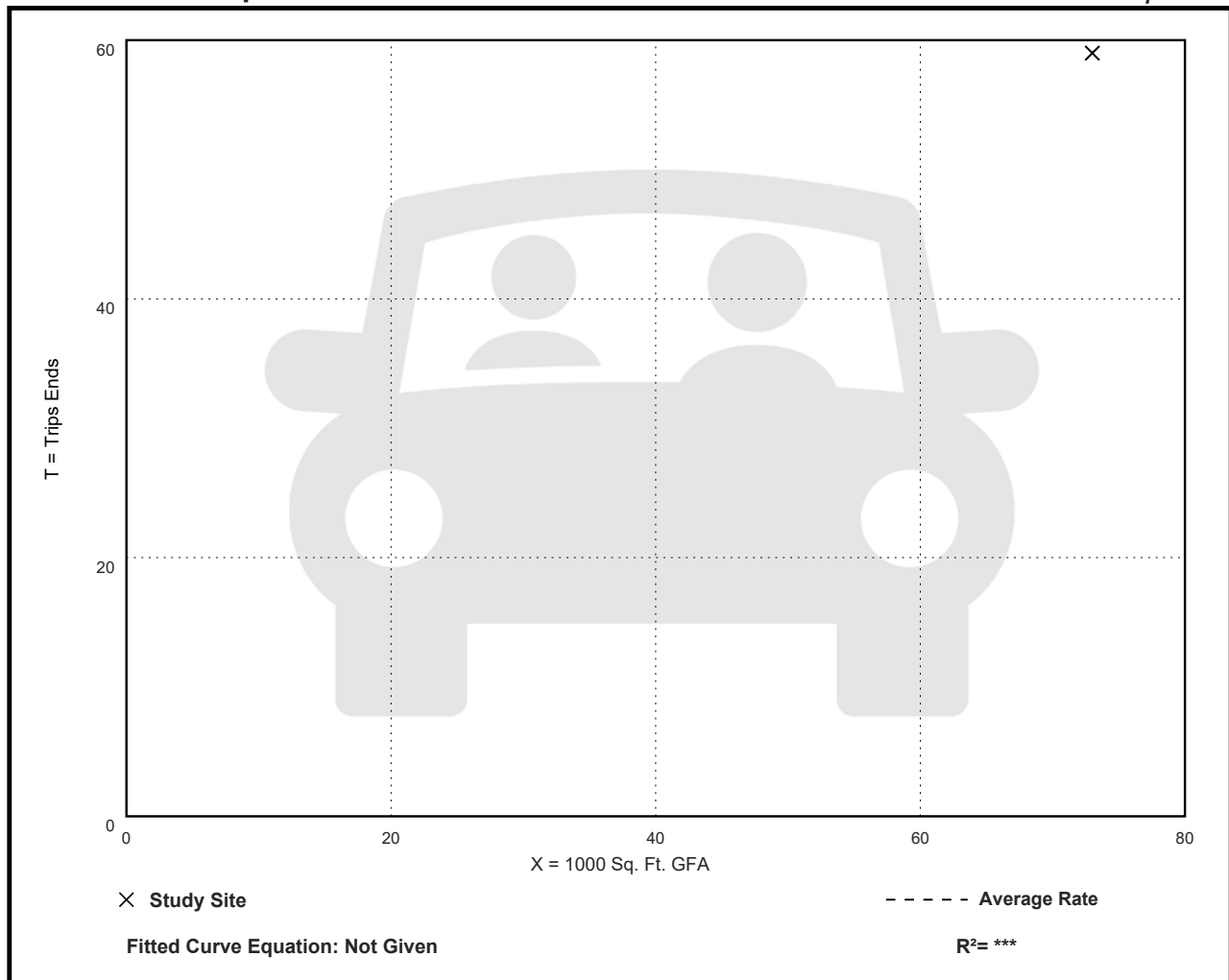
Directional Distribution: 95% entering, 5% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 0.81         | 0.81 - 0.81    | ***                |

## Data Plot and Equation

Caution – Small Sample Size



# Bowling Alley (437)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

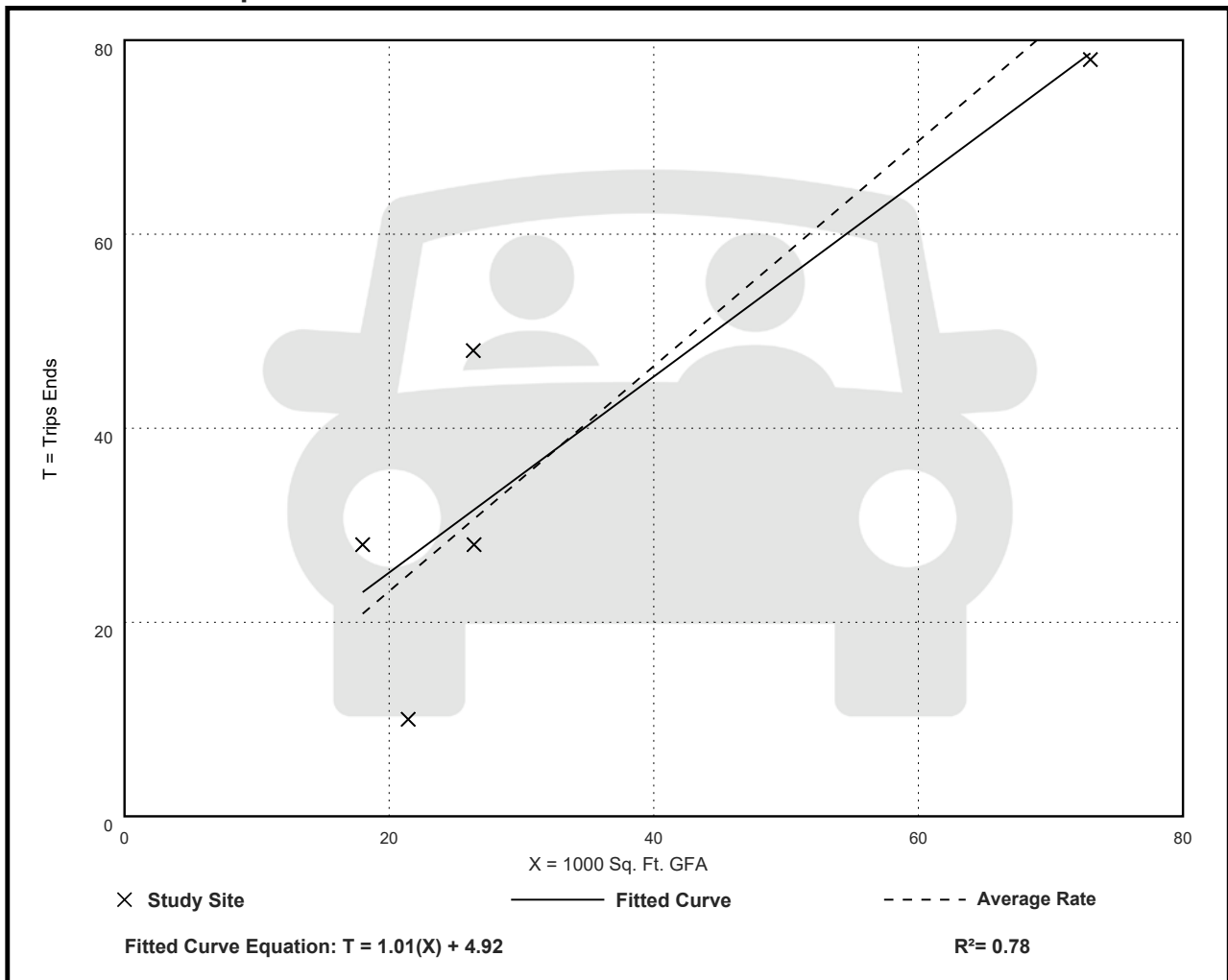
Avg. 1000 Sq. Ft. GFA: 33

Directional Distribution: 65% entering, 35% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 1.16         | 0.47 - 1.82    | 0.44               |

## Data Plot and Equation



# Land Use: 975

## Drinking Place

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### Description

A drinking place contains a bar, where alcoholic beverages and food are sold, and possibly some type of entertainment, such as music, television screens, video games, or pool tables. Establishments that specialize in serving food but also have bars are not included in this land use.

### Additional Data

All data for this land use were collected on Mondays through Thursdays.

The sites were surveyed in the 1980s, the 1990s, and the 2010s in Colorado, Florida, Oregon, Pennsylvania, and South Dakota.

### Source Numbers

291, 358, 583, 1020, 1053

# Drinking Place (975)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 12

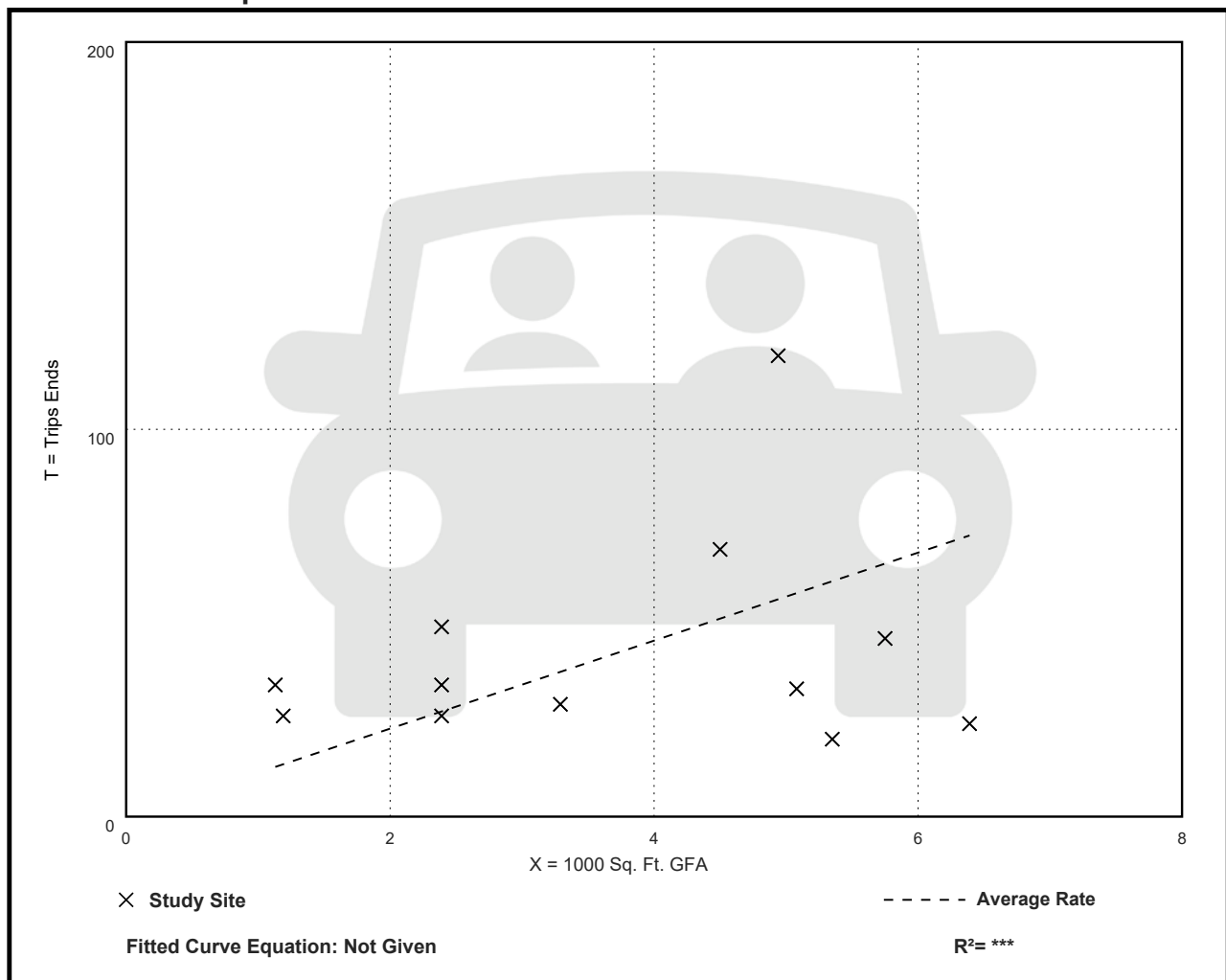
Avg. 1000 Sq. Ft. GFA: 4

Directional Distribution: 66% entering, 34% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 11.36        | 3.74 - 30.09   | 7.81               |

## Data Plot and Equation





# Drinking Place (975)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 8

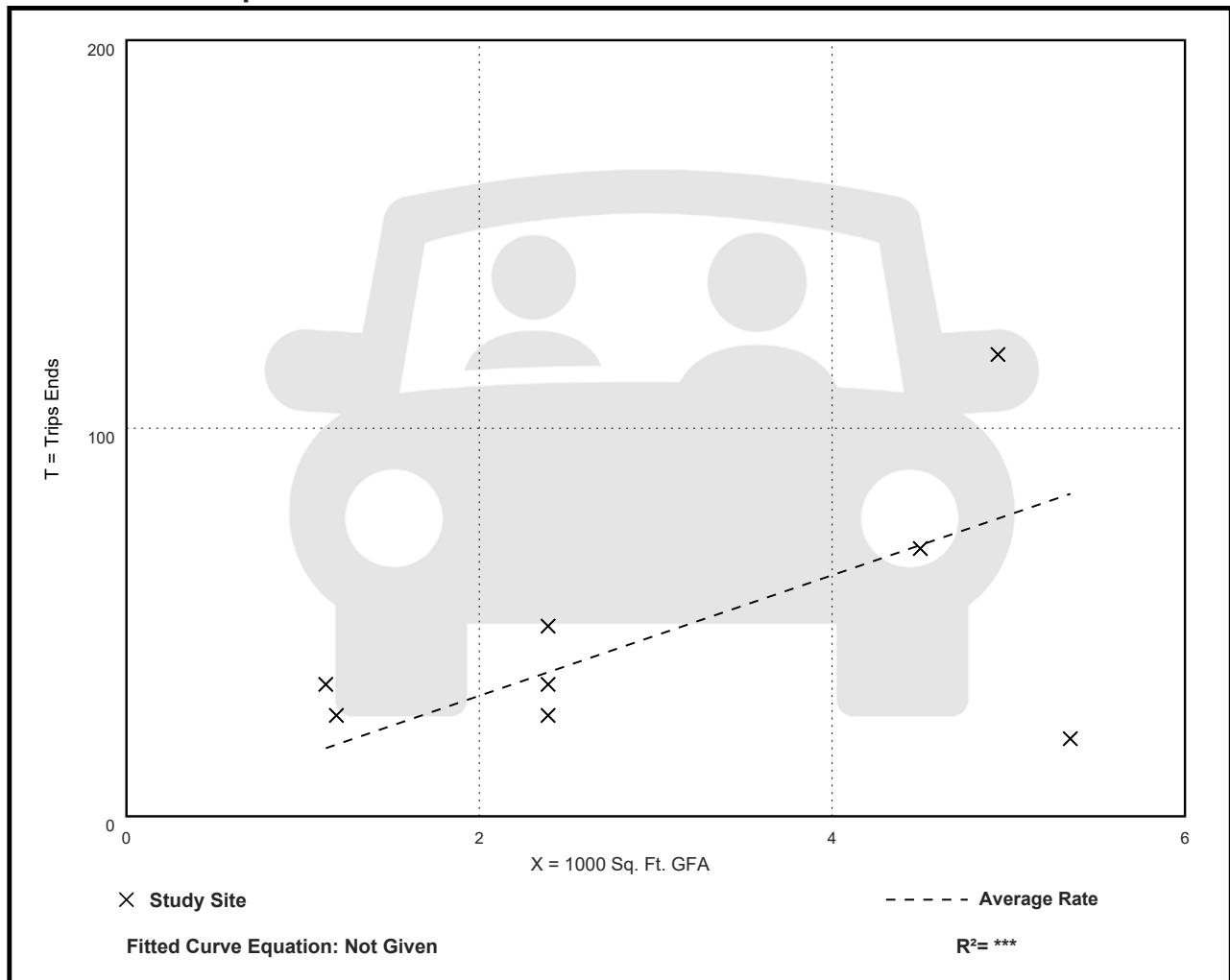
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 68% entering, 32% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 15.53        | 3.74 - 30.09   | 8.42               |

## Data Plot and Equation



# Drinking Place (975)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

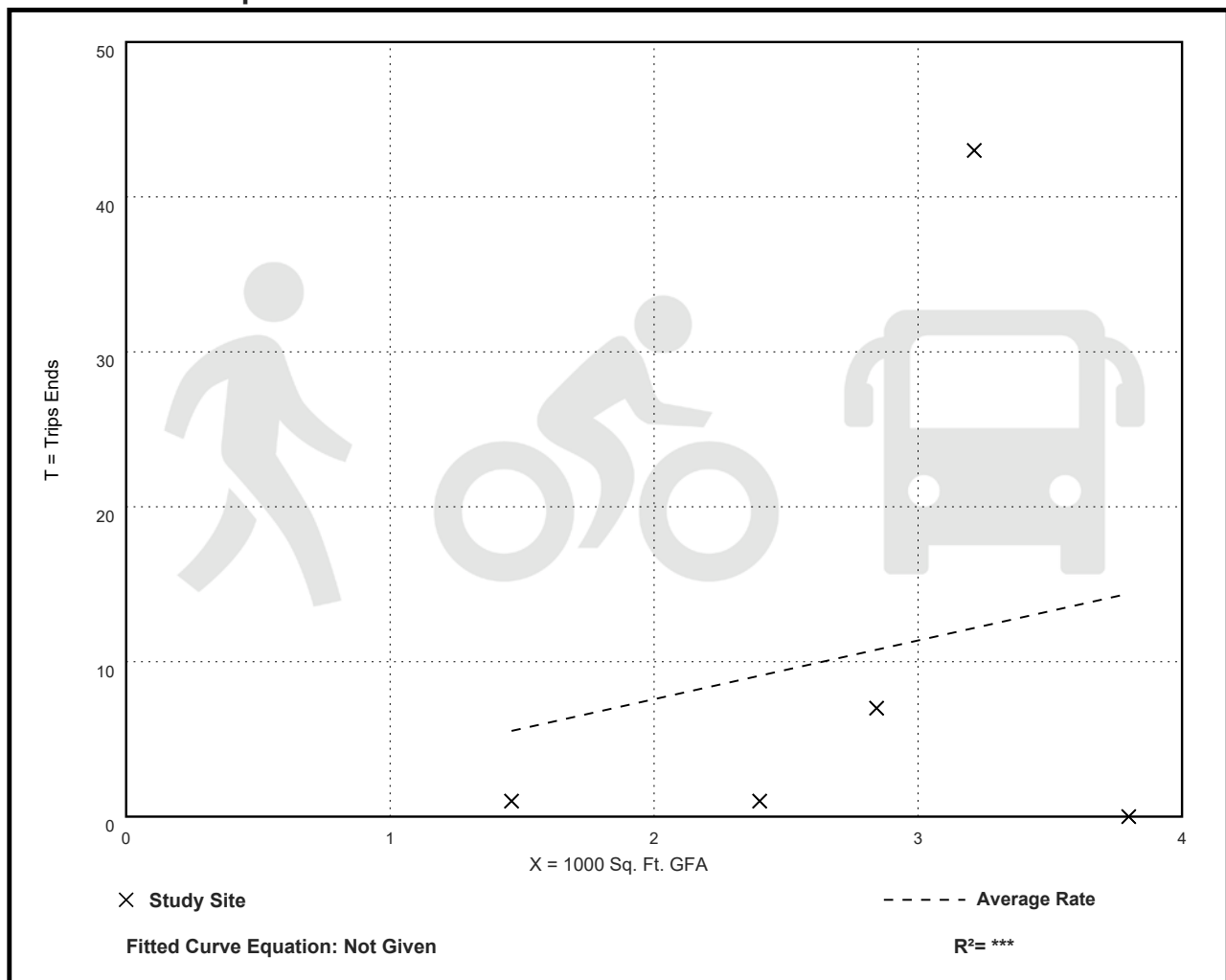
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 65% entering, 35% exiting

## Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 3.79         | 0.00 - 13.38   | 6.01               |

## Data Plot and Equation



# Drinking Place (975)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,  
PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

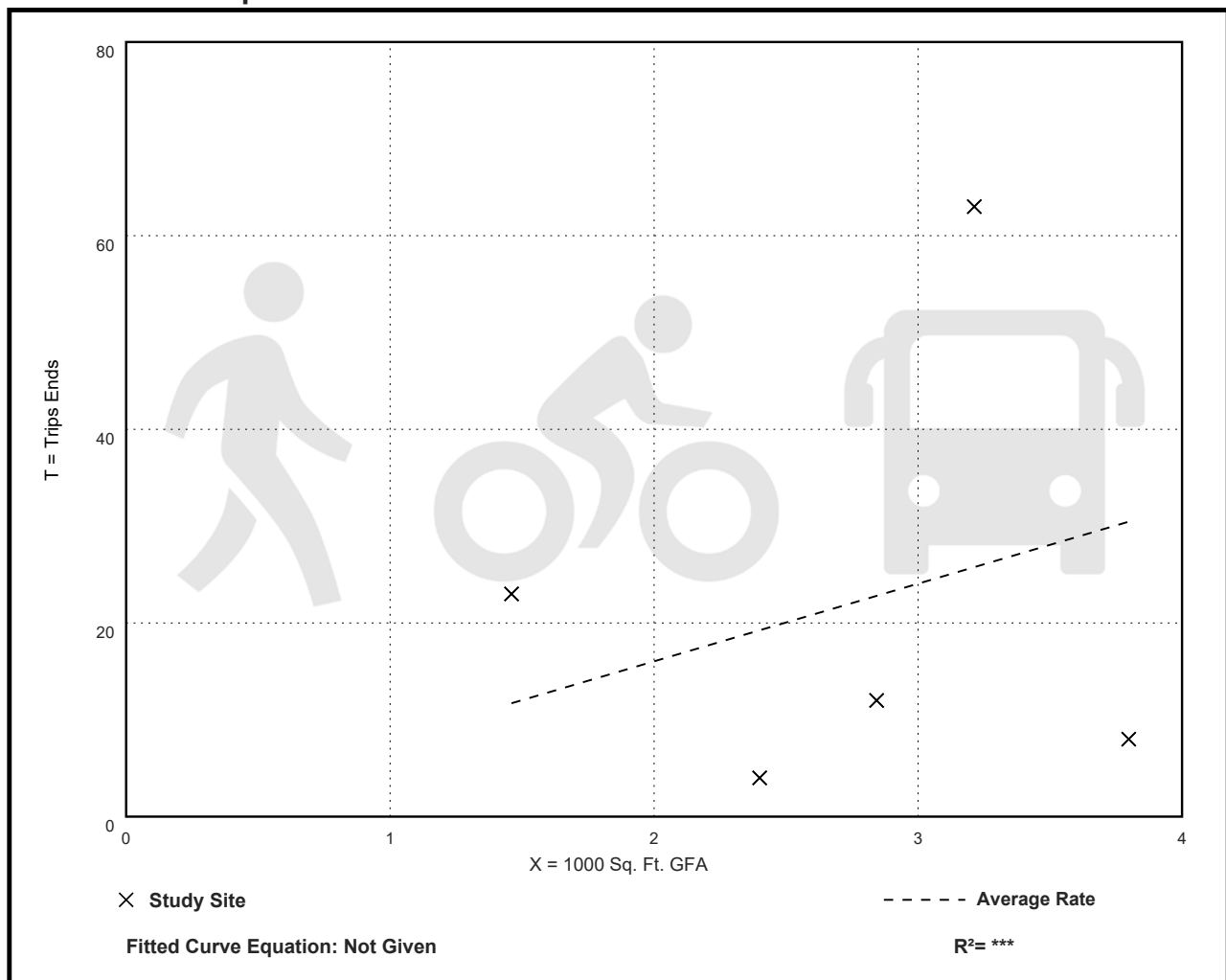
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 47% entering, 53% exiting

## Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 8.02         | 1.67 - 19.61   | 8.48               |

## Data Plot and Equation



# Drinking Place (975)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GFA

On a: Saturday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

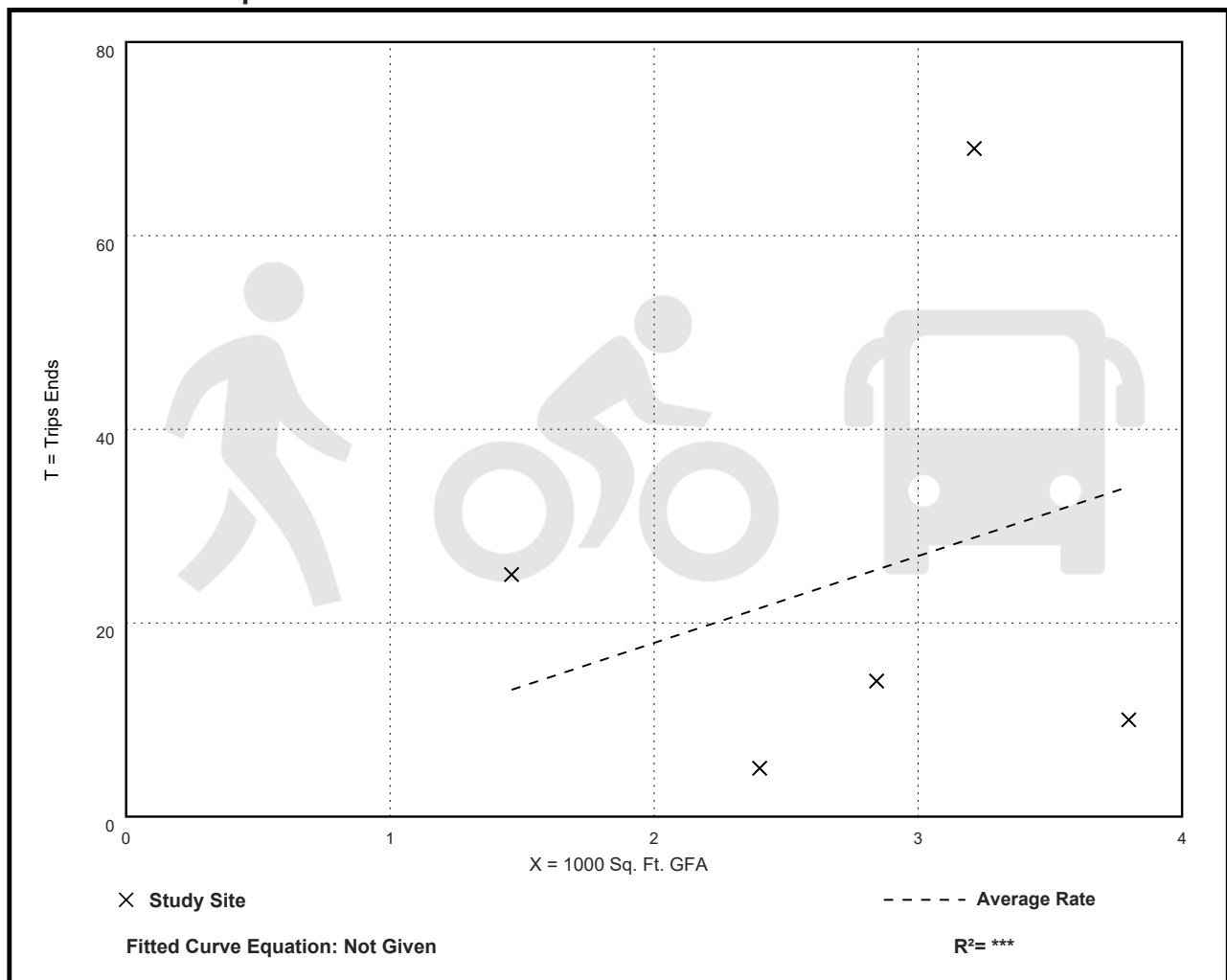
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 54% entering, 46% exiting

## Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GFA

| Average Rate | Range of Rates | Standard Deviation |
|--------------|----------------|--------------------|
| 8.97         | 2.08 - 21.48   | 9.12               |

## Data Plot and Equation



parking spaces and 854 parking deck spaces). Parking Area C is located north of Sears and Dick's Sporting Goods and includes 1,359 surface parking spaces.

### Existing Parking Zoning Requirements

The current zoning code requires that the mall provide 4,545 parking spaces. The breakdown of parking requirements based on Chapter 490-701 of the Salem Zoning Code is found in Table 1.

| <b>TABLE 1<br/>SALEM ZONING CODE –PARKING REQUIREMENTS</b> |                                |                            |                        |
|------------------------------------------------------------|--------------------------------|----------------------------|------------------------|
| <b>Use</b>                                                 | <b>Parking Demand Variable</b> | <b>Parking Requirement</b> | <b>Spaces Required</b> |
| First Floor Retail GFA                                     | 606,600 SF                     | 1 space / 200 SF GFA       | <b>3,033</b>           |
| Upper Floor Retail GFA                                     | 604,878 SF                     | 1 space / 400 SF GFA       | <b>1,512</b>           |
| <b>Total</b>                                               |                                |                            | <b>4,545</b>           |

## **3.0 EXISTING PARKING DEMAND**

Parking data was collected during the peak holiday shopping season of 2016 and in January 2017 in order to determine existing parking demand under various scenarios. Langan met with the town of Salem and its peer reviewer to develop the methodology of this study. Parking counts were conducted in half-hour intervals on Black Friday, November 25, 2016 from 7:00 a.m. to 4:00 p.m., on Saturday, December 10, 2016 from 9:00 a.m. to 3:00 p.m., and on Saturday, January 14, 2017 from 10:00 a.m. to 4:00 p.m. These were the anticipated peak shopping times for these dates. See Appendix B for detailed parking count spreadsheets.

Parking demand ratio is defined as the actual number of parked vehicles per 1,000 square-feet of gross floor area (GFA). Based on the existing parking observed, a peak parking demand ratio of 3.82 was observed on Black Friday, 3.12 was observed on Saturday, December 10 and 2.04 was observed on Saturday, January 14. Table 2 summarizes the existing peak parking demands for the entire mall.

| <b>TABLE 2<br/>EXISTING PARKING DEMANDS FOR ENTIRE MALL</b> |                                          |                         |                                              |                                           |
|-------------------------------------------------------------|------------------------------------------|-------------------------|----------------------------------------------|-------------------------------------------|
| <b>Peak Period</b>                                          | <b>Maximum Number of Occupied Spaces</b> | <b>Parking Provided</b> | <b>Percentage of Occupied Parking Spaces</b> | <b>Observed Peak Parking Demand Ratio</b> |
| Friday, November 25, 2016 @ 1:30 P.M.                       | 4,623                                    | 4,682 <sup>1</sup>      | <b>98.87</b>                                 | <b>3.82</b>                               |
| Saturday, December 10, 2016 @ 2:30 P.M.                     | 3,781                                    | 4,740                   | <b>79.8%</b>                                 | <b>3.12</b>                               |
| Saturday, January 14, 2017 @ 2:30 P.M.                      | 2,472                                    | 4,740                   | <b>52.2%</b>                                 | <b>2.04</b>                               |

<sup>1</sup> Available parking reduced by the 58 spaces blocked off by the Sears Auto Center on Black Friday

| TABLE 1<br>ANTICIPATED AVERAGE PARKING DEMAND - SALEM NH LYONS |                                  |                                      |                                             |                                      |                                  |                                      |                                  |
|----------------------------------------------------------------|----------------------------------|--------------------------------------|---------------------------------------------|--------------------------------------|----------------------------------|--------------------------------------|----------------------------------|
| USE                                                            | LAND<br>USE<br>CODE <sup>1</sup> | WEEKDAY (MON - THURS)                |                                             | FRIDAY                               |                                  | SATURDAY                             |                                  |
|                                                                |                                  | Average<br>Parking Rate <sup>2</sup> | Anticipated<br>Average<br>Parking<br>Demand | Average<br>Parking Rate <sup>2</sup> | Anticipated<br>Parking<br>Demand | Average<br>Parking Rate <sup>2</sup> | Anticipated<br>Parking<br>Demand |
| Loretta's Last Call<br>(5,343 SF)                              | 932                              | 8.97 spaces/KSF                      | 48                                          | 14.27 spaces/KSF                     | 76                               | 11.53 spaces/KSF                     | 62                               |
| Game On!<br>(143 Seats)                                        | 932                              | 0.28 spaces/seat                     | 40                                          | 0.47 spaces/seat                     | 67                               | 0.38 spaces/seat                     | 54                               |
| <b>Total Average Parking<br/>Demand</b>                        |                                  | <b>0.28 spaces/seat</b>              | <b>88</b>                                   | <b>0.47 spaces/seat</b>              | <b>143</b>                       | <b>0.38 spaces/seat</b>              | <b>116</b>                       |

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

| TABLE 2<br>ANTICIPATED PEAK (85TH PERCENTILE) PARKING DEMAND - SALEM NH LYONS |                                  |                                   |                                       |                                   |                                       |                                   |                                       |
|-------------------------------------------------------------------------------|----------------------------------|-----------------------------------|---------------------------------------|-----------------------------------|---------------------------------------|-----------------------------------|---------------------------------------|
| USE                                                                           | LAND<br>USE<br>CODE <sup>1</sup> | WEEKDAY (MON - THURS)             |                                       | FRIDAY                            |                                       | SATURDAY                          |                                       |
|                                                                               |                                  | Peak Parking<br>Rate <sup>2</sup> | Anticipated<br>Peak Parking<br>Demand | Peak Parking<br>Rate <sup>2</sup> | Anticipated<br>Peak Parking<br>Demand | Peak Parking<br>Rate <sup>2</sup> | Anticipated<br>Peak Parking<br>Demand |
| Loretta's Last Call<br>(5,343 SF)                                             | 932                              | 13.44 spaces/KSF                  | 72                                    | 20.24 spaces/KSF                  | 108                                   | 16.37 spaces/KSF                  | 87                                    |
| Game On!<br>(143 Seats)                                                       | 932                              | 0.48 spaces/seat                  | 69                                    | 0.79 spaces/seat                  | 113                                   | 0.56 spaces/seat                  | 80                                    |
| <b>Total Average Parking<br/>Demand</b>                                       |                                  | <b>0.28 spaces/seat</b>           | <b>141</b>                            | <b>0.47 spaces/seat</b>           | <b>221</b>                            | <b>0.38 spaces/seat</b>           | <b>167</b>                            |

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

| <b>TABLE 3</b><br><b>ANTICIPATED TRIP GENERATION - SALEM NH LYONS</b> |                               |              |            |            |                        |               |            |            |                         |
|-----------------------------------------------------------------------|-------------------------------|--------------|------------|------------|------------------------|---------------|------------|------------|-------------------------|
| USE                                                                   | LAND USE<br>CODE <sup>1</sup> | PM PEAK HOUR |            |            | WEEKDAY<br>DAILY TOTAL | SAT PEAK HOUR |            |            | SATURDAY<br>DAILY TOTAL |
|                                                                       |                               | ENTER        | EXIT       | TOTAL      |                        | ENTER         | EXIT       | TOTAL      |                         |
| Loretta's Last Call (5,343 SF)                                        | 932                           | 28           | 23         | 51         | 572                    | 30            | 30         | 60         | 654                     |
| Game On! (143 Seats)                                                  | 932                           | 32           | 24         | 56         | 625                    | 40            | 36         | 76         | 801                     |
| <b>Total New Trips</b>                                                |                               | <b>60</b>    | <b>47</b>  | <b>107</b> | <b>1197</b>            | <b>70</b>     | <b>66</b>  | <b>136</b> | <b>1455</b>             |
| Previously Approved Restaurant/Retail (19,600 SF)                     | -                             | 94           | 64         | 158        | 2022                   | 121           | 106        | 227        | 2508                    |
| <b>Net Change in Trips</b>                                            |                               | <b>-34</b>   | <b>-17</b> | <b>-51</b> | <b>-825</b>            | <b>-51</b>    | <b>-40</b> | <b>-91</b> | <b>-1053</b>            |

<sup>1</sup> Land Use Codes based on ITE Trip Generation Manual 11th Edition

<sup>2</sup> Volume based on ITE Trip Generation Manual 11th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

<sup>3</sup> Volume based on report titled "Traffic Impact Study - Redevelopment/Expansion of the Mall at Rockingham Park" by Langan dated June 2017.

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100 Cambridge Street, Suite 1310 Boston, MA 02114 T: 617.824.9100 F: 617.824.9101

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**To:** Jacob LaFontaine – Salem Planning Director

**From:** Richard Schlichting

**Date:** December 29, 2025

**Re:** Conditional Use Permit Request - Signage  
Lyons Group Restaurant at The Mall at Rockingham Park  
Salem, New Hampshire  
Langan Project No.: 151064501

---

In support of Lyons Group Restaurant request for a conditional use permit, Langan Engineering & Environmental Services, Inc. in conjunction with Salem NIM, LLC has prepared the following discussion of the conditional use permit criteria.

Lyons Group Restaurant seeks a Conditional Use Permit pursuant to Article VII, Section 490-702(D)(2), the business is allowed one wall sign not exceeding 16 square feet in size. The requested Conditional Use Permit requests eighteen signs surrounding the building.

On the West side of the building is the Game On main entrance. There are the following five signs.

Sign 1 is a 32 ft wide, 20 ft tall sign. It is a painted wall graphic and colored Red: #BA212A and Blue #183E6F. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 2 is the wording "DINE" DRINK PLAY" and WIN", all 1 ft 8 in. tall and 2 ft, 4 ft 3 in. and 2 ft wide, respectfully. It is an inner lit acrylic colored Red: #BA212A. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 3 is a 3 ft wide, 13 ft 2 in. tall, 1 ft wide sign. It is a lighted blade, inner lit acrylic and colored Red: #BA212A, Blue #183E6F, and White. It sits perpendicular to the building and has Game On's name and logo. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 12 is a 12 ft wide, 8 in. tall sign. It is an inner lit acrylic and colored Red: #BA212A. It sits on windows of the building and reads "Home of the Goat Fan". There are two of these signs on this side of the building and it is the only two on the building itself.



# MEMO

On the South side of the building is the Loretta's main entrance. There are the following four signs.

Sign 6 is a 14 ft 8 in. wide, 5 ft 6 in. tall sign. It is an LED inner lit sign and colored Pink: #D0679 and Maroon #5F100A. The sign is "Loretta's Last Call" logo. There is one of these signs on this side of the building and it is one of two on the building.

Sign 7 is a 14 ft 8 in. wide, 5 ft 6 in. tall sign. It is a back lit mounted decor and colored Brown: #593419. This sign is a Loretta's Star. There is one of these signs on this side of the building and it is one of three on the building.

Sign 8 is a 12 ft 11 in. wide, 22 ft 3 in. tall sign. It is a painted guitar graphic with LED strip lights on guitar strings and colored Red: #EE1816, Yellow: #E3DA8D, Green: #84B99A, Purple: #C84BC9, and Orange #D5673D. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 9 is a 3 ft 7 in. wide, 6 ft tall sign. It is an LED lit marquee sign colored Red: #FE030F. There is one of these signs on this side of the building and it is the only one on the building itself.

On the East side of the building is part of the Loretta's seating area. There are the following four signs.

The first sign is second Sign 6.

The second and third sign is Sign 7.

The final sign is Sign 5. Sign 5 is a 15 ft 11.5 in. wide, 7 ft 7 in. tall sign. It is an inner lit acrylic sign and colored Red: #BA212A and Blue #183E6F. The sign says Game On and has the logo. There is one of these signs on this side of the building and it is the only one on the building itself.

On the North side of the building is the back of the building. There are the following five signs.

Sign 4 is a 9 ft wide, 6 ft 5 in. tall sign. It is an inner lit sign and colored Pink: #D0679 and Maroon #5F100A. The sign is "Loretta's Last Call, Live Music" logo. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 10 is a 4 ft wide, 11 ft tall sign. It is a wall mounted fabric blade sign, perpendicular to the building. They will be a variety of colors. There are four of these signs on this side of the building and they are the only ones on the building.

(a) The modification complies with the purposes of the sign regulations noted in Subsection A.

**Response: The Town of Salem zoning regulations Subsection A of 490-702 "Signs", states the purpose of the section is "to regulate the erection of signs for the purpose of providing information and advertising in an orderly, effective and safe manner. Restrictions on type, location and size of signs protect the public from hazardous and distracting displays and create an attractive environment which is conducive to business, industry, and tourism".**

**The conditional use permit request pertains to the installation of additional building-mounted signage on all four sides of the structure. These signs are intended to improve business identification for customers approaching from multiple directions. Because the building is situated within a plaza and bordered by a ring road, it is visible from all angles. Therefore, each sign is strategically positioned to address a specific viewing corridor and ensure that drivers can identify the business safely and in advance.**

(b) The applicant's particular situation justifies a modification to the requirements.

**Response: Given the layout of the building and entrance to the mall and plaza, this modification is justifiable. It is imperative for both the success of the business as well as the pedestrian and vehicular safety that customers can easily identified their location and take direct routes to their destination.**

(c) The site is suitable for the proposed modification.

**Response: Given the layout of the plaza, this request to include additional wall signs is a suitable modification. The signage is proposed to face the proposed ring road and the adjacent surface parking. This will help vehicular and pedestrian traffic throughout the area.**

(d) There will be no adverse impact on neighboring properties.

**Response: This additional sign is proposed on a building façade that faces an internal parking area and other similar commercial uses. Because the sign is located within the site and oriented internally and towards a ring road, it will not create any adverse impacts on neighboring properties.**

(e) There will be no adverse impact on traffic or pedestrian safety.

**Response: The proposed signage informs motorists and pedestrians of the main business of the building. The sign will not be hazardous to traffic or pedestrian safety.**

(f) The aesthetic character of the site and the surrounding area will not be adversely affected.

**Response: The proposed sign will be consistent with the typical signage associated with this type of use and will align with the existing architectural style and character of the surrounding area.**

# MEMO

- (g) The modification will be consistent with the spirit and intent of this chapter and the Master Plan.

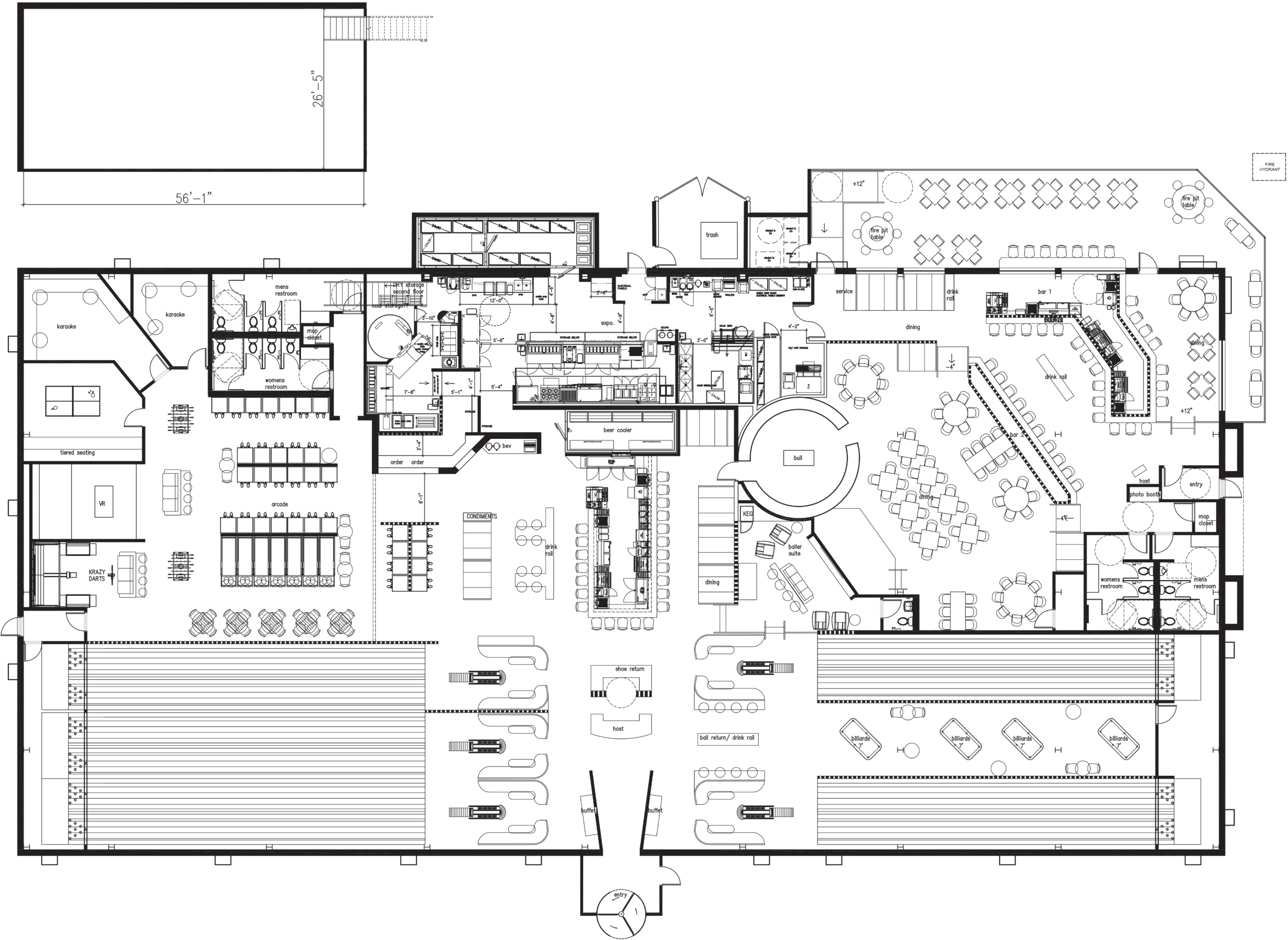
**Response: The proposed sign serves the Master Plan's goal of minimizing traffic impact on Salem streets and neighborhoods by clearly directing motorists directly to the proposed field.**

It is the opinion of the applicant and the project team that application meets all seven criteria necessary to be considered for a conditional use permit. We look forward to discussing this application with you at the next Planning Board hearing.

\\langan.com\data\bos\data5\151064501\project\_data\discipline\site\_civil\permit\_apps\site\_plan\_submission\parts\lyons\_group\_conditional use permit request - signage.docx

**GAME ON! / LORETTA'S LAST CALL**  
**EXTERIOR SIGNAGE PACKAGE**  
**12.22.2025**

# FLOOR PLAN



## GAME ON! COUNTS

DINING BAR SEATS: 36  
DINING ARCADE SEATS: 26  
BAR SEATS: 31  
PRIVATE SUITE: 10 +/-

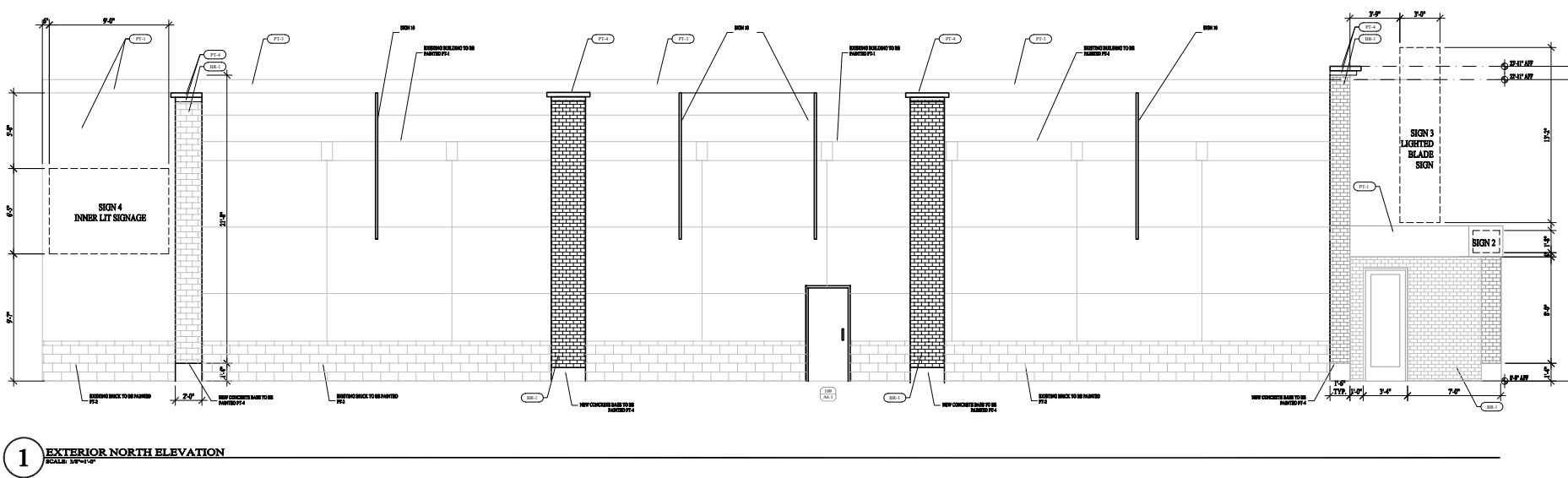
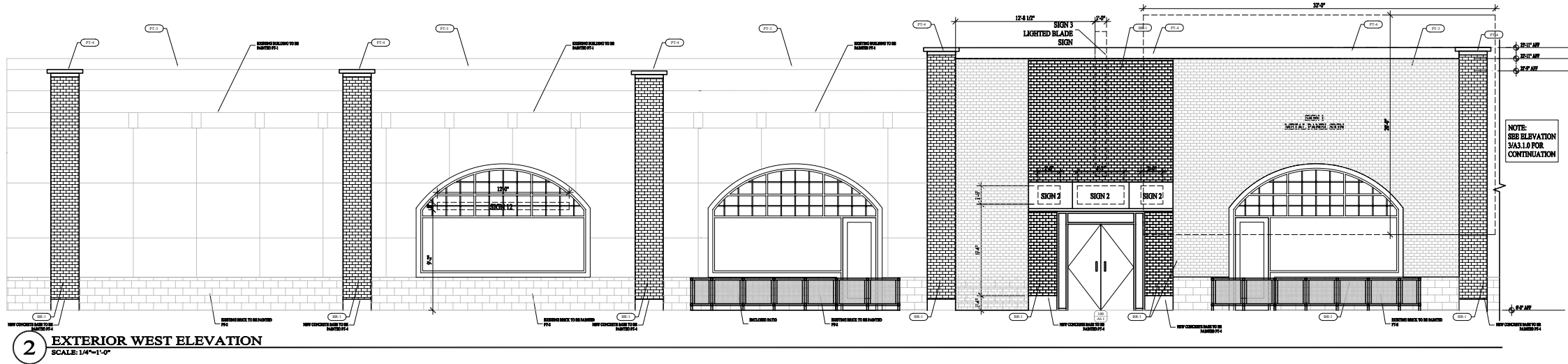
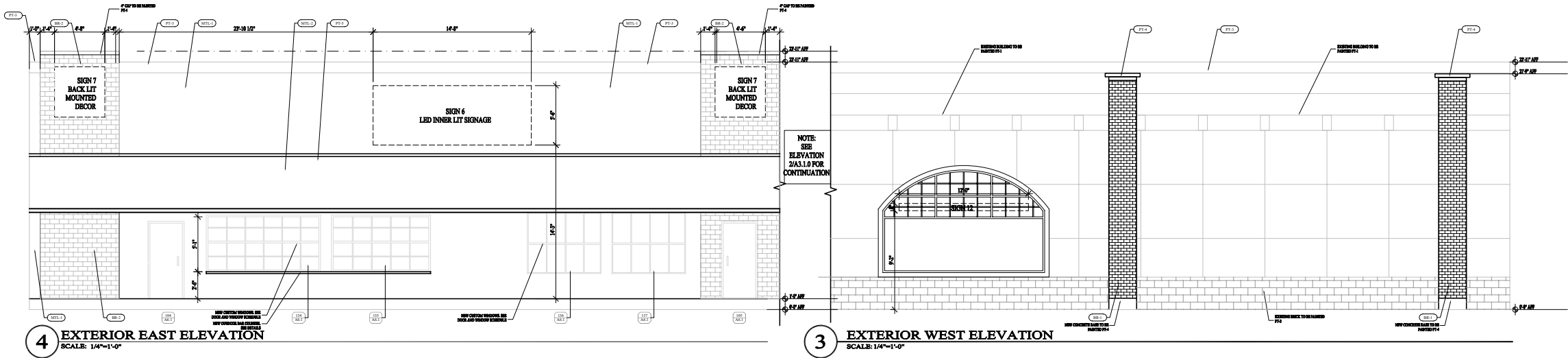
BOWLING LANES: 10 LANES  
INTERIOR SF: 12,556 +/-  
KITCHEN SF: 1,895 +/-  
OUTSIDE COOLER STORAGE: 332 +/-  
**UPSTAIRS STORAGE SF: 1464 +/-**  
**OVERALL SF: 16,247 +/-**

## LORETTA'S LAST CALL COUNTS

DINING SEATS: 104  
BAR 1 SEATS: 21  
BAR 2 SEATS: 10  
PATIO SEATS: 52  
PATIO BAR: 7

INTERIOR SF: 4045 +/-  
DANCE FLOOR SF: 1184 TOTAL  
LINE DANCING SF: 818  
**EXTERIOR PATIO SF: 1298 +/-**  
**OVERALL SF: 5343 +/-**

EXTERIOR ELEVATIONS



a MAVERICK studio

G

DESIGN FIRM:

Gravity Architecture & Design, LLC

733 W Smith Street

Second Floor

Orlando, Florida 32804

ARCHITECT:

Raymond R. Schaefer

License #:

001.015384

Phone #:

407.832.0949

GAME ON

LORETTA'S LAST CALL

77 ROCKINGHAM PARK BLVD

SALEM, HN 03079

| RECORD | DATE       | DESCRIPTION            |
|--------|------------|------------------------|
| 0      | 08/11/2025 | DESIGN INTENT DRAWINGS |

DATE

08/11/2025

SCALE

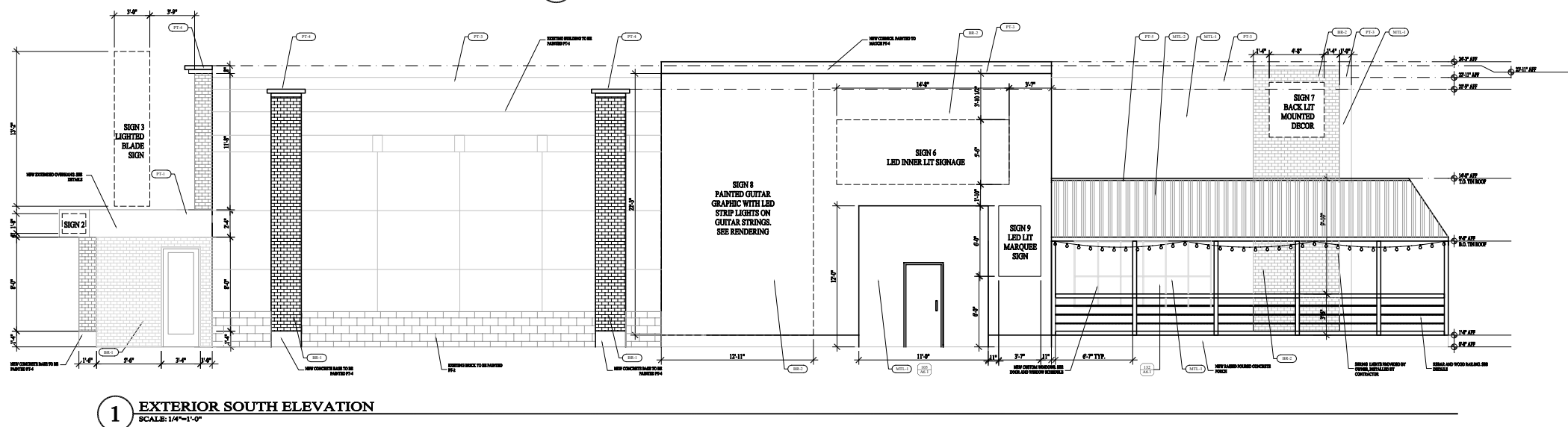
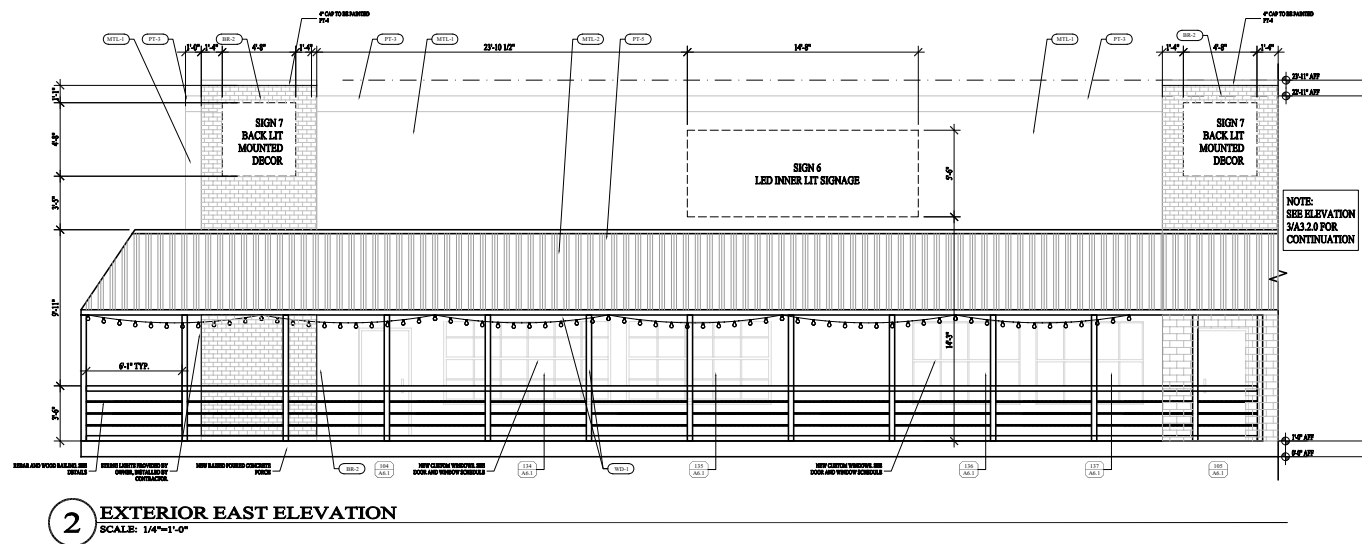
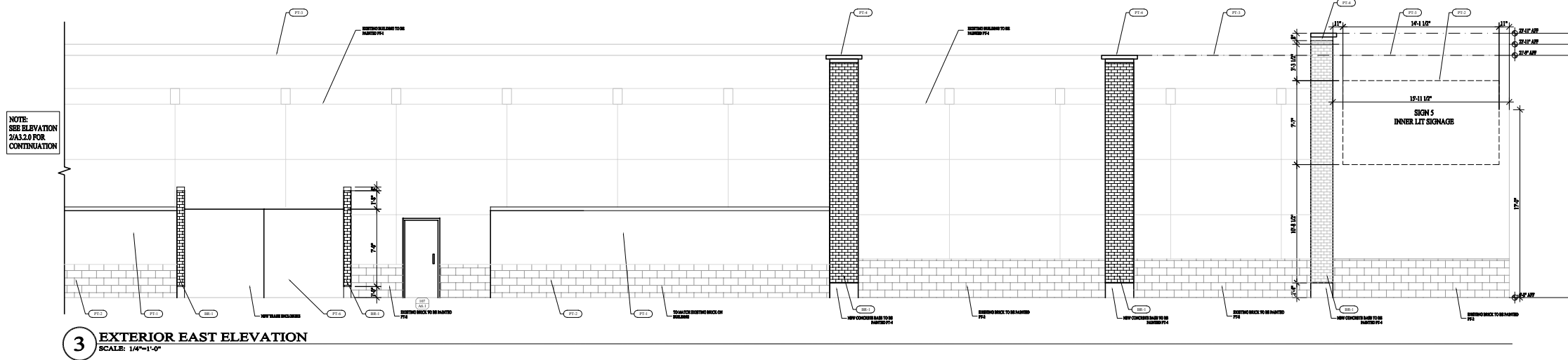
AS INDICATED

Exterior Elevations

A3.1.0

Drawing No.

EXTERIOR ELEVATIONS



DESIGN FIRM:  
Gravity Architecture & Design, LLC  
733 W Smith Street  
Second Floor  
Orlando, Florida 32804  
ARCHITECT:  
Raymond R. Schaefer  
License #: 001.015384  
Phone #: 407.832.0949

GAME ON  
LORETTA'S LAST  
CALL  
77 ROCKINGHAM PARK BLVD  
SALEM, HN 03079

| RECORD                 | DATE |
|------------------------|------|
| DESCRIPTION            |      |
| DESIGN INTENT DRAWINGS |      |
| N.S.                   |      |
| 0                      |      |

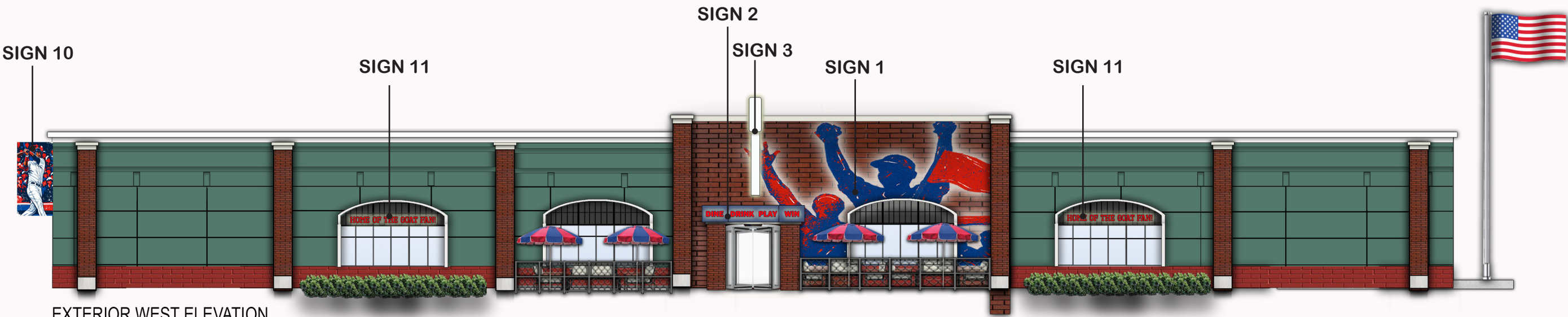
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08/11/2025  
SCALE  
AS INDICATED

Exterior  
Elevations

A3.2.0  
Drawing No.



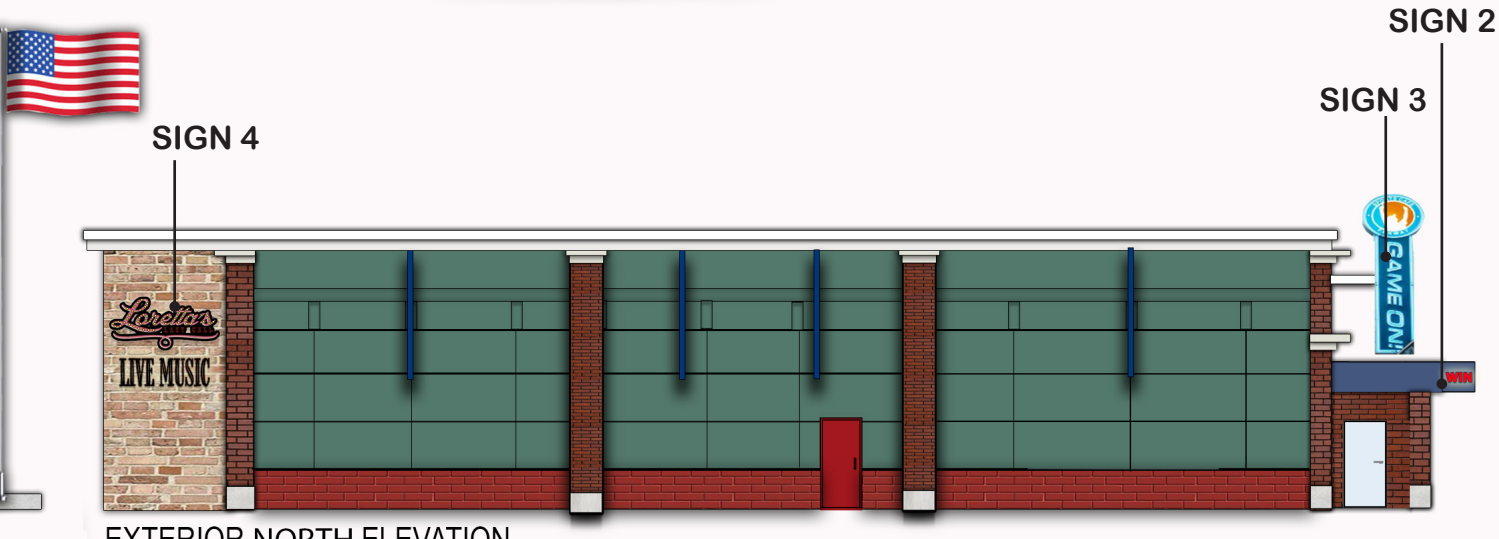
# COLORED EXTERIOR ELEVATIONS



EXTERIOR WEST ELEVATION



EXTERIOR SOUTH ELEVATION



EXTERIOR NORTH ELEVATION



EXTERIOR EAST ELEVATION



# SIGNAGE INFORMATION

SIGN1:



RED: #BA212A R: 186 G:33 B:42



BLUE: #183E6F R: 24 G:62 B:111

MATERIAL: PAINTED WALL GRAPHIC

APPROXIMATE ROUGH DIMENSION:  
15'-11 1/2"W X 17'-0"H

SIGN 2:



RED: #BA212A R: 186 G:33 B:42

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
2'-0" W X 1'-8" H

SIGN 3:



RED: #BA212A R: 186 G:33 B:42



BLUE: #183E6F R: 24 G:62 B:111



WHITE: R: 255 G:255 B:255

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
1'-0"W X 13'-0"H

SIGN 4:



PINK: #D06779 R: 205 G:106 B:124



MAROON: #5F100A R: 95 G:16 B:10

MATERIAL: REVERSE CHANNEL WITH LED  
BULBS AND INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
9'-0"W X 6'-5"H

# SIGNAGE INFORMATION

SIGN 5:



RED: #BA212A R: 186 G:33 B:42



BLUE: #183E6F R: 24 G:62 B:111

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
14'-1" W X 7'-7" H

SIGN 6



PINK: #D06779 R: 205 G:106 B:124

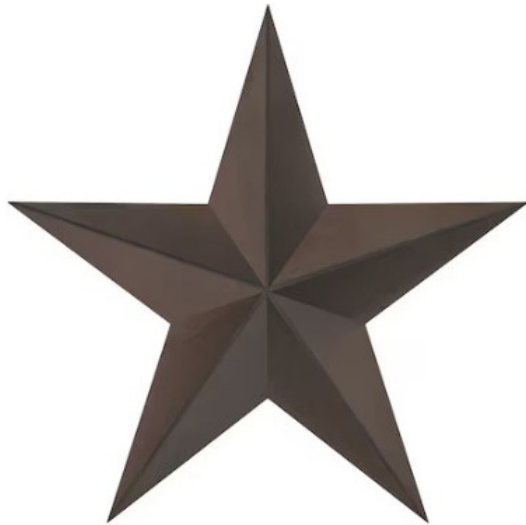


MAROON: #5F100A R: 95 G:16 B:10

MATERIAL: REVERSE CHANNEL WITH LED BUBS

APPROXIMATE ROUGH DIMENSION:  
14'-8"W X 5'-0" H

SIGN 7:



BROWN: #593419  
R: 89 G:52 B:26

MATERIAL: BACKLIT  
ALUMINUM STAR

APPROXIMATE  
ROUGH DIMENSION:  
4'-8" W X 4'-8" H

SIGN 8:



RED: #EE1816 R: 238 G:24 B:22



YELLOW: #E3DA8D R: 227 G:218 B:141



GREEN: #84B99A R: 132 G:185 B:154



PURPLE: #C84BC9 R: 200 G:75 B:201



ORANGE: #D5673D R: 213 G:103 B:61

MATERIAL: CMYK LED STRIPS. COLORS TO  
MATCH DESIGN INTENT IMAGE.

APPROXIMATE ROUGH DIMENSION:  
12'-11"W X 22'-3"H



# SIGNAGE INFORMATION

**SIGN 9:**

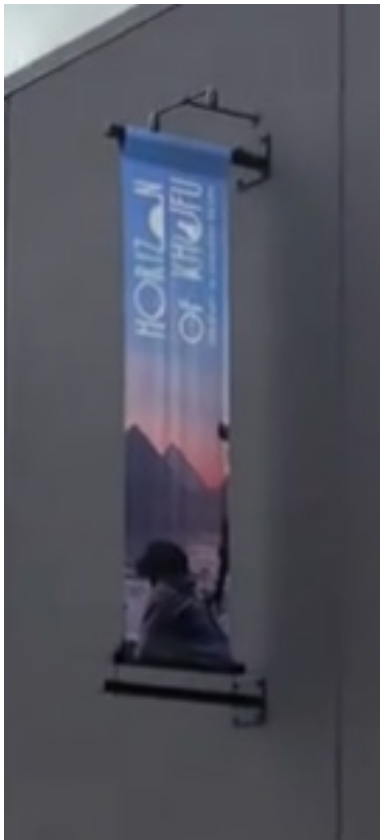


RED: #FE030F R: 254 G:3 B:15

MATERIAL: LED LIGHT BULBS MOUNTED ON ALUMINUM SIGN

APPROXIMATE ROUGH DIMENSION:  
3'-7"W X 6'-0"H

**SIGN 10:**



MATERIAL: WALL MOUNTED BLADE SIGN FARBIC

APPROXIMATE ROUGH DIMENSION:  
4'-0"W X 11'-0" H

# SIGNAGE INFORMATION

SIGN 11:



RED: #BA212A R: 186 G:33 B:42

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
12'-0" W X 8" H



Gravity Architecture & Design, LLC

ENVISION \* ESTABLISH \* ENGAGE

Material/Color Call-outs Salem, NH:

All Colors / Materials to match Fenway Park base colors as well as existing Loretta's brand colors where applicable.

|       |                                                                                      |
|-------|--------------------------------------------------------------------------------------|
| MTL-1 | Loretta's Accent Corrugated Metal Panel. Paint Dark Brown and seal/match brand color |
| MTL-2 | Aged Corrugated Metal Roof. MTL-5 base with art directed aging in field and seal     |
| MTL-5 | Raw Corrugated Metal sheeting. Clear Coat for metal show-thru and seal               |
| BR-1  | Face Brick/Thin Brick. Match Fenway Park Brick Medium Red/Brown                      |
| BR-2  | Face Brick/Thin Brick. Match Loretta's Fenway Interior Brick/light brown             |
| PT-1  | Green Paint – Match Fenway Park standard. Main body of building is existing EIFS     |
| PT-2  | Paint CMU @ trash enclosure to match BR-1                                            |
| PT-3  | White Paint on Metal Cap                                                             |
| PT-4  | White/Gray paint to match concrete color on metal cornice cap. Verify in Field       |
| PT-5  | Dark Brown metal trim. Match WD-1                                                    |
| PT-6  | Green paint to match PT-1 on metal fencing and slats at trash enclosure              |
| WD-1  | Wood Stain/Rustic Oak. Match Loretta's Fenway interior trim                          |

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(603) 953-3164  
eric@northamsurvey.com



**Re: Request for Waiver – Subdivision Regulations §278-4:1.15**

**Applicant:** Jacqui L. Fuschetti  
**Property:** 59 Millville Street, Salem, NH  
**Tax Map:** 81 | **Lot:** 2946  
**Project:** Two-Lot Subdivision (NS Project #1667)

Town of Salem Planning Board  
33 Geremonty Drive  
Salem, NH 03079

Dear Members of the Planning Board:

On behalf of the property owner, Jacqui L. Fuschetti, we respectfully request a waiver from the requirements of Section 278-4:1.15 of the Salem Subdivision Regulations, which requires preparation of a plan for minimizing soil erosion and sedimentation during construction and operation of the proposed development, unless deemed unnecessary by the Board.

The subject property is proposed to be subdivided into two residential lots, constituting a minor subdivision as defined by the Subdivision Regulations. The lots will be served by existing municipal water and sewer, and no new public or private streets, stormwater systems, or other municipal improvements are proposed. The subdivision results in minimal land disturbance beyond that associated with typical single-family residential development.

Further, a variance from the Zoning Ordinance was approved by the Salem Zoning Board of Adjustment on October 7, 2025 (Petition #08-2025-00032), with specific stipulations addressing stormwater and site impacts, including roof runoff infiltration, depiction of existing and proposed utilities, and driveway placement. These conditions ensure that drainage and site impacts are appropriately managed as part of future development.

At this stage, no construction activity is proposed as part of the subdivision approval itself. Detailed erosion control measures are more appropriately addressed during the construction and building permit phase, when final grading, driveway layout, and building footprints are known. Deferring the erosion and sedimentation plan to that phase will allow for a more accurate and effective plan consistent with actual site development.

Northam Survey, LLC  
750 Central Ave Suite N  
Dover, NH 03820  
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Granting this waiver would be consistent with the intent of the Subdivision Regulations, would not be contrary to public health or safety, and would not result in undue impacts to abutting properties or municipal infrastructure. The requested waiver is reasonable given the limited scope of the subdivision and the safeguards already in place.

Accordingly, we respectfully request that the Planning Board grant a waiver from Section 278-4:1.15, allowing erosion and sedimentation controls to be reviewed and approved during the construction phase rather than at the time of subdivision approval.

Thank you for your consideration of this request. Please do not hesitate to contact us should you require additional information.

Sincerely,

A handwritten signature in dark ink, appearing to read "Eric Salovitch", with a stylized flourish extending to the right.

Eric Salovitch, LLS  
*President*



Lot File: P:\1667 Jay Parent\Survey\DWGS\1667 SURVEY.lot

CRD File: P:\1667 Jay Parent\Survey\CRD Files\1667 SURVEY.crd

Lot: 2946 , Block: 1, Type: LOT

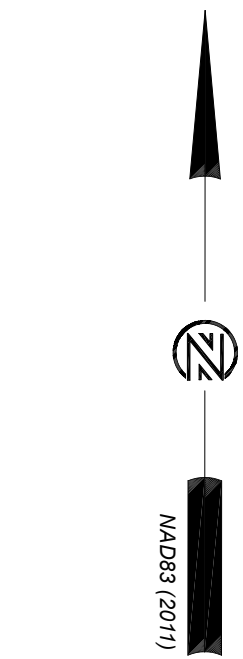
| PNT#                            | Bearing       | Distance   | Northing    | Easting | Station |
|---------------------------------|---------------|------------|-------------|---------|---------|
| 9226                            |               | 105676.606 | 1102354.439 | 0.000   |         |
|                                 | S 11°33'01" E | 81.589     |             |         |         |
| 9232                            |               | 105596.669 | 1102370.776 | 81.589  |         |
|                                 | S 83°00'45" W | 137.688    |             |         |         |
| 9233                            |               | 105579.919 | 1102234.110 | 219.277 |         |
|                                 | N 05°12'08" W | 85.378     |             |         |         |
| 9229                            |               | 105664.946 | 1102226.369 | 304.655 |         |
|                                 | N 84°47'52" E | 128.600    |             |         |         |
| 9226                            |               | 105676.606 | 1102354.439 | 433.255 |         |
| Closure Error Distance> 0.00000 |               |            |             |         |         |
| Total Distance> 433.255         |               |            |             |         |         |
| Area: 11089, 0.2546             |               |            |             |         |         |

Lot: PROPOSED LOT, Block: 1, Type: LOT

| PNT#                            | Bearing       | Distance   | Northing    | Easting | Station |
|---------------------------------|---------------|------------|-------------|---------|---------|
| 9232                            |               | 105596.669 | 1102370.776 | 0.000   |         |
|                                 | S 11°33'01" E | 79.398     |             |         |         |
| 9230                            |               | 105518.879 | 1102386.673 | 79.398  |         |
|                                 | S 84°47'52" W | 146.400    |             |         |         |
| 9231                            |               | 105505.605 | 1102240.876 | 225.798 |         |
|                                 | N 05°12'08" W | 74.622     |             |         |         |
| 9233                            |               | 105579.919 | 1102234.110 | 300.420 |         |
|                                 | N 83°00'45" E | 137.688    |             |         |         |
| 9232                            |               | 105596.669 | 1102370.776 | 438.108 |         |
| Closure Error Distance> 0.00000 |               |            |             |         |         |
| Total Distance> 438.108         |               |            |             |         |         |
| Area: 10911, 0.2505             |               |            |             |         |         |

Block 1 Total Area: 22000, 0.5051





"SOIL INFORMATION IS DEPICTED PER U.S. DEPARTMENT OF AGRICULTURE NATURAL RESOURCES CONSERVATION PUBLISHED 2021. A SITE SPECIFIC SOIL SURVEY WAS NOT CONDUCTED IN PART OF THIS SURVEY."

SOILS:

- 42C CANTON FINE SANDY LOAM, 8 TO 15 PERCENT SLOPES  
12B HINCKLEY LOAMY SAND, 3 TO 8 PERCENT SLOPES

| LEGEND:        |                                     |
|----------------|-------------------------------------|
| MAP 137 LOT 11 | ASSESSORS MAP AND LOT NUMBER        |
| BK, PG.        | BOOK / PAGE                         |
| CONC.          | CONCRETE                            |
| EP             | EDGE OF PAVEMENT                    |
| INV.           | INVERT                              |
| EL.            | ELEVATION                           |
| N/F            | NOW OR FORMERLY                     |
| RCRD           | ROCKINGHAM COUNTY REGISTRY OF DEEDS |
| S.F.           | SQUARE FEET                         |
| SBC            | SLOPED BIT CURB                     |
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| ⊙              | APPROXIMATE ABUTTER BOUNDARY LINE   |
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| ⊙              | EXISTING CONTOUR                    |
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PLAN REFERENCES:

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- "PLAN OF LAND PREPARED FOR MARION M. FOSS LOCATED IN SALEM, NEW HAMPSHIRE" PREPARED BY CHARLES E. CYR CIVIL ENGINEER. DATED MAY 1976. NOT RECORDED.

NOTES:

- SUBJECT PARCEL: TAX MAP 81 LOT 2946  
59 MILLVILLE STREET  
SALEM, NH 03079-2618  
NS PROJECT #1667
- OWNER OF RECORD: JACQUI L. FUSCHETTI  
59 MILLVILLE STREET  
SALEM, NH 03079-2618  
RCRD BOOK 5044, PAGE 2005
- PARCEL AREA: 22,000 S.F. OR 0.51 AC
- THE PURPOSE OF THIS PLAN IS TO SHOW A 2-LOT SUBDIVISION OF THE SUBJECT PARCEL.
- DIMENSIONAL REQUIREMENTS: ZONE: RESIDENTIAL  
MIN LOT AREA: 25,000 S.F. SINGLE/ 37,500 S.F. DUPLEX  
MIN LOT FRONTAGE: 150' SINGLE + 40' PER UNIT /125' CURVED STREET + 40' PER UNIT  
MIN FRONT SETBACK: 30'  
MIN SIDE SETBACK: 15'  
MIN REAR SETBACK: 30'  
MAX COVERAGE: 30%  
MAX BUILDING HEIGHT: 35'
- ZONING INFORMATION SHOWN HEREON IS PER THE TOWN OF SALEM ZONING ORDINANCE LAST REVISED FEBRUARY 26, 2024. ADDITIONAL REGULATIONS APPLY, THE LAND OWNER IS RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE TOWN, STATE, AND FEDERAL REGULATIONS.
- FLOOD HAZARD ZONE: "X" WITH NO BASE FLOOD ELEVATION, PER FIRM MAP #33015C0561E, WITH AN EFFECTIVE DATE OF 05/17/2005.
- THE INTENT OF THIS PLAN IS TO SHOW THE APPROXIMATE LOCATION OF BOUNDARIES FOR SUBDIVISION CONCEPT. IT IS NOT AN ATTEMPT TO DEFINE UNWRITTEN RIGHTS, DETERMINE THE EXTENT OF OWNERSHIP, OR DEFINE THE LIMITS OF TITLE.
- FIELD SURVEY COMPLETED BY NORTHAM SURVEY IN JUNE 2025 USING A TRIMBLE S5 TOTAL STATION WITH A TRIMBLE TSC3 DATA COLLECTOR, A TRIMBLE R12 GPS RECEIVER AND A SOKKIA B31 AUTO LEVEL.
- HORIZONTAL DATUM IS NAD83(2011) NEW HAMPSHIRE STATE PLANE COORDINATES PER STATIC GPS OBSERVATIONS.
- THE VERTICAL DATUM IS NAVD88 PER STATIC GPS OBSERVATIONS. THE CONTOUR INTERVAL IS 2 FEET.
- EASEMENTS, RIGHTS, AND RESTRICTIONS SHOWN OR IDENTIFIED ARE THOSE WHICH WERE FOUND DURING RESEARCH PERFORMED AT THE ROCKINGHAM COUNTY REGISTRY OF DEEDS. OTHER RIGHTS, EASEMENTS, OR RESTRICTIONS MAY EXIST WHICH A TITLE EXAMINATION OF SUBJECT PARCEL(S) WOULD DETERMINE.
- THE LOCATION OF UNDERGROUND UTILITY INFORMATION SHOWN ON THIS PLAN IS APPROXIMATE. NORTHAM SURVEY LLC MAKES NO CLAIM TO THE ACCURACY OR COMPLETENESS OF UNDERGROUND UTILITIES SHOWN. PRIOR TO ANY EXCAVATION ON SITE THE CONTRACTOR SHALL CONTACT DIG SAFE.
- WETLANDS WERE NOT DELINEATED IN PART OF THIS SURVEY. NORTHAM SURVEY RECOMMENDS JURISDICTIONAL WETLANDS ARE DELINEATED BY A CERTIFIED WETLAND SCIENTIST PRIOR TO DEVELOPMENT
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- NEW UTILITY SERVICES WILL BE CONSTRUCTED UNDERGROUND.
- ALL CONSTRUCTION DEBRIS SHALL BE DISPOSED OF IN A MANNER WHICH COMPLIES WITH ALL FEDERAL AND STATE REGULATIONS.
- A VARIANCE FROM ARTICLE III, SECTION 490-301C (1) WAS APPROVED AND GRANTED TO JACQUI FUSCHETTI BY THE SALEM, NH ZONING BOARD OF ADJUSTMENT ON OCTOBER 07, 2025 (PETITION #08-2025-00032) TO ALLOW THIS SUBDIVISION WITH STIPULATIONS AS FOLLOWS:
  - ROOF RUNOFF SHALL BE INFILTRATED.
  - EXISTING WATER AND SEWER MAINS AND PROPOSED WATER AND SEWER SERVICES SHALL BE SHOWN ON PLAN.
  - THE DRIVEWAY SHALL BE LOCATED AS FAR REMOVED FROM THE INTERSECTION AS POSSIBLE.
- A WAIVER REQUEST LETTER HAS BEEN SUBMITTED TO DEFER THE ITEMS TO CONSTRUCTION - PHASE AND FINAL PLANS.

|                                          |           |          |
|------------------------------------------|-----------|----------|
| APPROVED BY THE SALEM PLANNING BOARD ON: |           | DATE     |
| SIGNED BY:                               | POSITION: | POSITION |
| NAME                                     | NAME      | NAME     |
| SIGNED BY:                               | POSITION: | POSITION |
| NAME                                     | NAME      | NAME     |

PURSUANT TO NEW HAMPSHIRE REVISED STATUTES ANNOTATED 676:18, II, III AND IV AND 672:14:

I CERTIFY THAT THIS SURVEY AND PLAN WERE PREPARED BY ME OR THOSE UNDER MY DIRECT SUPERVISION. THIS SURVEY CONFORMS TO THE ACCURACY REQUIREMENTS OF AN URBAN SURVEY OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES OF THE BOARD OF LICENSURE FOR LAND SURVEYORS

I FURTHER CERTIFY THAT THIS SURVEY WAS MADE ON THE GROUND AND IS CORRECT TO THE BEST OF MY PROFESSIONAL KNOWLEDGE. RANDOM TRAVERSE SURVEY BY TOTAL STATION WITH A PRECISION GREATER THAN 1:15,000.



DECEMBER 18, 2025  
DATE

SUBDIVISION PLAN  
FOR  
JACQUI L. FUSCHETTI  
OF  
TAX MAP 81 LOT 2946  
59 MILLVILLE STREET  
SALEM, NEW HAMPSHIRE  
ROCKINGHAM COUNTY

SCALE: 1"=20' (22x34) 1"=40' (11x17)

|             |      |          |                 |
|-------------|------|----------|-----------------|
| JOB NO.     | 1667 | DATE:    | 2025-12-09      |
| DRAWN BY:   | TJR  | DRAWING: | 1667 SURVEY.DWG |
| CHECKED BY: | EJS  | SHEET:   | 1 OF 1          |

|     |      |             |    |
|-----|------|-------------|----|
|     |      |             |    |
|     |      |             |    |
|     |      |             |    |
| NO. | DATE | DESCRIPTION | BY |



Northam Survey, LLC  
750 Central Ave Suite N  
Dover, NH 03820  
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**Re: Request for Waiver – Subdivision Regulations §278-4:1.13**

**Applicant:** Jacqui L. Fuschetti  
**Property:** 59 Millville Street, Salem, NH  
**Tax Map:** 81 | **Lot:** 2946  
**Project:** Two-Lot Subdivision (NS Project #1667)

Town of Salem Planning Board  
33 Geremonty Drive  
Salem, NH 03079

Dear Members of the Planning Board:

On behalf of the property owner, Jacqui L. Fuschetti, we respectfully request a waiver from the requirements of Section 278-4:1.13 of the Salem Subdivision Regulations, pursuant to Article 7, Section 278-7:1 (Waivers). Section 278-4:1.13 requires that the minimum basement floor elevation for each lot be shown on the subdivision plan.

Article 7 authorizes the Planning Board to grant waivers where strict compliance with the Subdivision Regulations would result in unnecessary hardship or where such waiver would not be contrary to the intent of the Regulations or the public interest. The requested waiver meets these criteria for the reasons described below.

The proposed project consists of a two-lot residential subdivision, classified as a minor subdivision. At this time, no building or foundation design plans have been prepared for either proposed lot. As a result, establishing minimum basement floor elevations at the subdivision stage would be premature and speculative.

Final basement floor elevations are more appropriately determined during the building design and permitting phase, when detailed grading plans, foundation designs, utility connections, and drainage considerations are fully developed and reviewed by the appropriate Town departments. Deferring this requirement will allow for a more accurate and coordinated review that reflects the actual proposed construction.

Granting this waiver will not be contrary to the intent of the Subdivision Regulations and will not adversely affect public health, safety, or welfare. Compliance with flood hazard, drainage, and building code requirements will continue to be reviewed and enforced during the construction and building permit phase.

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750 Central Ave Suite N  
Dover, NH 03820  
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Accordingly, we respectfully request that the Planning Board grant a waiver from Section 278-4:1.13 in accordance with Article 7 of the Subdivision Regulations, allowing minimum basement floor elevations to be reviewed and approved during the design and construction permitting phase rather than at the time of subdivision approval.

Thank you for your consideration. Please feel free to contact us should you require additional information.

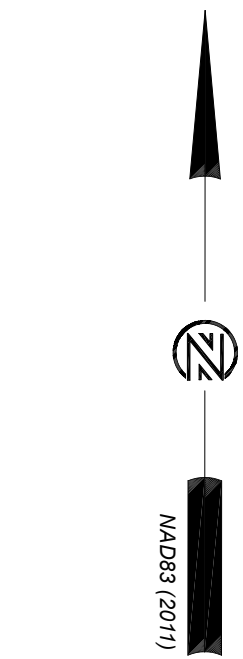
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Eric Salovitch, LLS

*President*

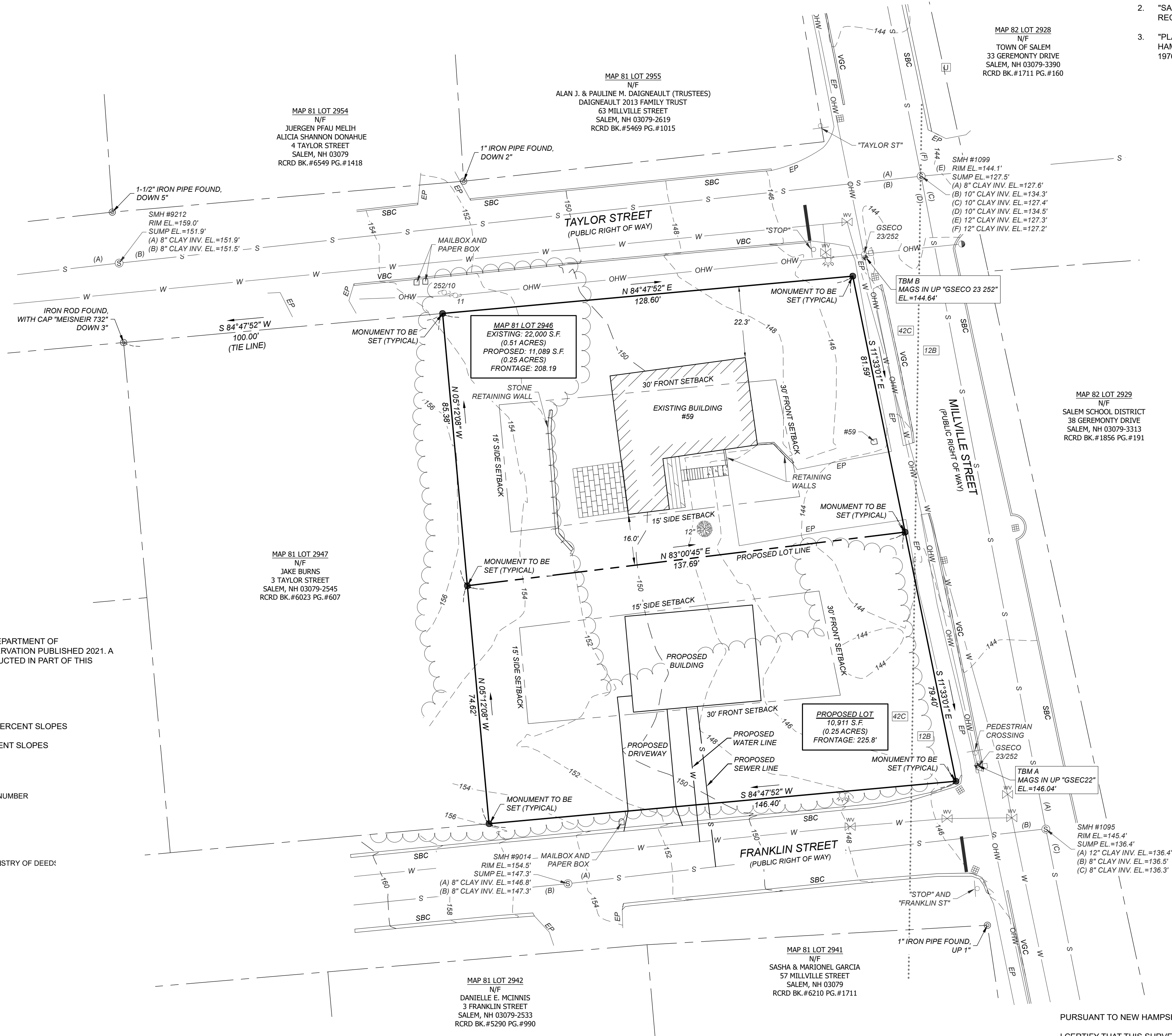




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| LEGEND:        |                                     |
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| APPROVED BY THE SALEM PLANNING BOARD ON: |          |
|------------------------------------------|----------|
| SIGNED BY:                               | DATE     |
| NAME                                     | POSITION |
| SIGNED BY:                               | DATE     |
| NAME                                     | POSITION |

### SUBDIVISION PLAN FOR JACQUI L. FUSCHETTI OF TAX MAP 81 LOT 2946 59 MILLVILLE STREET SALEM, NEW HAMPSHIRE ROCKINGHAM COUNTY

SCALE: 1"=20' (22x34) 1"=40' (11x17)

|             |      |          |                 |
|-------------|------|----------|-----------------|
| JOB NO.     | 1667 | DATE:    | 2025-12-09      |
| DRAWN BY:   | TJR  | DRAWING: | 1667 SURVEY.DWG |
| CHECKED BY: | EJS  | SHEET:   | 1 OF 1          |

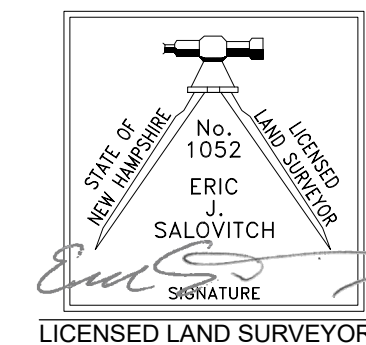
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|-----|------|-------------|----|
|     |      |             |    |
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|     |      |             |    |
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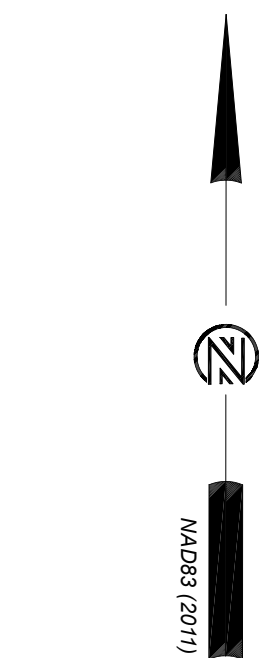
SALEM PLANNING BOARD  
APPROVAL



DECEMBER 18, 2025  
DATE





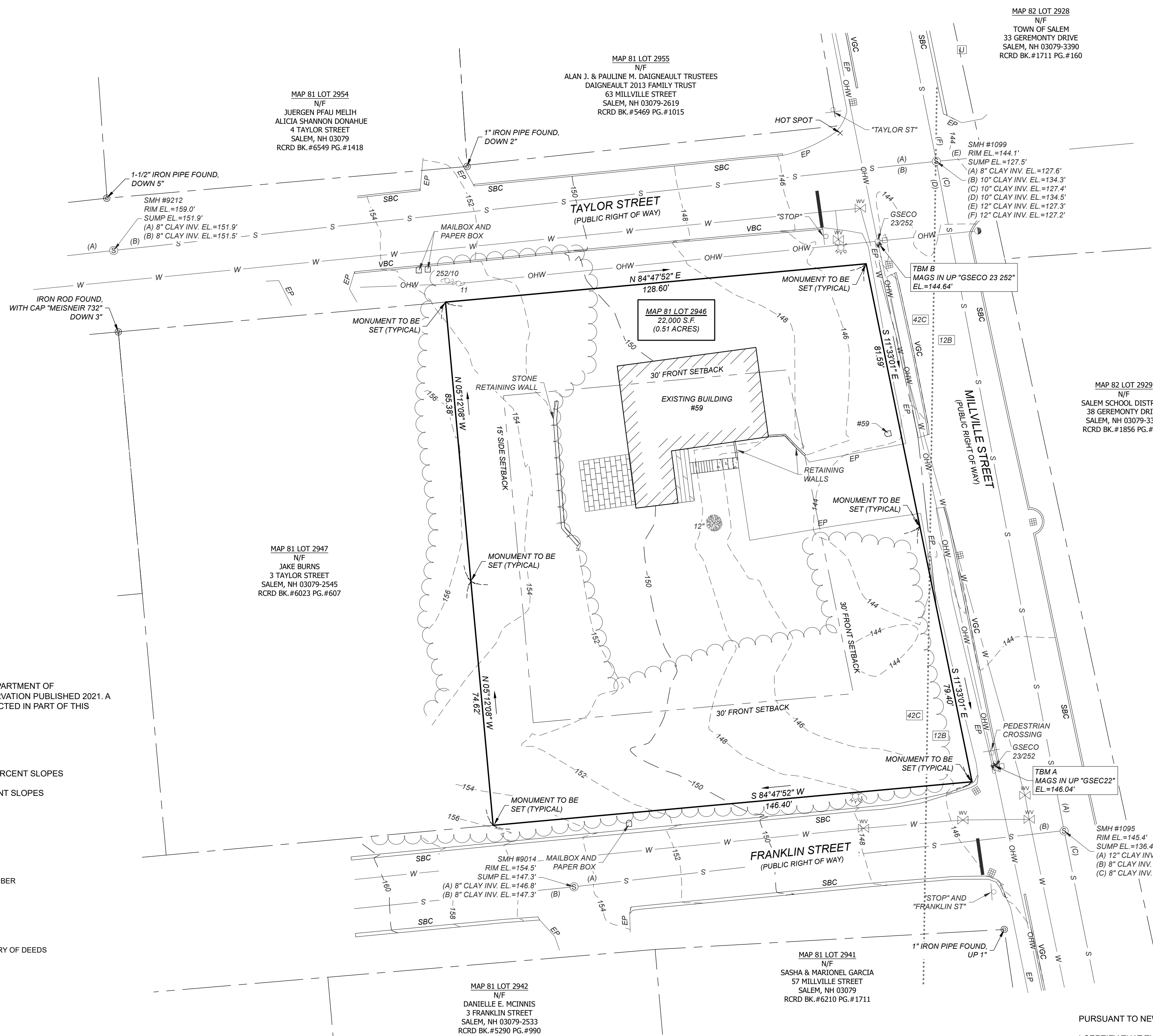


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| EL.            | ELEVATION                           |
| N/F            | NOW OR FORMERLY                     |
| RCRD           | ROCKINGHAM COUNTY REGISTRY OF DEEDS |
| S.F.           | SQUARE FEET                         |
| SBC            | SLOPED BIT CURB                     |
| TBM            | TEMPORARY BENCHMARK                 |
| VBC            | VERTICAL BIT CURB                   |
| VGC            | VERTICAL GRANITE CURB               |
| ○              | IRON PIPE/ROD FOUND                 |
| ○              | MONUMENT TO BE SET                  |
| ○              | GUY POLE                            |
| ○              | GUY WIRE                            |
| ○              | UTILITY POLE                        |
| ○              | UTILITY POLE W/LIGHT                |
| ○              | UTILITY BOX                         |
| ○              | CATCH BASIN                         |
| ○              | MAILBOX                             |
| ○              | DECIDUOUS TREE                      |
| ○              | SEWER MANHOLE                       |
| ○              | HYDRANT                             |
| ○              | WATER GATE VALVE                    |
| ○              | SIGN                                |
| ○              | OVERHEAD WIRE                       |
| ○              | TREE LINE                           |
| S              | SEWER LINE                          |
| W              | WATER LINE                          |
| PL             | APPROXIMATE BOUNDARY LINE           |
| ---            | APPROXIMATE ABUTTER BOUNDARY LINE   |
| ---            | SETBACK LINE                        |
| ---            | EXISTING CONTOUR                    |
| CONCRETE       |                                     |



PURSUANT TO NEW HAMPSHIRE RSA 676:18 III

I CERTIFY THAT THIS SURVEY PLAT IS NOT A SUBDIVISION PURSUANT TO THIS TITLE AND THAT THE LINES OF STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC OR PRIVATE STREETS OR WAYS ALREADY ESTABLISHED AND THAT NO NEW WAYS ARE SHOWN.

I CERTIFY THAT THIS SURVEY AND PLAN WERE PREPARED BY ME OR THOSE UNDER MY DIRECT SUPERVISION. THIS SURVEY CONFORMS TO THE ACCURACY REQUIREMENTS OF AN URBAN SURVEY OF THE NEW HAMPSHIRE CODE OF ADMINISTRATIVE RULES OF THE BOARD OF LICENSURE FOR LAND SURVEYORS. I FURTHER CERTIFY THAT THIS SURVEY WAS MADE ON THE GROUND AND IS CORRECT TO THE BEST OF MY PROFESSIONAL KNOWLEDGE. RANDOM TRAVERSE SURVEY BY TOTAL STATION WITH A PRECISION GREATER THAN 1:15,000.



NOVEMBER 24, 2025  
DATE

EXISTING CONDITIONS  
FOR  
JACQUI L. FUSCHETTI  
OF  
TAX MAP 81 LOT 2946  
59 MILLVILLE STREET  
SALEM, NEW HAMPSHIRE  
ROCKINGHAM COUNTY

SCALE: 1"=20' (22x34) 1"=40' (11x17)

|             |      |          |                 |
|-------------|------|----------|-----------------|
| JOB NO.     | 1667 | DATE:    | 2025-11-24      |
| DRAWN BY:   | EEC  | DRAWING: | 1667 SURVEY.DWG |
| CHECKED BY: | EJS  | SHEET:   | 1 OF 1          |

|     |      |             |    |
|-----|------|-------------|----|
|     |      |             |    |
|     |      |             |    |
|     |      |             |    |
|     |      |             |    |
| NO. | DATE | DESCRIPTION | BY |



#### PLAN REFERENCES:

- "SALEM HILL PARK SALEM, N.H. OWNED BY HOWARD L. GORDON" DATED 1923. RECORDED AT THE R.C.R.D. AS PLAN #213
- "SALEM HILL PARK SALEM, N.H. OWNED BY HOWARD L. GORDON" DATED 1923. RECORDED AT THE R.C.R.D. AS PLAN #284
- "PLAN OF LAND PREPARED FOR MARION M. FOSS LOCATED IN SALEM, NEW HAMPSHIRE" PREPARED BY CHARLES E. CYR CIVIL ENGINEER, DATED MAY 1976. NOT RECORDED.

#### NOTES:

- SUBJECT PARCEL: TAX MAP 81 LOT 2946  
59 MILLVILLE STREET  
SALEM, NH 03079-2618  
NS PROJECT #1667
- OWNER OF RECORD: JACQUI L. FUSCHETTI  
59 MILLVILLE STREET  
SALEM, NH 03079-2618  
R.C.R.D. BOOK 5044, PAGE 2005
- PARCEL AREA: 21780 S.F. OR 0.51 AC
- THE PURPOSE OF THIS PLAN IS TO SHOW EXISTING CONDITIONS OF THE SUBJECT PARCEL.
- DIMENSIONAL REQUIREMENTS:**  
MIN LOT AREA: 25,000 S.F. SINGLE/ 37,500 S.F. DUPLEX  
MIN LOT FRONTAGE: 150' SINGLE + 40' PER UNIT / 125' CURVED STREET + 40' PER UNIT  
MIN FRONT SETBACK: 30'  
MIN SIDE SETBACK: 15'  
MIN REAR SETBACK: 30'  
MAX BUILDING HEIGHT: 35'  
  
ZONING INFORMATION SHOWN HEREON IS PER THE TOWN OF SALEM ZONING ORDINANCE LAST REVISED FEBRUARY 26, 2024. ADDITIONAL REGULATIONS APPLY, THE LAND OWNER IS RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE TOWN, STATE, AND FEDERAL REGULATIONS.
- FLOOD HAZARD ZONE: "X" WITH NO BASE FLOOD ELEVATION, PER FIRM MAP #33015C0561E, WITH AN EFFECTIVE DATE OF 5/17/2005.
- THE INTENT OF THIS PLAN IS TO SHOW THE LOCATION OF BOUNDARIES IN ACCORDANCE WITH THE CURRENT LEGAL DESCRIPTIONS. IT IS NOT AN ATTEMPT TO DEFINE UNWRITTEN RIGHTS, DETERMINE THE EXTENT OF OWNERSHIP, OR DEFINE THE LIMITS OF TITLE.
- FIELD SURVEY COMPLETED BY NORTHAM SURVEY IN JUNE 2025 USING A TRIMBLE S5 TOTAL STATION WITH A TRIMBLE TSC3 DATA COLLECTOR, A TRIMBLE R12 GPS RECEIVER AND A SOKKIA B31 AUTO LEVEL.
- HORIZONTAL DATUM IS NAD83(2011) NEW HAMPSHIRE STATE PLANE COORDINATES PER STATIC GPS OBSERVATIONS.
- THE VERTICAL DATUM IS NAVD88 PER STATIC GPS OBSERVATIONS. THE CONTOUR INTERVAL IS 2 FEET.
- EASEMENTS, RIGHTS, AND RESTRICTIONS SHOWN OR IDENTIFIED ARE THOSE WHICH WERE FOUND DURING RESEARCH PERFORMED AT THE ROCKINGHAM COUNTY REGISTRY OF DEEDS. OTHER RIGHTS, EASEMENTS, OR RESTRICTIONS MAY EXIST WHICH A TITLE EXAMINATION OF SUBJECT PARCEL(S) WOULD DETERMINE.
- THE LOCATION OF UNDERGROUND UTILITY INFORMATION SHOWN ON THIS PLAN IS APPROXIMATE. NORTHAM SURVEY LLC MAKES NO CLAIM TO THE ACCURACY OR COMPLETENESS OF UNDERGROUND UTILITIES SHOWN. PRIOR TO ANY EXCAVATION ON SITE THE CONTRACTOR SHALL CONTACT DIG SAFE.
- WETLANDS WERE NOT DELINEATED IN PART OF THIS SURVEY. NORTHAM SURVEY RECOMMENDS JURISDICTIONAL WETLANDS ARE DELINEATED BY A CERTIFIED WETLAND SCIENTIST PRIOR TO DEVELOPMENT