

## 1. AGENDA

Documents:

[PB AGENDA-01-13-26.PDF](#)

## 2. 11 Mall Road - Game On & Loretta's Last Call

Documents:

[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-RTC-STAFF.PDF](#)  
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-RTC-TRAFFIC.PDF](#)  
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-SITE PLAN.PDF](#)  
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-TRAFFIC AND PARKING UTILIZATION STUDY.PDF](#)  
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-CUP-SIGNAGE.PDF](#)  
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-ELEVATIONS AND SIGNAGE PACKAGE.PDF](#)  
[11 MALL ROAD-GAME ON AND LORETTAS LAST CALL-ELEVATIONS LEGEND.PDF](#)

## 3. 59 Millville Street - Subdivision

Documents:

[59 MILLVILLE STREET-WAIVER REQUEST.PDF](#)  
[59 MILLVILLE STREET-WAIVER REQUEST-MINIMUM BASEMENT FLOOR ELEVATION.PDF](#)  
[59 MILLVILLE STREET-SUBDIVISION PLAN.PDF](#)



# Town of Salem, New Hampshire

Community Development Department - Planning Division  
33 Geremonty Drive, Salem, New Hampshire 03079  
(603) 890-2080 - Fax (603) 898-1223

## **PLANNING BOARD AGENDA FOR JANUARY 13, 2026 - 7:00 PM**

Knightly Meeting Room, Salem Town Hall

### **ROLL CALL:**

<input type="checkbox"/> Joseph Feole, Chair	<input type="checkbox"/> Bianca Carlson	<input type="checkbox"/> Beverly Donovan, Alt.
<input type="checkbox"/> Sean Lewis, Vice Chair	<input type="checkbox"/> Jim Harvey	<input type="checkbox"/> Victor Helenic, Alt.
<input type="checkbox"/> Damon Norcross, Secretary	<input type="checkbox"/> Chuck Saba	
<input type="checkbox"/> Jeff Hatch, Town Council Rep.		

### **REVIEW OF MINUTES**

1. December 9, 2025 – Regular Meeting

### **OLD BUSINESS**

1. **Salem NIM, LLC Site Plan** – Public Hearing for change of use for 19,960 sq. ft. restaurant and entertainment use with conditional use permit for reduced parking and additional signage at 11 Mall Road, Map 107, Lot 11154. Continued from 12/9/2025.

### **BUSINESS**

1. **Canobie Lake Toyota Site Plan** – Public Hearing for conditional use permit for additional wall signage at 417 South Broadway, Map 144, Lot 204.
2. **Fuschetti Subdivision Plan** – Preliminary Hearing for two-lot subdivision at 59 Millville Street, Map 81, Lot 2946.
3. **Suraj Realty, LLC Site Plan** – Public Hearing for amended site plan and phasing scheduled for 6,000 sq. ft. office, vehicle maintenance and dry ice manufacturing building, 20,000 sq. ft. warehouse/storage building, and paved tractor trailer storage area with conditional use permit for reduced wetland setback at 44 Pelham Road, Map 96, Lot 7775.

### **PUBLIC MATTERS**

1. Unscheduled

### **PLANNING BOARD MATTERS**

1. Proposed Zoning Amendments
2. Correspondence
3. Other

Note: No new agenda items will be heard after 10:30 PM. If necessary, the Board will recess to another date to take up the remainder of the agenda.

Per Section 13.7 of the Planning Board Rules of Procedure, all discussions between Planning Board members and applicants or their agents regarding matters to be decided by the Board shall take place at public hearings as part of the public record.

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100 Cambridge Street, Suite 1310 Boston, MA 02114 T: 617.824.9100 F: 617.824.9101

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**To:** Jacob LaFontaine

**From:** Richard Schlichting

**Date:** December 29, 2025

**Re:** Exterior design  
11 Mall Road-Loretta's Last Call & Game On!  
Langan Project No.: 151064501

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Below please find our responses comments discussed in our December 9 2025 meeting and at the PB meeting. Below please find each comment followed by our response in **bold**.

- Elevations:
  - Add legend – materials and color

**COMMENT RESPONSE:** Additional page has been added to the plans with the legend.
  - See [Design Guidelines](#) for Façade Design – pages 30-33
    - “For retail structures, any facade that faces a public or private street should have display windows, entry areas, or other transparent features along 40% or more of its horizontal length... Facades should not extend for more than 50 horizontal feet in length without incorporating architectural features, such as pilasters, windows, cornices, porches, corners, or offsets.”
  - **COMMENT RESPONSE:** This concern was addressed by adding the large window on either side of the entrance as well as two additional large windows beyond those two windows that are decorative.
  - Please see staff comments: “elevation drawings provided: proposing to retain significant portion of existing façade and paint to match Fenway Park motif – existing brick to be painted red, existing concrete wall to be painted green, cornice to be painted black; adding new Game On entryway on west side of building with brick columns, - have eliminated awning, patio enclosed by black steel fence, new windows and new projection on façade for signage; Loretta's Last Call to have western motif – rusted tin roof panel, catenary lights, new windows, brick façade entrance, new patio enclosed with rebar and wood railing

fence; adding new brick for equipment storage and dumpster enclosure. Proposed modifications are attractive and welcomed improvements however building has blank walls on large portions of western, northern and eastern facades and eliminated transparency on western facade. Elevations should still be revised to better comply with Site Plan Regulations and Design Guidelines (see excerpts attached) – specifically to reduce blank walls, increase transparency, vary roofline; dumpster enclosure should be revised to match building.”

**COMMENT RESPONSE:** This concern was addressed by adding the large window on either side of the entrance as well as two additional large windows beyond those two windows that are decorative. In addition, we have added banners on the North side of the building as well as an additional pilaster on the east face behind the equipment enclosure.

- Signage:

- Add true sign dimensions instead of “rough dimensions”

**COMMENT RESPONSE:** Sizes are true sizes. The sizes have been called out on the Signage Package, on the Elevations and in the Signage CUP.

- Revise CUP to include sign dimensions and quantity of signage – please find example attached

**COMMENT RESPONSE:** The Signage CUP has been updated to note all dimensions, locations and quantities of each sign.

- Please try to conform with the [Tuscan Village Sign Standards](#) which allow for more flexibility – One of your Kings locations is actually used as an example for a tagline sign (see page 15)
  - Please also refer to Design Guidelines – see pages 81-92 for Signage

**COMMENT RESPONSE:** The new signage plan and elevations has been discussed and worked through with the Planning Director to ensure the signage plan is matching the aesthetic of the area and following guidelines.

- Site Plan:

- Add pedestrian connection to north sidewalk

**COMMENT RESPONSE:** Curb ramp with crosswalk has been added to the plans. Please see CS101.

- Add tip down at ADA spaces to ped. connection to south

**COMMENT RESPONSE:** Tip down to ADA spaces has been added to south sidewalk connection, as seen on CS101.

- Add dimension of striped space to south

**COMMENT RESPONSE:** Dimensions have been added to restriped southern parking area, seen on CS101.

- Clarify square footage: 19,775 sf or 19,960?

**COMMENT RESPONSE:** Square footage has been updated to 19,960 SF.

- Note height of flagpole (max 80ft)

**COMMENT RESPONSE:** Exact flagpole height to be coordinated as construction plans progress. Flagpole will be less than 80 feet.

- Eliminate zoning table on Existing Conditions Plan

**COMMENT RESPONSE:** Zoning Table has been removed from VT901.

- Clarify materials and colors of dumpster enclosure – site plan and elevations conflict

- Please note Design Guidelines state “Structural screens and fencing should complement the design of the main structure by repetition of materials, detailing, scale, and color.”

**COMMENT RESPONSE:** The equipment enclosure will mimic the building with the Brick wainscoting and Fenway green. The garbage enclosure will be black chainlink fence with black slats to hide the dumpster.

- Show existing landscaping in addition to proposed – Design Guidelines state “parking lots with 10 or more spaces should have at least one tree per eight spaces, planted in or within five feet of the lot... Larger and more visible parking lots should have more intensive landscape treatments.”
  - Please note any dead, diseased, dying or missing vegetation will be replaced

**COMMENT RESPONSE:** Regarding the landscaping comments for the Lyons Group project, we would like to clarify that the proposed design actually decreases the total parking count by 16 spaces. This reduction means the project is well within the required thresholds; however, we are still proposing additional trees beyond the required one per eight spaces minimum.

Furthermore, we are not removing any existing trees, and the project remains in full compliance with the Town's Design Guidelines.

# MEMO

December 29, 2025

Page 4 of 4

- PB Comments:
  - Enhance elevations and landscaping

COMMENT RESPONSE: Updated elevations and plans have been attached to this application.
  - Confirm whether overflow employee parking to north is available for use
  - Address parking agreement with Mall

COMMENT RESPONSE: Exhibit "B" Site Plan from 11/09/2023 has been attached to this comment response showing shared parking areas in the parking lot.
  - Connect sidewalk to pedestrian connection to Pleasant Street

COMMENT RESPONSE: Southern sidewalk now has a connection to the parking lot. This sidewalk connects to a crosswalk to cross at the intersection where they can head north to other pathing.
  - Submit truck turn plan for trash pickup

COMMENT RESPONSE: Truck turning plan has been added to the set on TM101.

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December 29, 2025

Mr. Stephen G. Pernaw, P.E.  
Stephen G. Pernaw & Company, Inc.  
P.O. Box 1721  
Concord, NH 03302

**Re: Proposed Reuse of Former Sears Auto Center  
Langan Project #151064501**

Dear Mr. LaFontaine:

Enclosed please find our responses to your letter dated December 19, 2025. Below please find each comment followed by our response in **bold**.

1. It is not clear if the outdoor seating is included in the 337 seat total, as this could affect all study projections.

**COMMENT RESPONSE:** *Outdoor seating is included in the 337 total seat count. A clarification has been added to the project description.*

2. ITE description for LUC 932 does not necessarily reflect bowling lanes, pool tables, arcades, and a dancing venue...using "seats" rather than the "gross floor area" as the independent variable produced the lowest estimates...we believe it is prudent to rely on the higher GFA-based estimates for planning purposes (Estimate C).

**COMMENT RESPONSE:** *We first attempted to use LUC 437 (Bowling Alley) to produce a parking demand rate but found that the land use generates very low parking demand. LUC 932 was determined to be the best fit for this development, as both "Game On!" and "Loretta's Last Call" will serve food and have table seating for customers. "Seats" was chosen to be the independent variable, as "Game On!" has large spaces that are not occupiable such as the 10 proposed bowling lanes within the establishment. Using square footage for the independent variable in this situation would produce overly conservative parking demand estimates, and it is our opinion that Alternate Estimate C is an overestimation.*

3. The results reflect average parking demand rather than the 85th percentile parking demand

**COMMENT RESPONSE:** *The report has been amended to include both average and 85<sup>th</sup> (peak) parking demand presented in Tables 1 and 2.*

Key Finding 1: *"Existing parking demand in Parking Lot C peaks at 28% utilization on Friday and 27% on Saturday in the evenings."*

SGP Comment: This finding pertains to July 2025 conditions, and does not reflect peak-month conditions.

**COMMENT RESPONSE:** *Confirmed, no comment.*

Key Finding 2: "The combination of existing parking utilization and anticipated parking demand would be less than 50% utilization of Parking Lot C during off-peak season."

SGP Comment: According to the Alternative Estimate C, the utilization of Lot C could be on the order of 65% during July conditions.

**COMMENT RESPONSE: Using 85<sup>th</sup> percentile parking demand, the utilization of Lot C is estimated to be 53%. This is matches with Alternative Estimate A. The independent variable of seats was maintained due to the reasoning outlined in the response for Comment #2.**

Key Finding 3: "The Mall will require an amended Conditional Use Permit (CUP) to reduce the required parking to 4,563 to account for the removal of 16 parking spaces for the proposed restaurants."

SGP Comment: We do not have sufficient documentation to verify that the required parking is 4,563 spaces. However, the removal of 16 parking spaces for the proposed restaurants is considered de minimis.

**COMMENT RESPONSE: Comment acknowledged.**

Key Finding 4: "The estimated trip generation for the proposed restaurants is 26 less evening peak hour trips and 549 daily trips less than the previous proposals."

SGP Comment: The alternative trip estimates for the PM peak hour case range from 155 to 179 trips. When compared to the previous proposal estimate of 158 trips, it is reasonable to expect anything from a minor decrease to a small increase in trips. The weekday daily estimate on Table 2 (1,473 trips) should be used with caution, as this estimate is based on only one ITE data point. The weekday daily estimate for the two restaurants based on the 12<sup>th</sup> Edition (and the total GFA) results in an estimate of 2,023 trips (5 ITE data points). According to Table 2, the previously approved restaurant/retail uses generated 2,022 trips; essentially a "wash;" not a reduction.

**COMMENT RESPONSE: As reasoned in the response for Comment #2, it is our opinion that "seats" is a more appropriate independent variable to use for this development due to the large unoccupiable space taken up by the proposed bowling lanes. Therefore, we believe that the proposed development is expected to generate less peak hour and daily trips.**

CUP Request – It is our understanding that the request is for the removal of 16 parking spaces for the proposed restaurant project. Given that the required parking supply for the Mall exceeds four thousand spaces, 16 fewer spaces is considered de minimis.

**COMMENT RESPONSE: Comment acknowledged.**

Site Development Plan – A vehicle tracking analysis should be prepared that demonstrates: 1) compatibility with Salem fire apparatus, and 2) how the dumpsters will be serviced. According to Sheet CS101, 68 spaces are proposed within the subject site. It should be noted that the peak parking demand estimates range from 158 to 395 occupied spaces. This means that there is the

potential for the entire C1 parking section (with 373 net spaces) to be fully occupied during peak periods.

**COMMENT RESPONSE: Vehicle turning movement demonstrations are provided on Sheets TM101 and TM102. The 85<sup>th</sup> percentile (peak) parking estimates for Loretta's and Game On! are 266 spaces, which is 71% of the available parking spaces within Lot C1.**

Traffic Impact Fee – The Applicant should submit an impact fee calculation for Town review based on the current version of the “*Impact Fees For New Development*” schedule. The current rate for high-turnover restaurants is \$5.89 per sf. The impact fee calculation should reflect a “credit” for the former use (auto center).

**COMMENT RESPONSE: See the attached impact fee calculation.**

Sincerely,  
**Langan Engineering and Environmental Services, Inc.**



Christopher McLean  
Project Engineer

CM/jsp

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<b>Impact Fee Calculation Table</b>					
	<b>Existing</b>		<b>Proposed</b>		
	Square Footage	\$/SF	Square Footage	\$/SF	
Sears Auto Center (Small Retail)	19,900	\$ 5.57	-		
Loretta's Last Call & Game On! (High-Turnover Sit Down Restaurant)	-		19,900	\$ 5.89	
Estimated Impact Fee	\$ 110,843.00		\$ 117,211.00		
<b>New Impact Fee</b>	<b>\$ 6,368.00</b>				

# LYONS GROUP RESTAURANT AT THE MALL AT ROCKINGHAM PARK PLANNING BOARD SUBMISSION

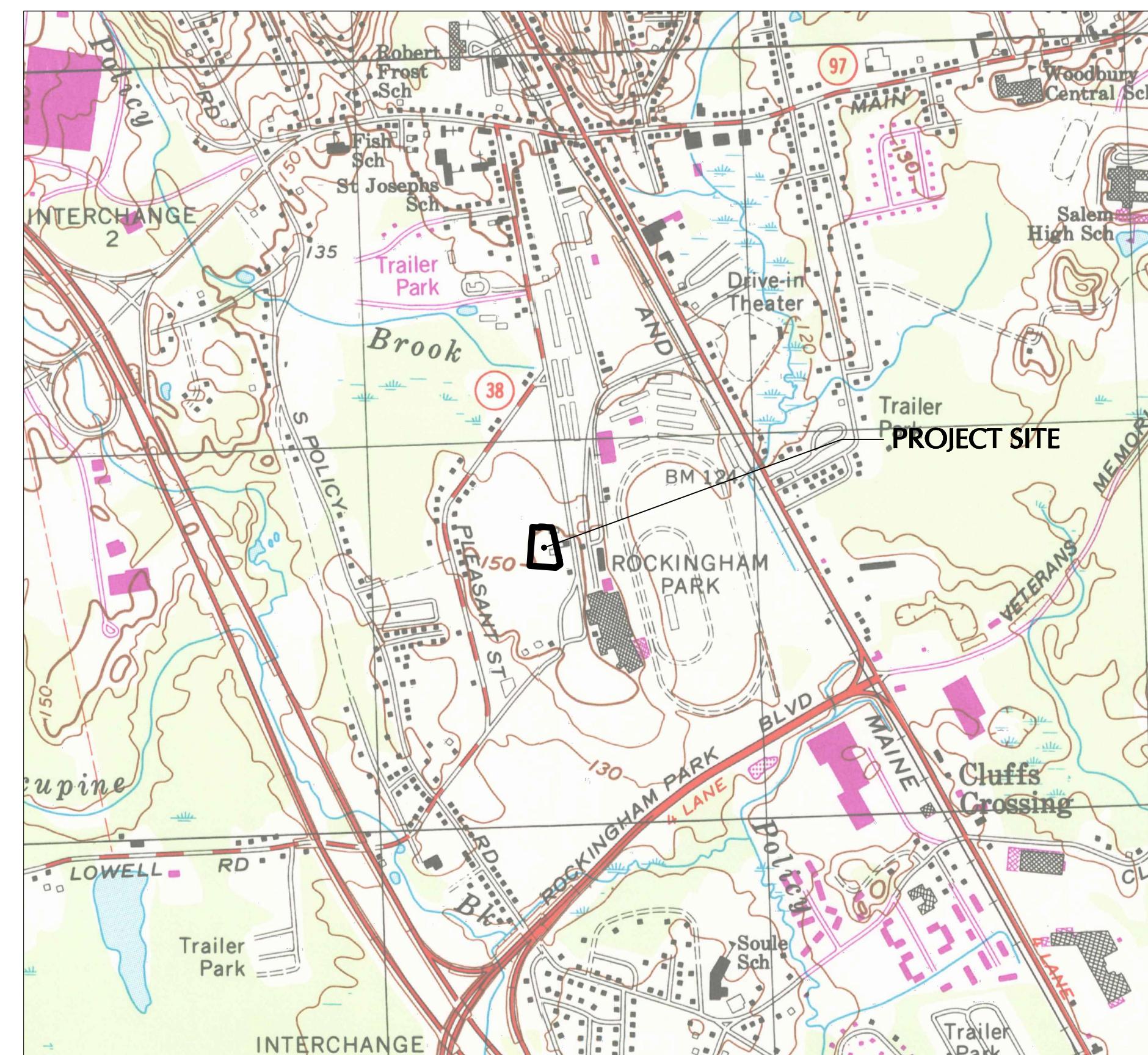
MAP: 107 LOT: 11154

1 MALL ROAD

TOWN OF SALEM, ROCKINGHAM COUNTY, NEW HAMPSHIRE

## CIVIL DRAWING INDEX

SHEET NUMBER	DRAWING TITLE	DATE	LAST REVISED
CS001	SITE COVER SHEET	11/18/2025	12/29/2025
CS002	MASTER LEGEND & NOTES	11/18/2025	-
CS003	AREA MAP	11/18/2025	-
VT901	EXISTING CONDITIONS SURVEY	10/23/2025	-
CD101	SITE PREPARATION & DEMOLITION PLAN	11/18/2025	-
CS101	EXTERIOR SITE PLAN	11/18/2025	12/29/2025
CS501	SITE DETAILS I	11/18/2025	-
CS502	SITE DETAILS II	11/18/2025	-
TM101	GARBAGE TRUCK MOVEMENT PLAN	12/29/2025	-
TM102	FIRE TRUCK MOVEMENT PLAN	12/29/2025	-
CG101	GRADING & DRAINAGE PLAN	11/18/2025	12/29/2025
CU101	UTILITY PLAN	11/18/2025	-
CU501	UTILITY DETAILS	11/18/2025	-
LL101	SITE LIGHTING PLAN	11/18/2025	-
LL501	SITE LIGHTING DETAILS	11/18/2025	-
LP101	PLANTING PLAN	11/18/2025	-
LP501	PLANTING DETAILS	11/18/2025	-



MAP REFERENCE: USGS SALEM QUADRANGLE (7.5-MINUTE SERIES)

## LOCATION MAP

SCALE: 1" = 1000'

**OWNER / APPLICANT**  
SALEM NIM, LLC  
C/O: LYONS GROUP  
355 CONGRESS STREET  
BOSTON, MA 02210  
PHONE: 617-262-2605

**ARCHITECT**  
GRAVITY ARCHITECTURE & DESIGN, LLC  
C/O: RAYMOND R SCHAEFER  
777 WEST SMITH STREET  
ORLANDO, FL 32804  
PHONE: 407-832-0949

**CIVIL ENGINEER, LAND SURVEYOR, TRAFFIC  
ENGINEER & LANDSCAPE ARCHITECT**  
LANGAN ENGINEERING & ENVIRONMENTAL  
SERVICES, INC  
C/O: RICHARD SCHLICHTING  
100 CAMBRIDGE STREET  
BOSTON, MA 02114  
PHONE: 617-824-9100

CS001	SITE COVER SHEET
Site Development Plan For: LYONS GROUP RESTAURANT AT THE MALL AT ROCKINGHAM PARK 11 MALL ROAD SALEM, NH	
Assessors Map 107 & Lot 11154	
Prepared by:	Langan Engineering & Environmental Services, LLC 100 Cambridge Street, Suite 1310 Boston, MA 02114
Scale:	As noted
October 30, 2025	
 Owner: SALEM NIM, LLC C/O LYONS GROUP 355 CONGRESS ST BOSTON, MA 02210 Salem Planning Board Approval	
Zoning Classification: COMMERCIAL-INDUSTRIAL DISTRICT, SUBDISTRICT C (CI-C)	

**LANGAN**

GENERAL NOTES	
1. PLANIMETRIC & TOPOGRAPHIC INFORMATION SHOWN HEREON HAS BEEN OBTAINED FROM GROUND SURVEYS PERFORMED BY ODONE SURVEY & MAPPING IN DECEMBER 2014 AND LAGAN CT, INC IN JUNE 2016 AND SUPPLEMENTED BY FURTHER GROUND SURVEYS PERFORMED BY LANGAN ENGINEERING & ENVIRONMENTAL SERVICES, INC IN MAY 2023.	11. ALL UTILITY WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS AND SPECIFICATIONS/DETAILS OF THE UTILITY COMPANY HAVING AUTHORITY OVER THE PROPOSED WORK. ALL PROPOSED UTILITY WORK SHALL BE PERFORMED IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL ORDINANCES/REQUIREMENTS GOVERNING THE PROPOSED WORK.
2. THE SITE LIES IN ZONE X (UNSHADED) AS SHOWN ON THE "FLOOD INSURANCE RATE MAP" ROCKINGHAM COUNTY, NEW HAMPSHIRE, FEMA MAP NUMBERS 330150563, EFFECTIVE MAY 17, 2005.	12. ANY UTILITY EASEMENTS REQUIRED BY ANY OF THE VARIOUS UTILITY COMPANIES SHALL BE OBTAINED, EXECUTED, AND RECORDED PRIOR TO ANY OF THE AFFECTED UTILITY WORK BEING PERFORMED.
3. PROPOSED SITE WORK IMPROVEMENTS SHALL CONFORM TO ANY STANDARD DETAILS AND SPECIFICATIONS OF THE TOWN OF SALEM. IN THE ABSENCE OF LOCAL STANDARDS, SITE WORK SHALL CONFORM TO THE REQUIREMENTS OF NEW HAMPSHIRE DOT STANDARD DETAILS.	13. ALL PROPOSED UTILITIES WILL BE LOCATED UNDERGROUND.
4. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING DIG SAFE (WWW.DIGSAFE.COM), EXCAVATION TEST (TEST-LOC.COM), AND OTHER LOCAL UTILITY COMPANIES FOR FIELD LOCATIONS NECESSARY TO PROTECT AND MAINTAIN ALL EXISTING UTILITIES TO REMAIN THROUGHOUT THE CONSTRUCTION PERIOD. ANY CONFLICTS BETWEEN EXISTING UTILITIES AND PROPOSED UTILITIES DISCOVERED DURING CONSTRUCTION SHALL BE PROMPTLY REPORTED TO THE PROJECT ENGINEER.	14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESETTING ALL EXISTING SANITARY AND DRAINAGE STRUCTURES AS A RESULT OF ANY UTILITY WORK TO NEW HAMPSHIRE STATE STANDARDS AND AS REQUIRED BY REPAIRING, MILLING, OR OVERLAYING.
5. SEE ARCHITECTURAL DRAWINGS FOR LOCATIONS, DIMENSIONS, AND DETAILS OF ALL DOORS, AND WALLS ASSOCIATED WITH THE BUILDING.	15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PAVEMENT REPAIRS REQUIRED AS A RESULT OF ANY UTILITY WORK.
6. FOR AREAS OUTSIDE THE PROPERTY LINES, REPAIR AND/OR REPLACE ALL DAMAGE DONE TO EXISTING ELEMENTS (SIDEWALKS, PAVING, LANDSCAPING, ETC) AS REQUIRED BY OWNER AND/OR GOVERNING AUTHORITY.	16.1. PAVEMENT MARKINGS SHALL BE WHITE ON ASPHALT AND YELLOW ON CONCRETE. 16.2. PAVEMENT MARKINGS PAINT SHALL BE FAST DRYING WATERBORNE TRAFFIC PAINT MEETING THE REQUIREMENTS OF TT-P-1952F TYPE II. PAINT SHALL BE APPLIED AS SPECIFIED BY MANUFACTURER. 16.3. ALL PAVEMENT STRIPES SHALL BE 4-INCHES WIDE UNLESS OTHERWISE NOTED.
7. ALL SIGNS AND PAVEMENT MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE MUTCD AND NEW HAMPSHIRE DEPARTMENT OF TRANSPORTATION REGULATIONS.	17. CONCRETE JOINTS ON SITE ARE TO BE FILLED WITH HOT-APPLIED JOINT FILLER, TO INCLUDE CONCRETE PAVEMENTS, MONOLITHIC CURBING AND MONOLITHIC SIDEWALKS.
8. CONTRACTOR SHALL PREVENT DUST, SEGMENT AND DEBRIS FROM EXITING THE SITE AND SHALL BE RESPONSIBLE FOR CLEAN-UP, REPAIR AND FORCING REOPENING SUCH LOCATIONS, ADJOINING STREETS AND PROPERTIES TO BE KEPT FREE OF DEBRIS RESULTING FROM DEMOLITION AND SHALL BE CLEANED ON A DAILY BASIS OR AS NEEDED.	18. SPOT SHOTS ALONG CURB LINES REPRESENT THE BASE OF THE CURB UNLESS NOTED OTHERWISE.
9. DUST CONTROL TREATMENTS SHALL BE APPLIED AS NECESSARY TO CONTROL AND REDUCE THE AMOUNT OF DUST WHICH MAY CAUSE OFF-SITE DAMAGE, BE A HEALTH HAZARD TO HUMANS, WILDLIFE AND PLANT LIFE, OR POSE A HAZARD TO TRAFFIC SAFETY.	19. ALL ON-SITE CONCRETE TO BE 4,500 PSI WITH 5% TO 7% AIR ENTRAPMENT UNLESS OTHERWISE NOTED.
10. ABBREVIATIONS:	R.O.W. = RIGHT OF WAY CONC. = CONCRETE INV. = INVERT EX- EXISTING HDPE = HIGH DENSITY POLYETHYLENE PIPE PROP. = PROPOSED L.F. = LENGTH RL = ROOF LEADER TC = TOP OF CURB BC = BOTTOM OF CURB TW = TOP OF WALL BW = BOTTOM OF WALL HP = HIGHPOINT L.S. = LANDSCAPED AREA N.T.S. = NOT TO SCALE INV. = INVERT SMH = SANITARY SEWER MANHOLE FFE = FINISHED FLOOR ELEVATION FES = FLARED END SECTION

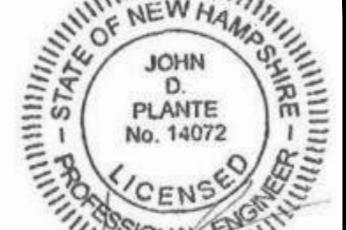
CONTRACTOR NOTES	
1. THESE PLANS REPRESENT THE OVERALL SITWORK IMPROVEMENTS REQUIRED FOR PROJECT CONSTRUCTION. THE CONTRACTOR SHALL FURNISH, INSTALL, TEST AND COMPLETE ALL WORK TO THE SATISFACTION OF THE OWNER AND OWNER'S ENGINEER IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR MEANS AND METHODS OF CONSTRUCTION; AS SUCH, THESE PLANS DO NOT COMPLETELY REPRESENT, NOR ARE THEY INTENDED TO REPRESENT, ALL SPECIFIC INSTRUCTIONS REQUIRED FOR SITWORK CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE TO CONSTRUCT ALL IMPROVEMENTS DEPICTED ON THESE PLANS IN ACCORDANCE WITH ALL APPLICABLE RULES, REGULATIONS AND LAWS IN EFFECT AT THE TIME OF CONSTRUCTION.	
2. THE CONTRACTOR SHALL ACCEPT THE SITE AS IS. THE CONTRACTOR SHALL ASSESS CONDITIONS, AND THE KIND, QUALITY AND QUANTITY OF WORK REQUIRED. THE OWNER AND ENGINEER MAKES NO GUARANTEES REGARD TO THE ACCURACY OF THE INFORMATION THAT WAS PROVIDED. THE CONTRACTOR SHALL MAKE A THOROUGH SITE INSPECTION IN ORDER TOIELD CONDITIONS, CORRELATE CONDITIONS, CORRELATE CONDITIONS WITH THE DRAWINGS AND RESOLVE ANY POSSIBLE CONSTRUCTION CONFLICTS WITH THE OWNER AND ENGINEER PRIOR TO COMMENCEMENT OF WORK. THE CONTRACTOR SHALL PERFORM ADDITIONAL TOPOGRAPHIC SURVEYS HE/SHE DEEMS NECESSARY, PROVIDED THEY ARE COORDINATED WITH THE OWNER AND ENGINEER PRIOR TO THE START OF WORK. THE CONTRACTOR THAT DIFFER FROM THE INFORMATION SHOWN ON THE DRAWINGS THAT ARE NOT BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER PRIOR TO THE START OF WORK SHALL NOT BE CONSIDERED GROUNDS FOR ADDITIONAL PAYMENT OR CHANGES TO THE CONTRACT DURATION, OR ANY OTHER CLAIMS AGAINST THE OWNER OR OWNER'S ENGINEER.	
3. THE CONTRACTOR SHALL, WHEN HE/SHE DEEMS NECESSARY, PROVIDE A WRITTEN REQUESTS FOR INFORMATION (RFI) TO THE OWNER AND/OR OWNER'S DESIGNATED REPRESENTATIVE, AND ENGINEER PRIOR TO THE CONSTRUCTION OF ANY SPECIFIC SITWORK ITEM. (THE RFI) SHALL BE IN A FORM ACCEPTABLE TO OWNER AND/OR OWNER'S DESIGNATED REPRESENTATIVE, AND ENGINEER AND SHALL ALLOW FOR A MINIMUM OF THREE WORK DAYS FOR A WRITTEN REPLY. RFIS SHALL BE NUMBERED CONSECUTIVELY BY DATE SUBMITTED. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR SITWORK ITEMS CONSTRUCTED DIFFERENTLY THAN INTENDED OR AS DEPICTED ON THE PLANS.	
4. INFORMATION RELATED TO ELEVATIONS AND PROPOSED UTILITIES (SUCH AS ROADWAY GRADES, INVERT ELEVATIONS, RIM ELEVATIONS, GRATE ELEVATIONS, BUILDING FINISHED FLOOR ELEVATIONS, ETC.) MAY BE FOUND IN MORE THAN ONE LOCATION IN THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL SUFFICIENTLY REVIEW ALL PLANS, PROFILES AND ANY OTHER INFORMATION IN THE CONTRACT DOCUMENTS FOR CONSISTENCY PRIOR TO BID. ANY INCONSISTENCIES OR DISCREPANCIES THAT ARE FOUND BY THE CONTRACTOR OR HIS ASSIGNS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER IN WRITING, IN THE FORMAT OF AN RFI PRIOR TO BID.	
5. THERE ARE ADDITIONAL NOTES, SPECIFICATIONS AND REQUIREMENTS CONTAINED THROUGHOUT THE PLAN SET AS WELL AS REFERENCES TO SPECIFICATIONS FROM APPLICABLE GOVERNING AUTHORITIES AND INDUSTRY STANDARDS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN, REVIEW AND ADHERE TO ALL THESE DOCUMENTS.	
6. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT ALL CONSTRUCTION STAKEOUT FOR THIS PROJECT MUST BE COMPLETED FROM THE SITE-SPECIFIC SURVEY CONTROL (HORIZONTAL AND VERTICAL) UPON WHICH THIS DESIGN IS BASED. THE CONTRACTOR SHOULD NOT RELY ON OR RE-ESTABLISH SURVEY CONTROL BY GPS OR OTHER METHODS FOR USE IN CONSTRUCTION STAKEOUT OR ANY OTHER PURPOSE FOR THIS PROJECT. ANY DISCREPANCIES BETWEEN THE EXISTING HORIZONTAL OR VERTICAL DATA SHOWN ON THESE DRAWINGS AND THAT ENCOUNTERED IN THE FIELD MUST BE REPORTED TO THE DESIGN TEAM PRIOR TO CONSTRUCTION FOR RESOLUTION.	

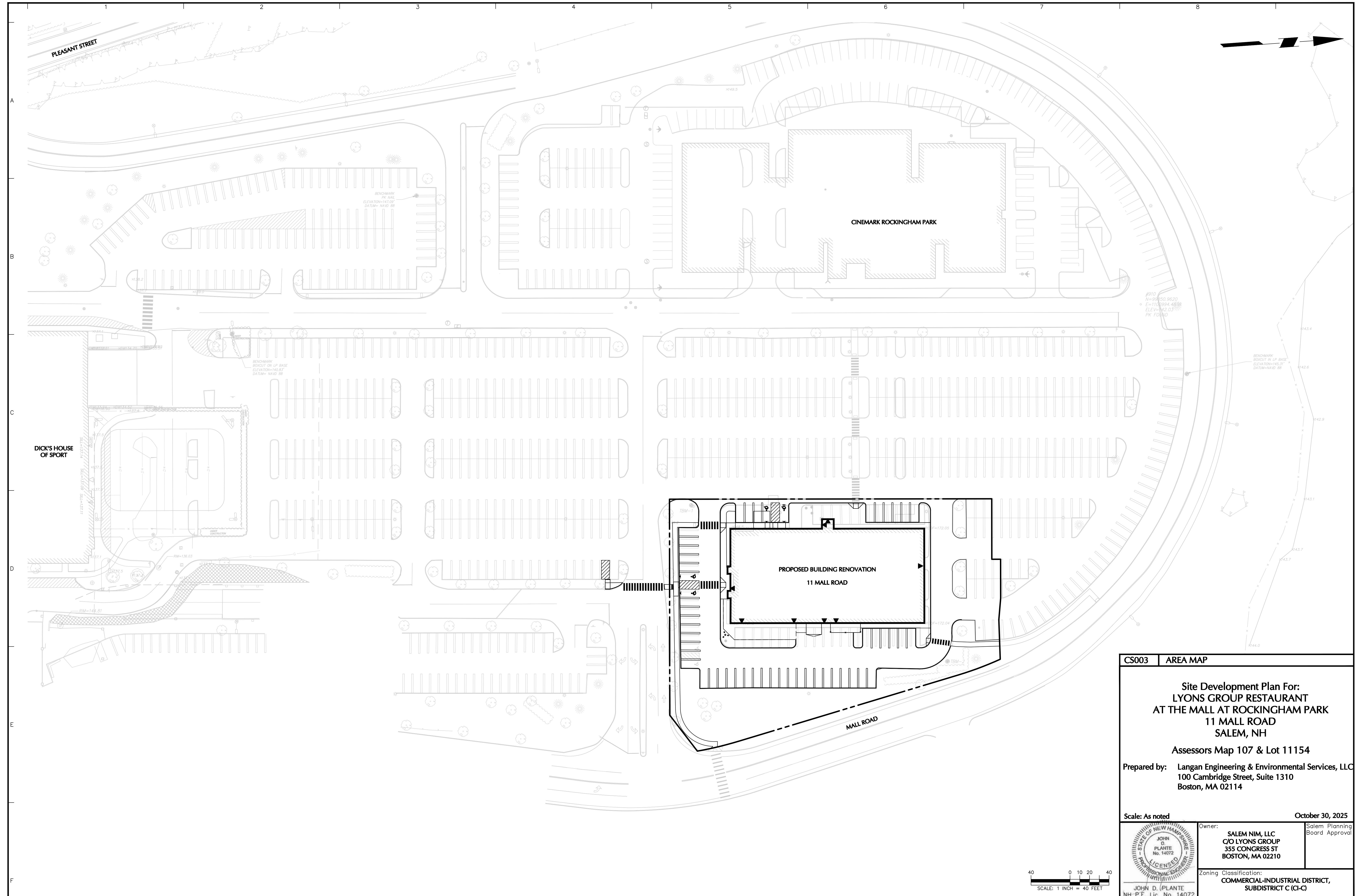
DEMOLITION NOTES	
GENERAL:	1. THE CONTRACTOR SHALL INSPECT THE SITE THOROUGHLY AND FIELD-VERIFY THE LOCATION AND DEPTH OF ALL EXISTING UTILITIES. SOME UNDERGROUND UTILITIES MAY BE LOCATED DIFFERENTLY THAN SHOWN ON THIS PLAN. IN ADDITION, THERE MAY BE EXISTING UNDERGROUND UTILITIES PRESENT THAT ARE NOT SHOWN ON THIS PLAN. ALL CONTRACTORS USING THESE PLANS FOR DEMOLITION OR CONSTRUCTION SHALL CONFIRM ALL UTILITIES IN THE FIELD TO ESTABLISH LOCATIONS, SIZES, MATERIALS, AND ELEVATIONS. ANY DISCREPANCIES SHALL IMMEDIATELY BE BROUGHT TO THE ATTENTION OF THE ENGINEER AND THE CONSTRUCTION MANAGER.
2.	THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING DIG SAFE, EXCAVATING THE HOLES, AND PREPARING ANY ADDITIONAL RESEARCH OR INVESTIGATIONS REQUIRED IN ORDER TO ENSURE THAT PROPOSED UTILITIES TO REMAIN ARE PROTECTED FROM DAMAGE DURING DEMOLITION. ANY DAMAGE TO EXISTING UTILITIES TO REMAIN IS REQUIRED TO BE REPAIRED BY THE CONTRACTOR.
3.	THE CONTRACTOR SHALL NOTIFY AND OBTAIN ALL SHUTOFFS FOR ALL APPLICABLE UTILITIES PRIOR TO THE COMMENCEMENT OF DEMOLITION.
4.	THE CONTRACTOR SHALL LOCATE/CONFIRM ALL DRAINAGE INFRASTRUCTURE AND MAINTAIN ADEQUATE STORM DRAINAGE THROUGHOUT CONSTRUCTION.
5.	THE CONTRACTOR SHALL FIELD LOCATE AND PROPERLY DISCONNECT APPROPRIATE LATERALS TO LIVE MAINS PRIOR TO DEMOLITION AND IN ACCORDANCE WITH UTILITY COMPANY STANDARDS.
6.	THE CONTRACTOR SHALL MAINTAIN THE SAFETY AND PROTECTION OF BOTH PEDESTRIANS AND MOTORISTS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE ALL NECESSARY PRECAUTIONARY AND PROTECTIVE MEASURES, INCLUDING SIGNS, LIGHTS, BARRIERS, FENCING AND TRAFFIC FLAGGING.
7.	THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY DEMOLITION ASSOCIATED PERMITS.
8.	CONTRACTOR TO PROTECT AND MAINTAIN ALL UTILITIES TO REMAIN UNTIL TEMPORARY OR PERMANENT NEW LINES ARE INSTALLED.
DEMOLITION:	9. THE CONTRACTOR SHALL REMOVE AND LEGALLY DISPOSE OF ALL STRUCTURES WITHIN THE PROJECT LIMITS INDICATED TO BE REMOVED.
10.	STUMPS SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH NEW HAMPSHIRE DEPARTMENT OF ENVIRONMENTAL SERVICES STANDARDS. PRIOR TOWN APPROVAL IS REQUIRED FOR ANY ON-SITE STUMP BURIALS. STUMPS CANNOT BE BURIED OR OTHERWISE DISPOSED OF WITHIN THE TOWN OWNED RIGHT-OF-WAY.
11.	THE CONTRACTOR SHALL INCLUDE IN HIS BID ALL TEMPORARY FACILITIES AND SERVICES NECESSARY TO SATISFY FEDERAL, STATE, & LOCAL REQUIREMENTS INCLUDING BUT NOT LIMITED TO BRACING, SHORING, PAVEMENT REPAIR, FENCING, PEDESTRIAN AND VEHICLE ACCESS, CONCRETE PADS, ETC.

ACCESSIBILITY NOTES	
1.	WALKWAYS ALONG ACCESSIBLE ROUTES NOT TO EXCEED 5% RUNNING SLOPE AND 2% CROSS SLOPE.
2.	ALL ADA PARKING SPACES AND PASSENGER LOADING ZONES SHALL NOT EXCEED 2% IN ANY DIRECTION.
3.	ALL ADA EGESSES SHALL BE FLUSH WITH ADJOINING MATERIAL UNLESS NOTED OTHERWISE.
4.	ALL RAMPS SHALL NOT EXCEED 1:12 AND 2% CROSS SLOPE.
5.	CONTRACTOR SHALL CONFIRM FIELD CONDITIONS ARE CONSISTENT WITH CONSTRUCTION DOCUMENTS AND NOTIFY LANGAN ENGINEERING & ENVIRONMENTAL SERVICES, INC. OF ANY COMPLIANCE ISSUES PRIOR TO CONSTRUCTION.
6.	PROPOSED SITE WORK IMPROVEMENTS SHALL CONFORM WITH THE LATEST EDITION OF THE ADA STANDARDS FOR ACCESSIBLE DESIGN TO THE BEST OF THE DESIGNER'S KNOWLEDGE.

GRADING & DRAINAGE NOTES	
1.	ALL PROPOSED STORM DRAINAGE PIPING TO UTILIZE WATER-TIGHT JOINTS.
2.	LOCATIONS AND ELEVATIONS OF ROOF LEADERS SHOULD BE COORDINATED WITH ARCHITECTURAL & MEP DRAWINGS PRIOR TO CONSTRUCTION.
3.	CLEANOUTS SHALL BE PROVIDED FLUSH TO GRADE AT ALL LOCATIONS OF ROOF DRAIN INTERSECTIONS, BENDS AND UPSTREAM ENDS.
4.	ALL REQUIRED STORM LATERALS SERVICING THE BUILDINGS SHALL BE COORDINATED AND CONSTRUCTED TO WITHIN FIVE FEET OF EACH BUILDING LATERAL ENTRANCE LOCATION AT THE INVERTS NOTED. ANY NECESSARY EXTENSIONS, RELOCATIONS, OR CORRECTIONS WITHIN FIVE FEET OF THE BUILDING NECESSARY TO COMPLETE CONNECTION OF LATERALS TO THE BUILDINGS SHALL BE MADE BY THE BUILDING CONTRACTOR.
5.	CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE APPROPRIATE SIZES OF THE DRAINAGE STRUCTURES (CATCH BASINS, MANHOLES, YARD DRAINS, ETC.) TO ACCOMMODATE THE PIPING SHOWN.
6.	STORM DRAINAGE PIPING INSTALLATION SHALL COMMENCE AT THE FURTHEST DOWNSTREAM POINT AND PROCEED UPSTREAM "IN THE DRY".
7.	CONTRACTOR SHALL CONNECT ANY NEW ROOF DRAINS TO THE DRAINAGE SYSTEM. LOCATIONS AND ELEVATIONS OF ROOF LEADERS SHOULD BE COORDINATED WITH MEP AND ARCHITECTURAL DRAWINGS PRIOR TO CONSTRUCTION.
8.	THE CONTRACTOR WILL BE REQUIRED TO CLEAN THE ENTIRE DRAINAGE SYSTEM OF ALL DEBRIS AND OBSTRUCTIONS BOTH DURING CONSTRUCTION AND AT THE END OF CONSTRUCTION PRIOR TO ACCEPTANCE. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, REMOVAL OF ALL FORMWORK FROM STRUCTURES, CONCRETE AND MOLDED DROPPINGS, CONSTRUCTION DEBRIS AND SOIL. THE CONTRACTOR SHALL BE DUTCHMAN FLUSHED AND THE CONTRACTOR SHALL FURNISH ALL NECESSARY HOSE, PUMPS, PIPE, AND OTHER EQUIPMENT THAT MAY BE REQUIRED FOR THIS PURPOSE. NO DEBRIS SHALL BE FLUSHED INTO EXISTING STORM DRAINS, WETLANDS, OR WATERCOURSES; ALL DEBRIS SHALL BE REMOVED FROM THE SYSTEM AND DISPOSED OF IN ACCORDANCE WITH ALL GOVERNING AGENCIES.
9.	ALL MANHOLE COVERS, GRATES, INLETS, AND RIMS TO REMAIN SHALL BE ADJUSTED TO PROPOSED GRADE.
10.	CONTRACTOR TO PROVIDE ALL FITTINGS AND BENDS NECESSARY TO ACCOMPLISH WORK.

LEGEND	
EXISTING	PROPOSED
PROPERTY LINE	—
SETBACK LINE	—
BUILDING LINE	—
DOOR LOCATION	—
6" CURB LINE	—
TRAFFIC SIGN	—
STEEL BOLLARD	•
CONCRETE	—
BITUMINOUS CONCRETE	—
LANDSCAPE AREA	—
GRAVITY SANITARY LINE	—
WATER LINE	—
UNDERGROUND TELECOMMUNICATIONS	—
UNDERGROUND ELECTRIC	—
UNDERGROUND GAS	—
STORM PIPE	—
STORM CATCH BASIN	—
GATE VALVE	—
HYDRANT	—
CONTOUR	—
SPOT ELEVATION	—
COMPOST FILTER SOCK	—
LIMIT OF DISTURBANCE	—
INLET PROTECTION	—
TREE	○
MANHOLES	○○○○
MANHOLES	○
POWER STRUCTURE	—
BENCHMARK	○
SAWCUT	—
CURB REMOVAL	—

CS002   MASTER LEGEND & NOTES	
Site Development Plan For: LYONS GROUP RESTAURANT AT THE MALL AT ROCKINGHAM PARK 11 MALL ROAD SALEM, NH	
Assessors Map 107 & Lot 11154	
Prepared by: Langan Engineering & Environmental Services, LLC 100 Cambridge Street, Suite 1310 Boston, MA 02114	
Scale: As noted	October 30, 2025
 JOHN D. (PLANTE NH P.E. Lic. No. 14072 SALEM NIM, LLC C/O LYONS GROUP 355 CONGRESS ST BOSTON, MA 02210	Salem Planning Board Approval
Zoning Classification: COMMERCIAL-INDUSTRIAL DISTRICT, SUBDISTRICT C (CI-C)	



**CS003 AREA MAP**

Site Development Plan For:  
**LYONS GROUP RESTAURANT**  
 AT THE MALL AT ROCKINGHAM PARK  
 11 MALL ROAD  
 SALEM, NH

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
 100 Cambridge Street, Suite 1310  
 Boston, MA 02114

Scale: As noted

October 30, 2025

	Owner: <b>SALEM NIM, LLC</b> C/O LYONS GROUP 355 CONGRESS ST BOSTON, MA 02210	Salem Planning Board Approval
	Zoning Classification: <b>COMMERCIAL-INDUSTRIAL DISTRICT, SUBDISTRICT C (CI-C)</b>	

SCALE: 1 INCH = 40 FEET

## NOTES

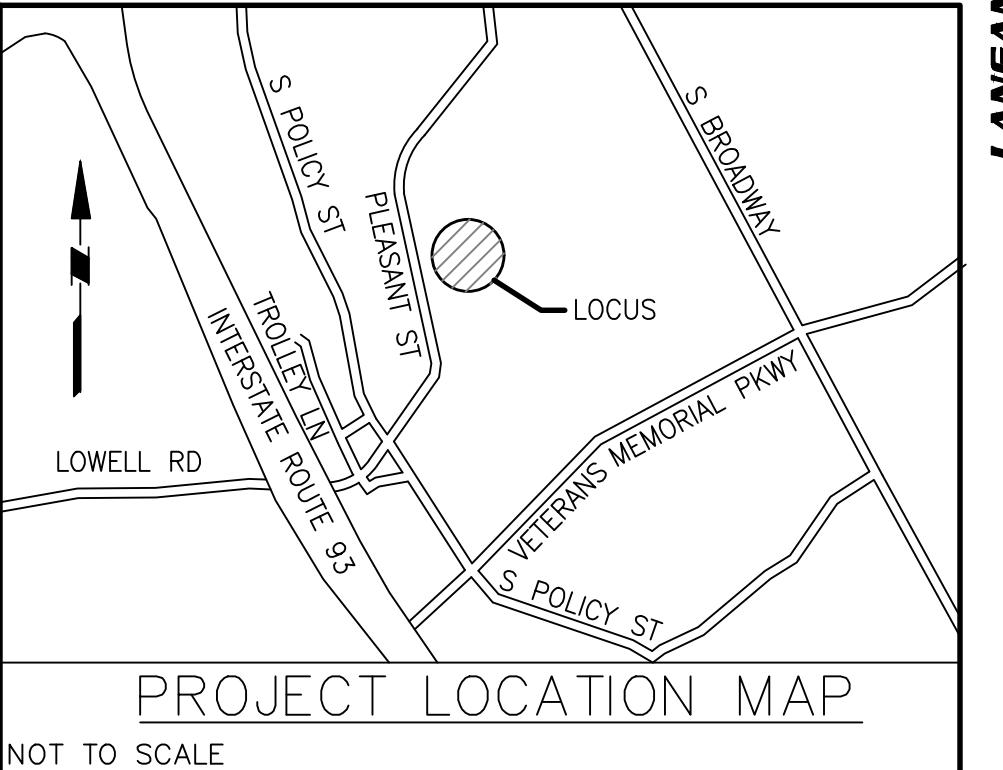
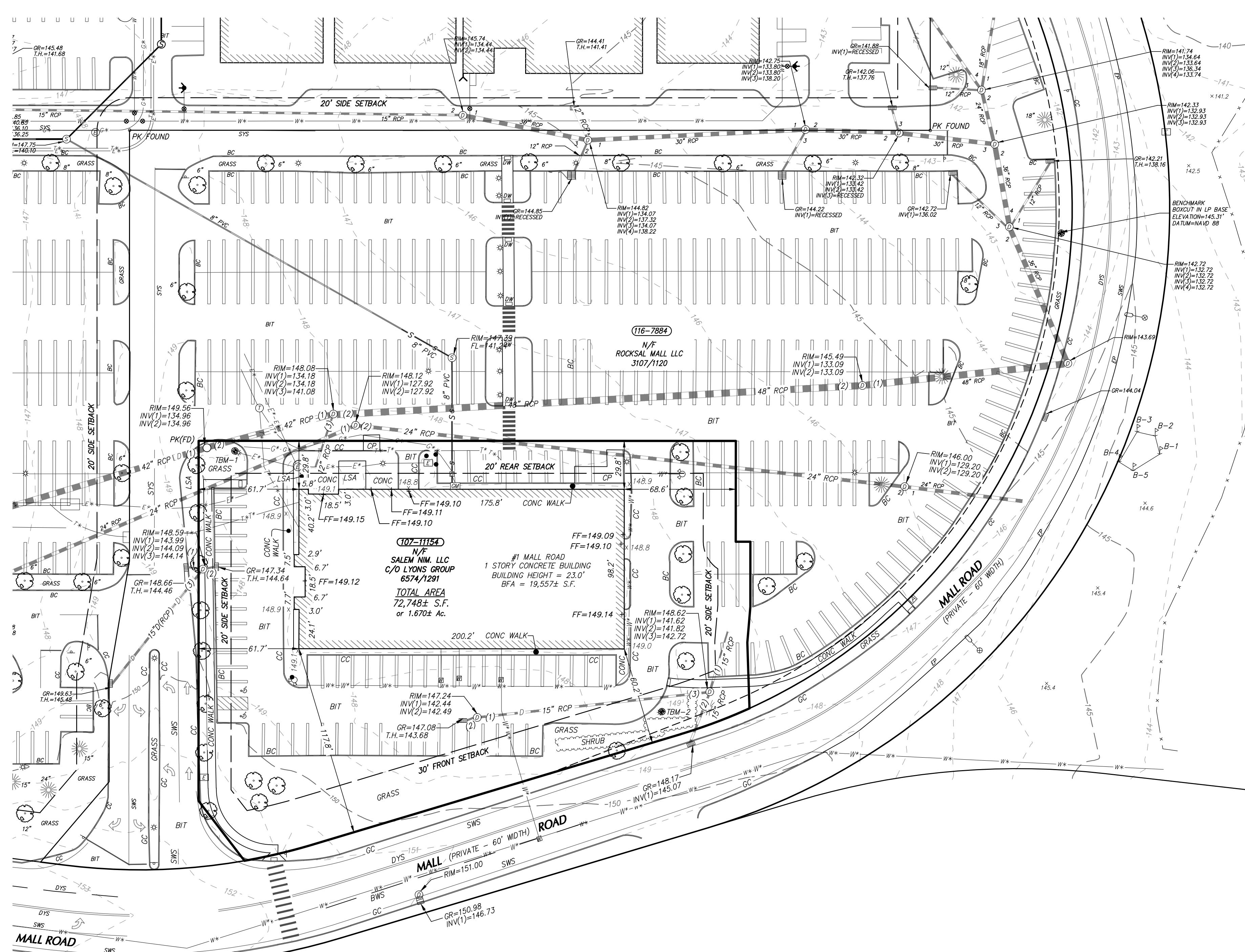
1. THIS SURVEY IS BASED UPON EXISTING PHYSICAL CONDITIONS FOUND AT THE SUBJECT SITE, DEED INFORMATION AND THE FOLLOWING REFERENCES:  
A. COMMITMENT FOR TITLE INSURANCE ISSUED BY STEWART TITLE GUARANTY COMPANY, COMMITMENT NUMBER: 2024-61, EFFECTIVE DATE JULY 2, 2024.
2. BENCH MARK INFORMATION:  
TEMPORARY BENCH MARKS SET:  
TBM-1: PK NAIL SET IN A LIGHT POLE BASE (AS SHOWN HEREON) ELEVATION = 151.66  
TBM-2: PK NAIL SET IN A LIGHT POLE BASE (AS SHOWN HEREON) ELEVATION = 151.53
3. ELEVATIONS ARE REFERENCED TO NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) (GEOD 12B) AS DETERMINED BY GNSS
4. THIS PLAN IS THE RESULT OF AN ON-THE-GROUND SURVEY PERFORMED BY LANGAN ENGINEERING AND ENVIRONMENTAL SERVICES, LLC, ON MARCH 6, 2024.
5. BY GRAPHIC PLOTTING ONLY, THIS PROPERTY IS IN ZONE X, OF THE FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NO. 330142 0563 E, WHICH BEARS AN EFFECTIVE DATE OF MAY 17, 2005 AND IS NOT IN A SPECIAL FLOOD HAZARD AREA. AS SHOWN ON THE FEMA WEBSITE ([HTTP://MSC.FEMA.GOV](http://MSC.FEMA.GOV)) BY FIRMETTE CREATED ON MARCH 19, 2024, WE HAVE LEARNED THIS COMMUNITY DOES CURRENTLY PARTICIPATE IN THE PROGRAM.
6. UNLESS SPECIFICALLY NOTED HEREON, STORM AND SANITARY SEWER INFORMATION (INCLUDING PIPE INVERT, PIPE MATERIAL, AND PIPE SIZE) WAS OBSERVED AND MEASURED AT FIELD LOCATED STRUCTURES (MANHOLES/CATCH BASINS, ETC.). CONDITIONS CAN VARY FROM THOSE ENCOUNTERED AT THE TIMES WHEN AND LOCATIONS WHERE DATA IS OBTAINED. DESPITE MEETING THE REQUIRED STANDARD OF CARE, THE SURVEYOR CANNOT, AND DOES NOT WARRANT THAT PIPE MATERIAL AND/OR PIPE SIZE THROUGHOUT THE PIPE RUN ARE THE SAME AS THOSE OBSERVED AT EACH STRUCTURE, OR THAT THE PIPE RUN IS STRAIGHT BETWEEN THE LOCATED STRUCTURES.
7. ADDITIONAL UTILITY (WATER, GAS, ELECTRIC ETC.) DATA IS SHOWN FROM FIELD LOCATED SURFACE MARKINGS (BY OTHERS), EXISTING STRUCTURES, AND/OR FROM EXISTING DRAWINGS.
8. UNLESS SPECIFICALLY NOTED HEREON, THE SURVEYOR HAS NOT EXCAVATED TO PHYSICALLY LOCATE THE UNDERGROUND UTILITIES. THE SURVEYOR MAKES NO GUARANTEES THAT THE SHOWN UNDERGROUND UTILITIES ARE EITHER IN SERVICE, ABANDONED, OR SUITABLE FOR USE, NOR ARE IN THE EXACT LOCATION OR CONFIGURATION INDICATED HEREON.
9. ALL BUILDINGS AND STRUCTURES WERE LOCATED AND MEASURED AT GROUND LEVEL. THE SURVEYOR MAKES NO DETERMINATIONS OR GUARANTEES AS TO THE ABSENCE, EXISTENCE, OR LOCATION OF UNDERGROUND STRUCTURES, FOUNDATIONS, FOOTINGS, PROJECTIONS, WALLS, TANKS, SEPTIC SYSTEMS, ETC. NO TEST PITS, EXCAVATIONS OR GROUND PENETRATING RADAR WERE PERFORMED AS PART OF THIS SURVEY.
10. PRIOR TO ANY DESIGN OR CONSTRUCTION, THE PROPER UTILITY AGENCIES MUST BE CONTACTED FOR VERIFICATION OF UTILITY TYPE AND FOR FIELD LOCATIONS.
11. THE PROPERTY HAS DIRECT ACCESS TO MALL ROAD, A PRIVATE WAY.

## LEGEND

★	PEDESTAL LIGHT
○○○○○	MANHOLE (TYPE AS LABELED)
■	GAS VALVE
—	WATER VALVE
□	UNKNOWN VALVE
—	CATCH BASIN
—	SPOT ELEVATION
—	TREE
—	SIGN
—	ELECTRIC METER
—	ELECTRIC BOX
—	GAS METER
—	TEMPORARY BENCHMARK
—	ACRES
—	BITUMINOUS CURB
—	BITUMINOUS
—	BROKEN WHITE STRIPE
—	CONCRETE CURB
—	CONCRETE
—	CONCRETE PAD
—	DRILL HOLE
—	DETECTABLE WARNING
—	DOUBLE YELLOW STRIPE
—	FOUND
—	FINISHED FLOOR ELEVATION
—	FLOW LINE
—	GRANITE CURB
—	GRATE
—	IRON BAR
—	LANDSCAPED AREA
—	PK NAIL
—	POLYVINYL CHLORIDE
—	REINFORCED CONCRETE PIPE
—	SET
—	STONE BOUND
—	SQUARE FEET
—	SINGLE WHITE STRIPE
—	TOP OF HOOD
—	TEMPORARY BENCH MARK

## LEGEND

—	PROPERTY LINE
—	CONTOUR LINE
—	GAS MARK OUT LINE
—	WATER MARK OUT LINE
—	ELECTRIC MARK OUT LINE
—	TELEPHONE MARK OUT LINE
—	SANITARY MARK OUT LINE
—	DRAINAGE MARK OUT LINE
—	REFERENCE UTILITY LINE (TYPE AS NOTED) - PLOTTED FROM EXISTING MAPPING



PROJECT NO. 140141903

Date	Description	No.
REVISIONS		
 10/23/2025 IAN C. WISURI, PLS No. 988 DATE SIGNED		

**LANGAN**  
Langan Engineering and Environmental Services, LLC.  
100 Cambridge Street, Suite 1310  
Boston, MA 02114  
T: 617.824.9100 F: 617.824.9101 [www.langan.com](http://www.langan.com)

Project LYONS GROUP RESTAURANT AT THE MALL AT ROCKINGHAM  
1 MALL ROAD  
SALEM, 03079  
ROCKINGHAM COUNTY NEW HAMPSHIRE  
Prepared for

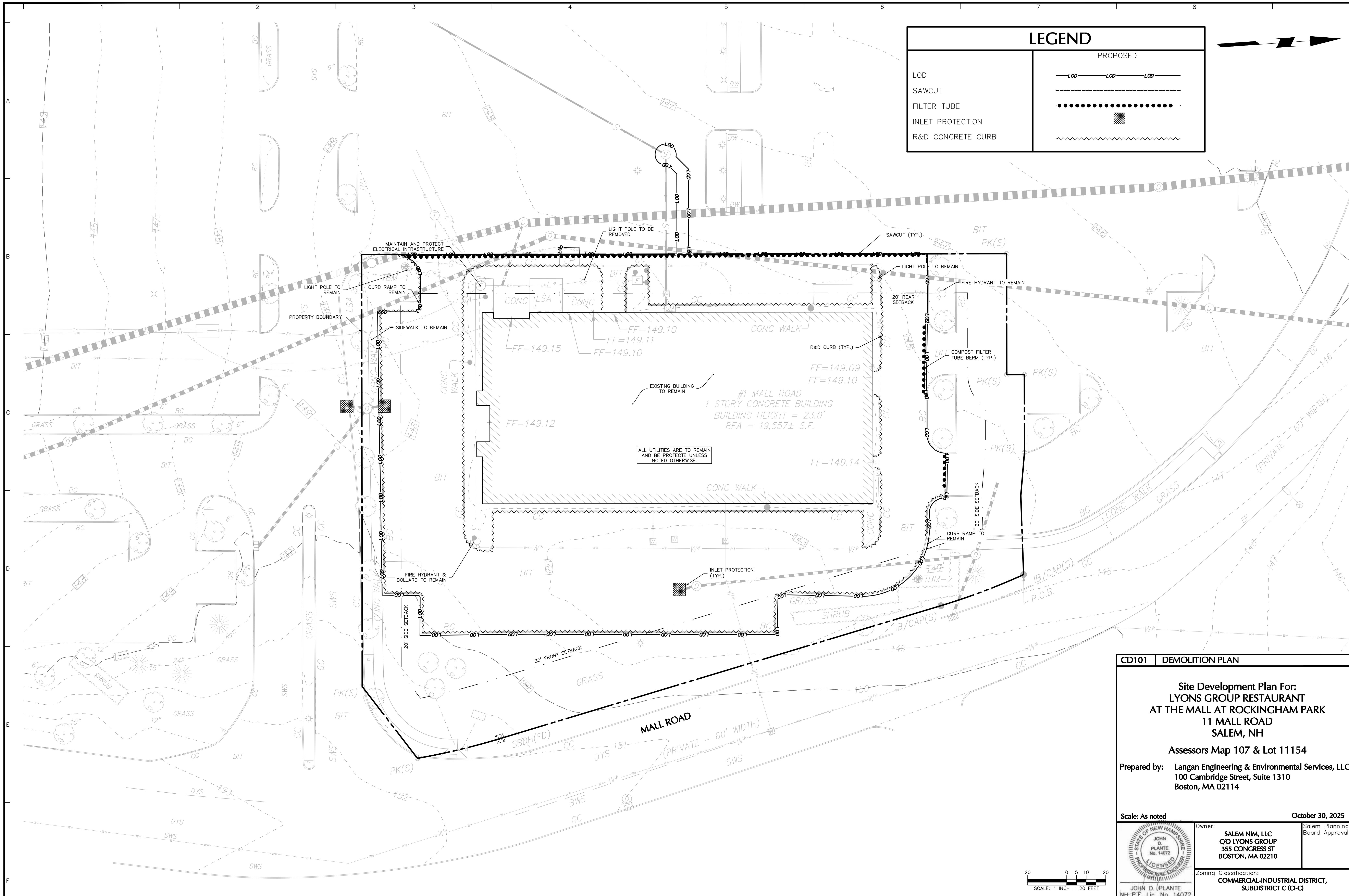
SALEM NIM, LLC  
C/O LYONS GROUP  
335 CONGRESS STREET  
BOSTON, MA 02210

Record Owner  
SALEM NIM, LLC  
C/O LYONS GROUP  
6574/1291

Drawing Title  
EXISTING CONDITIONS

Project No. 151041901  
Drawing No. VT901  
Date OCTOBER 23, 2025  
Drawn By BAL  
Checked By ICW  
Sheet 1 of 1

VT901

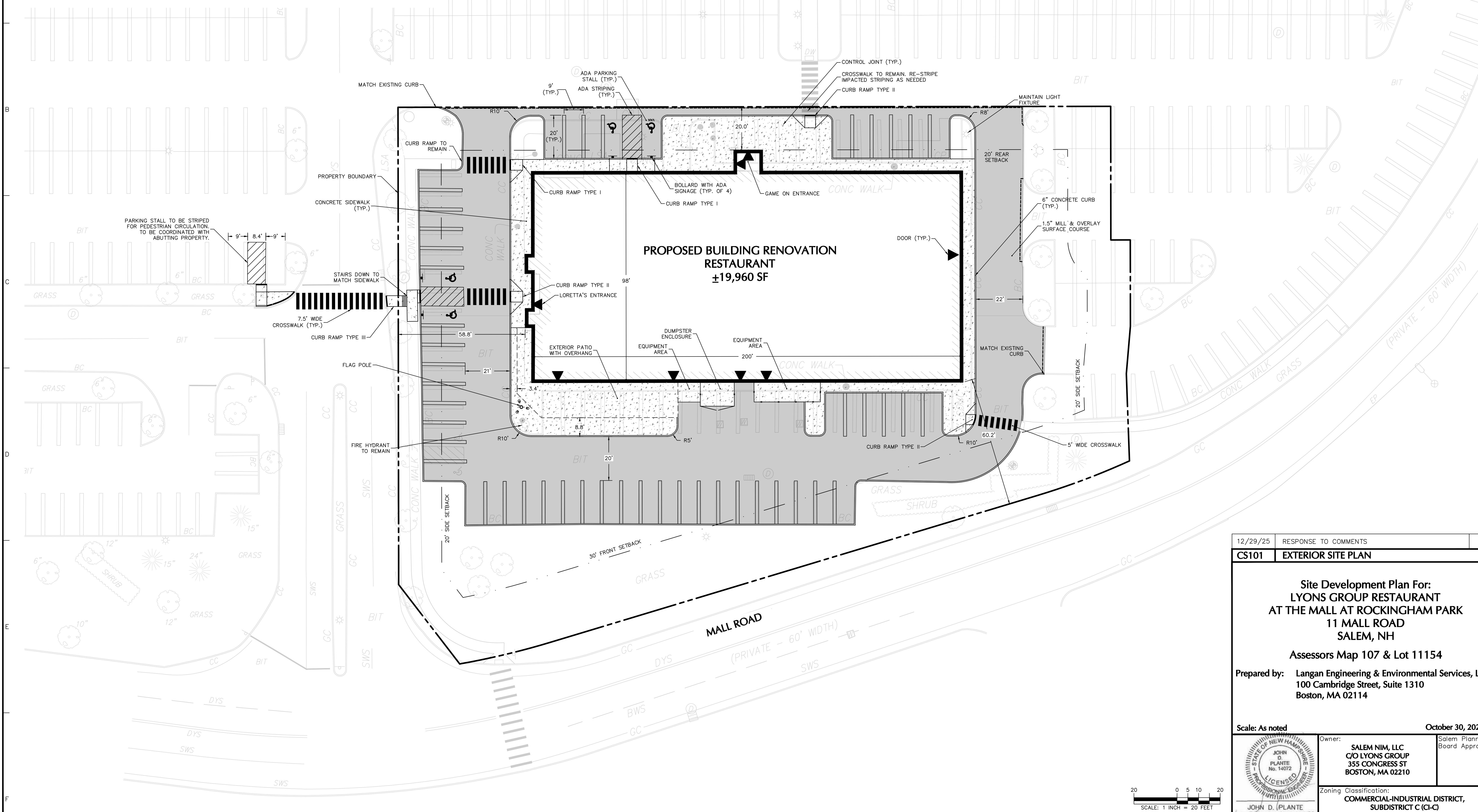


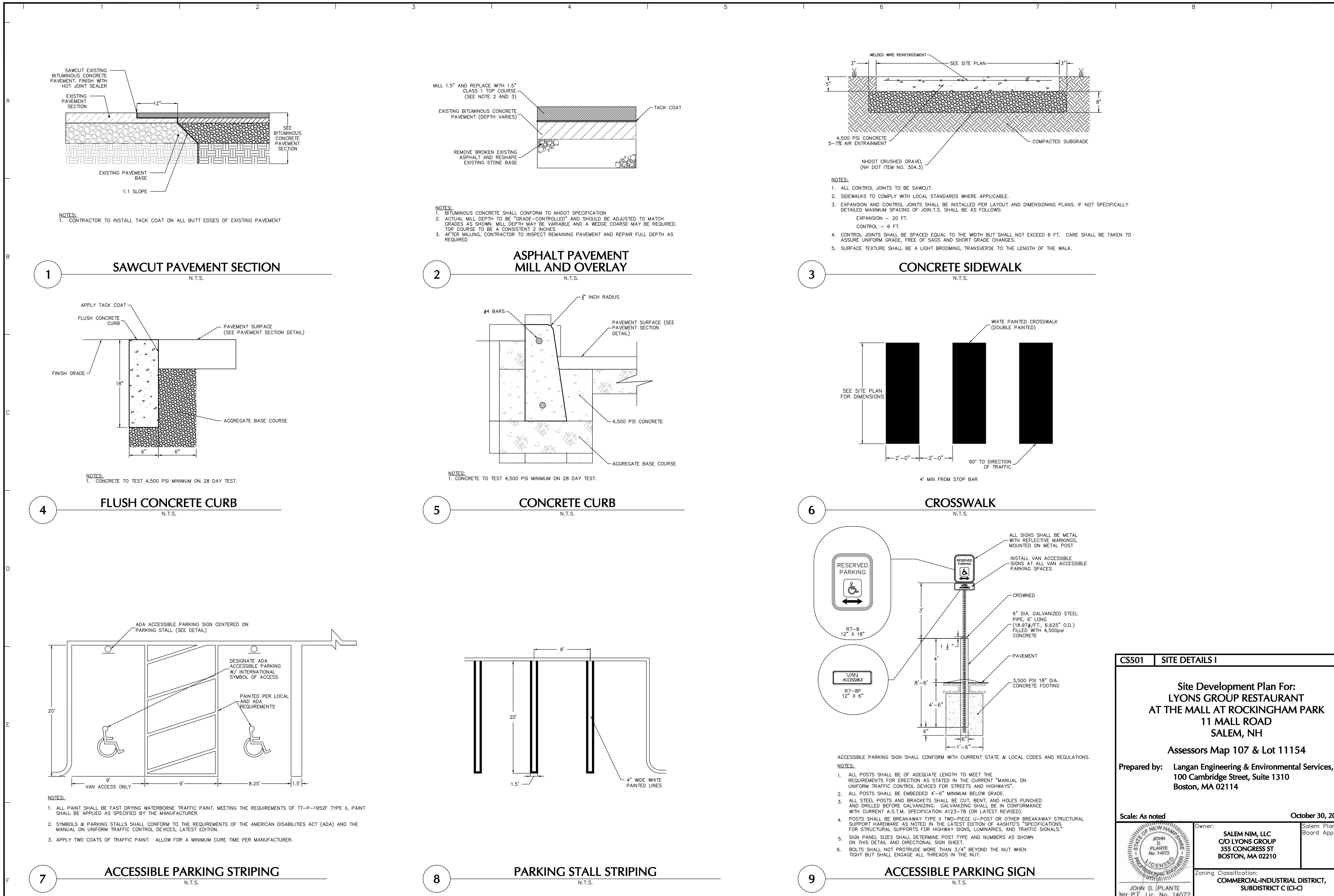
## ZONING REQUIREMENTS

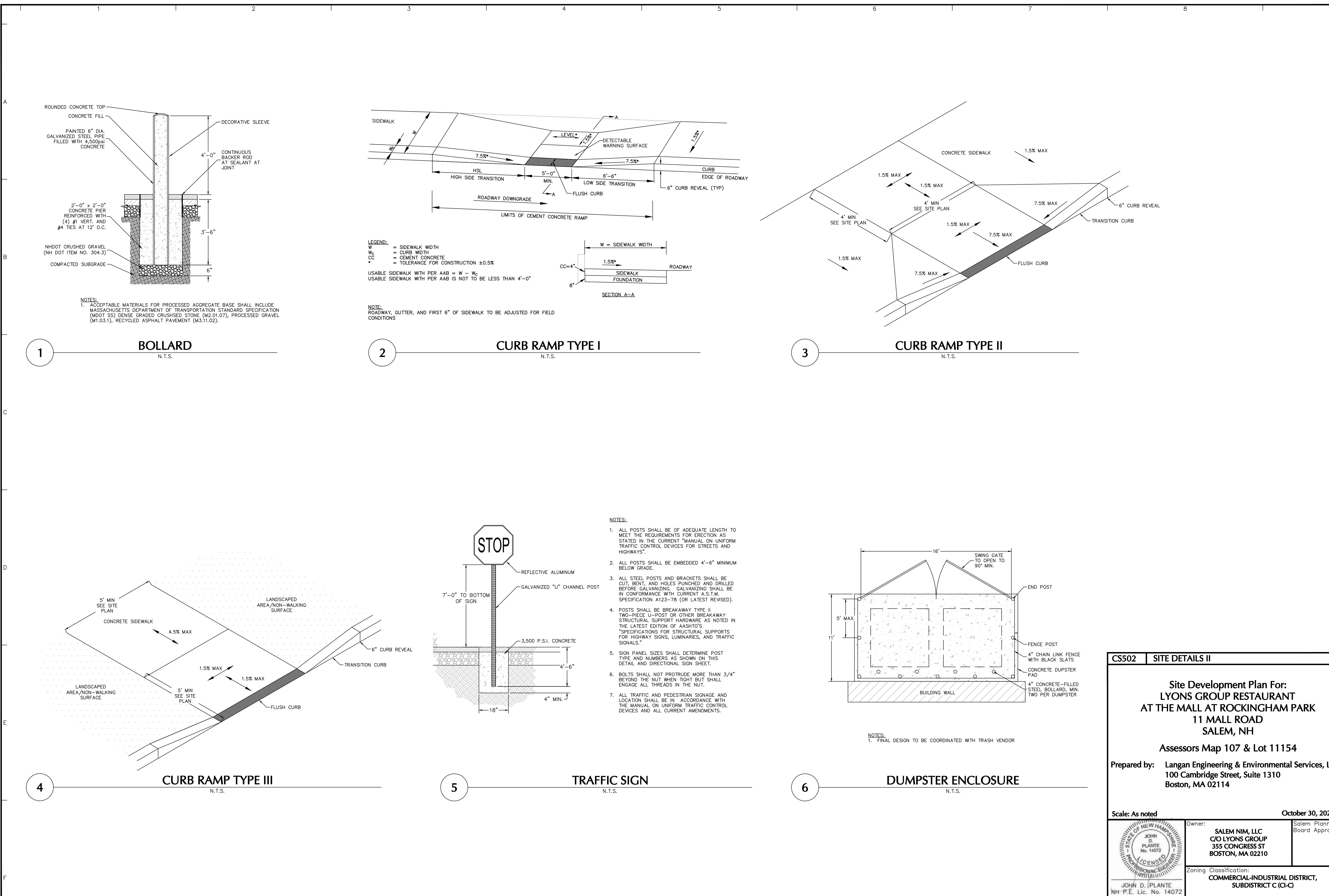
ZONE: COMMERCIAL-INDUSTRIAL DISTRICT, SUBDISTRICT C (CI-C)

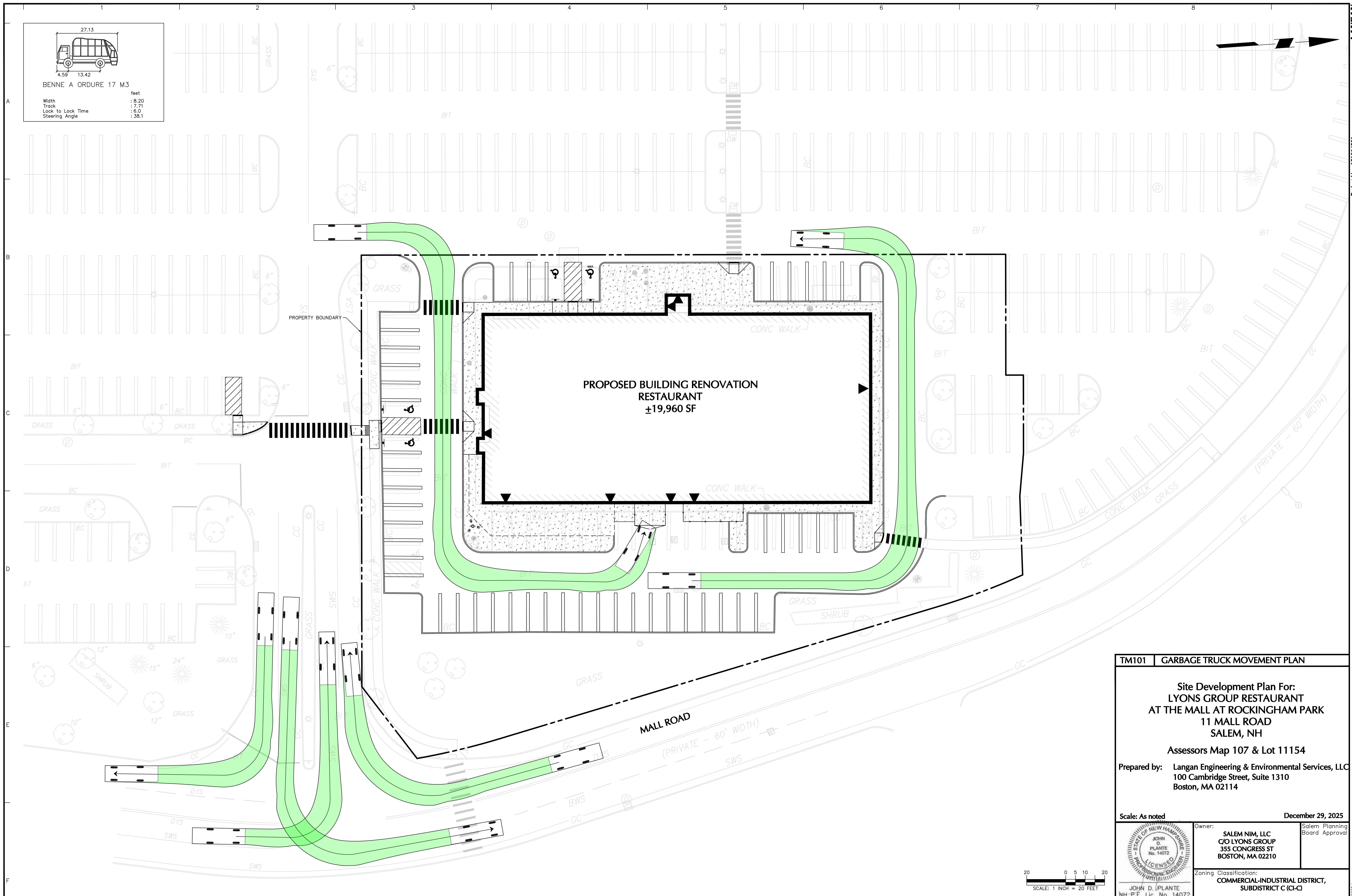
PROPOSED USE: RESTAURANT - ALLOWED BY RIGHT

LOT/BUILDING	REQUIRED	EXISTING	PROPOSED	SECTION
MINIMUM LOT SIZE	NONE	72,748 SF	72,748 SF	490-202
FRONT YARD SETBACK	30 FT	±60 FT	±60.2 FT	490-202
SIDE YARD SETBACK	20 FT	±58 FT	±58.8 FT	490-202
REAR YARD SETBACK	20 FT	±30 FT	±20.0 FT	490-202
MAXIMUM LOT COVERAGE	70%	70%	78%	490-202
MAXIMUM BUILDING HEIGHT	45 FT	23 FT	23 FT	490-202
PARKING				
CAR PARKING DIMENSIONS	9 FT x 20 FT	9 FT x 20 FT	9 FT x 20 FT	490-701
NUMBER OF SPACES				
1 PER 75 SF GROSS FLOOR AREA OR 1 PER 2 SEATS, WHICHEVER IS GREATER	19,960 SF / 75 = 266 SPACES	84 SPACES	68 SPACES	490-701









M101 | GARBAGE TRUCK MOVEMENT PLAN

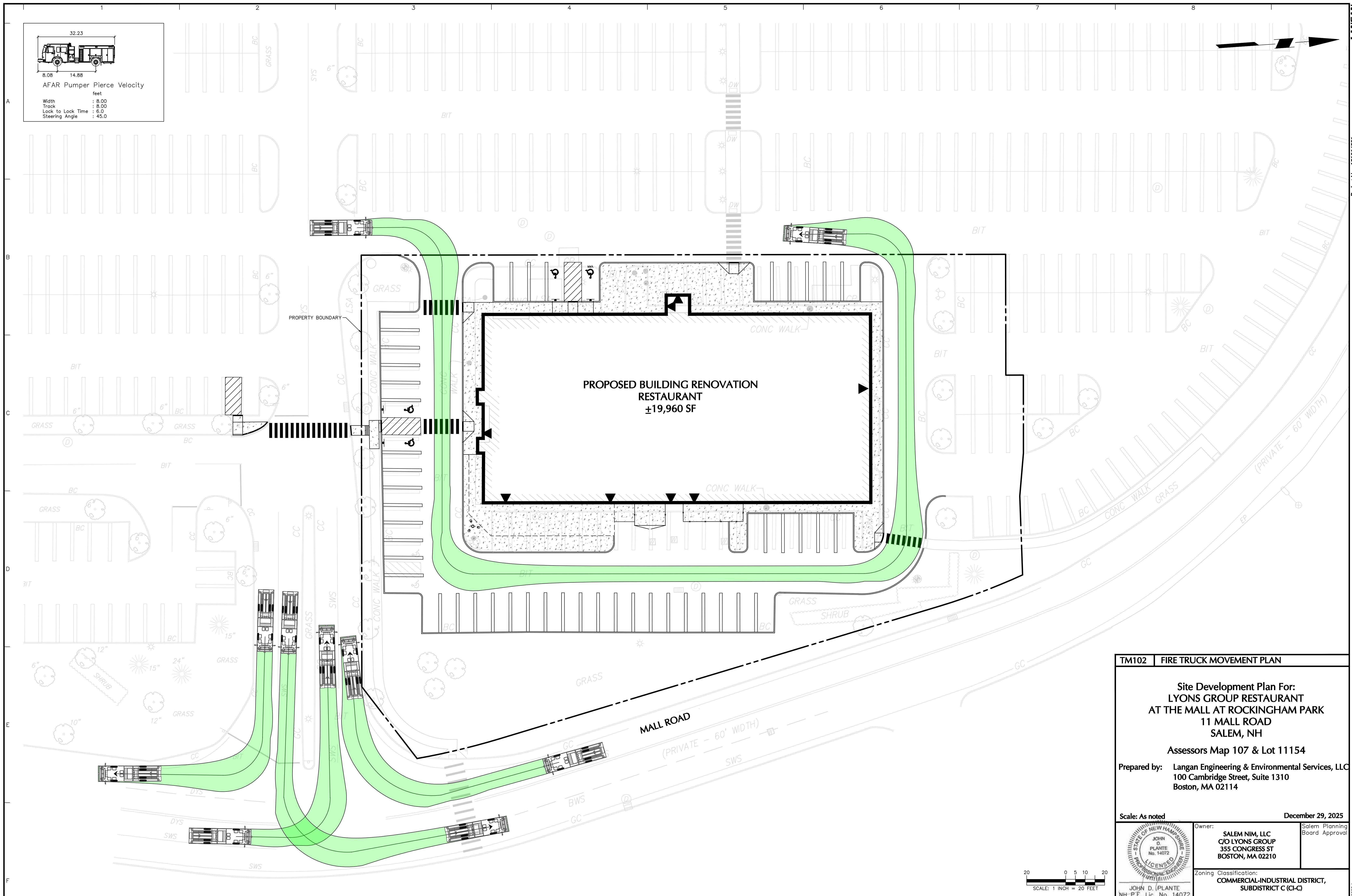
**Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH**

Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

cale: As noted	December 29, 2025	
	<p>Owner:</p> <p><b>SALEM NIM, LLC</b>  <b>C/O LYONS GROUP</b>  <b>355 CONGRESS ST</b>  <b>BOSTON, MA 02210</b></p>	Salem Planning Board Approval
	Zoning Classification:	

Zoning Classification:  
**COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (CI-C)**



FM102 FIRE TRUCK MOVEMENT PLAN

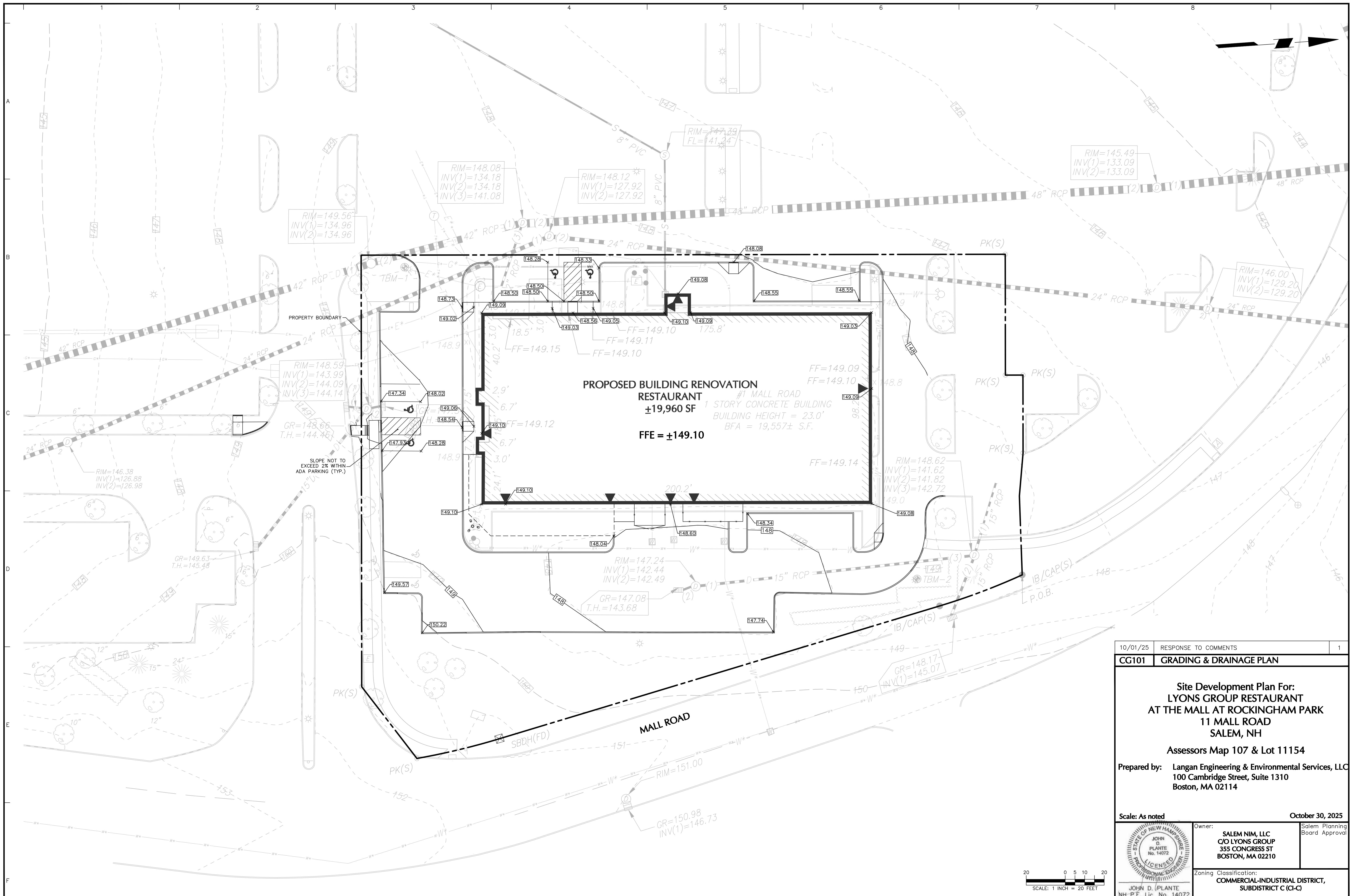
**Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH**

**Assessors Map 103 & Lot 11154**

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

cale: As noted	December 29, 2025
	<p>Owner: <b>SALEM NIM, LLC</b>  <b>C/O LYONS GROUP</b>  <b>355 CONGRESS ST</b>  <b>BOSTON, MA 02210</b></p>
Zoning Classification:	Salem Planning Board Approval

Zoning Classification:  
**COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (CI-C)**



VANGA

Project No. 151064501

0/01/25	RESPONSE TO COMMENTS	1
0/01/25	DRAWING 9 DRAWING PLAN	

**Site Development Plan For:  
LYONS GROUP RESTAURANT  
AT THE MALL AT ROCKINGHAM PARK  
11 MALL ROAD  
SALEM, NH**

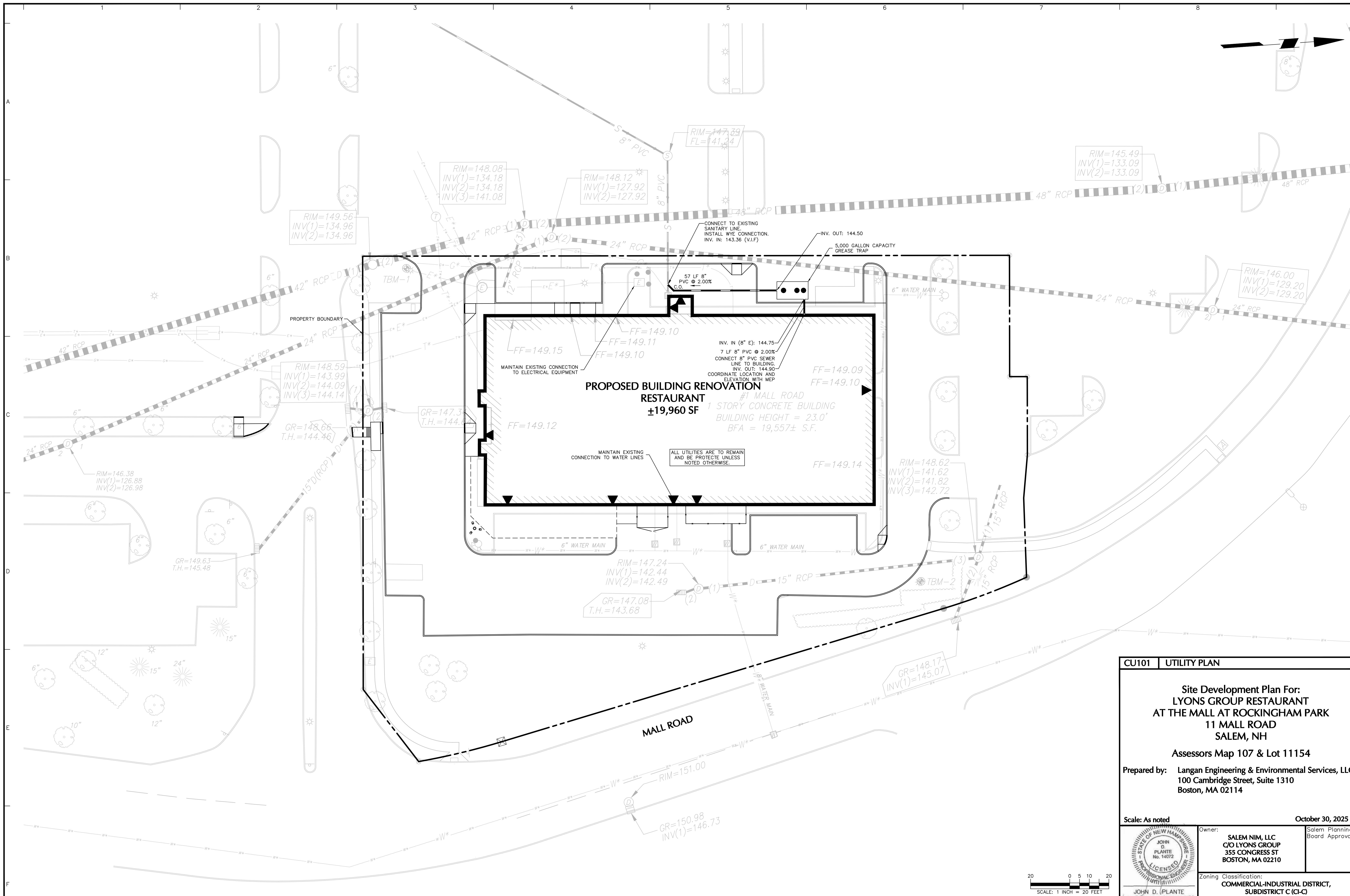
## Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

cale: As noted October 30, 2025

The image shows a circular seal for John D. Plante. The outer ring contains the text "PROFESSIONAL ENGINEER" at the top and "MASSACHUSETTS" at the bottom. The inner circle contains "JOHN D. PLANTE" at the top and "No. 14072" at the bottom. The bottom half of the inner circle contains the word "LICENSED" on the left and "PRACTICE" on the right, separated by a vertical line.

COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (CI-C)

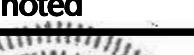


CU101 | UTILITY PLAN

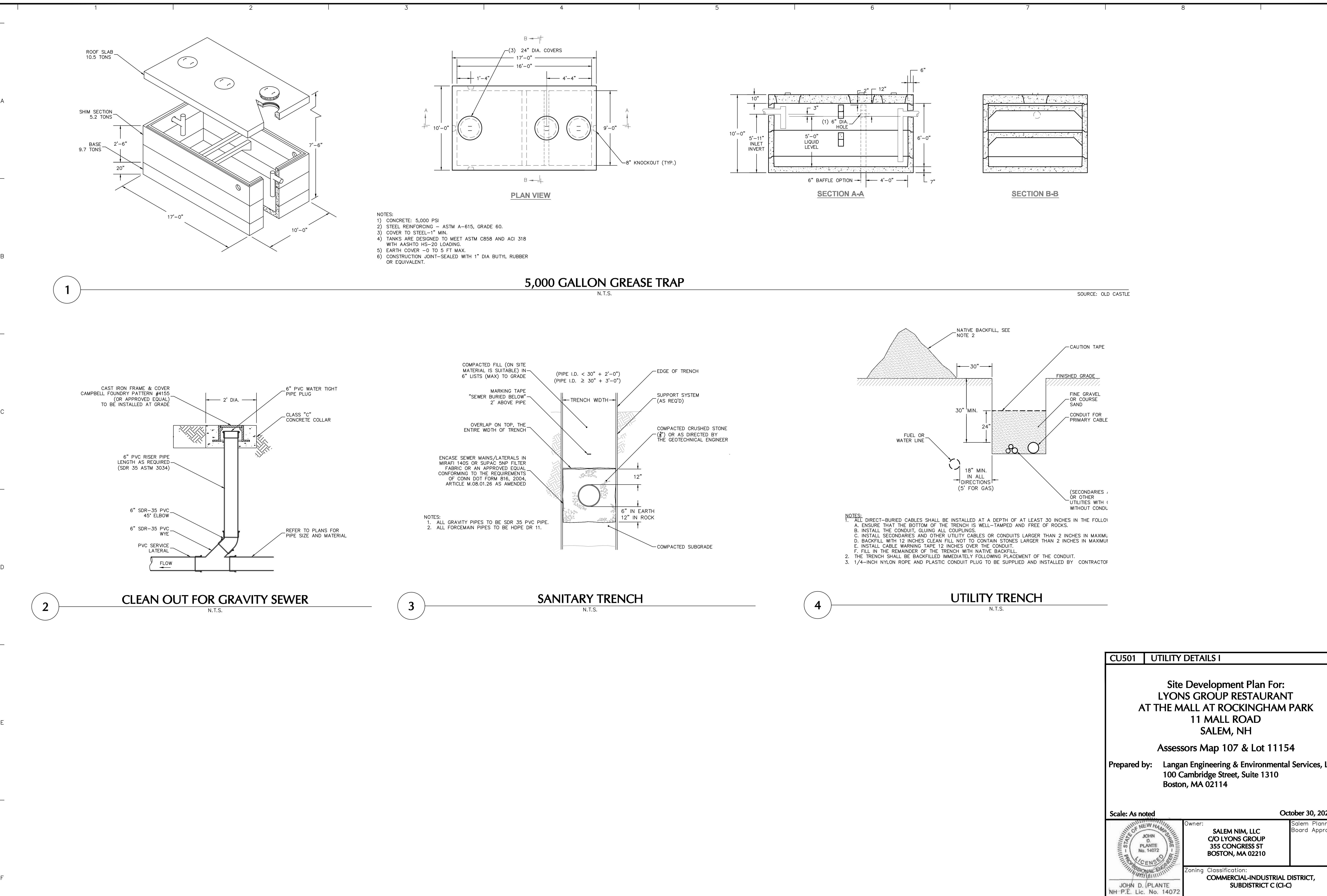
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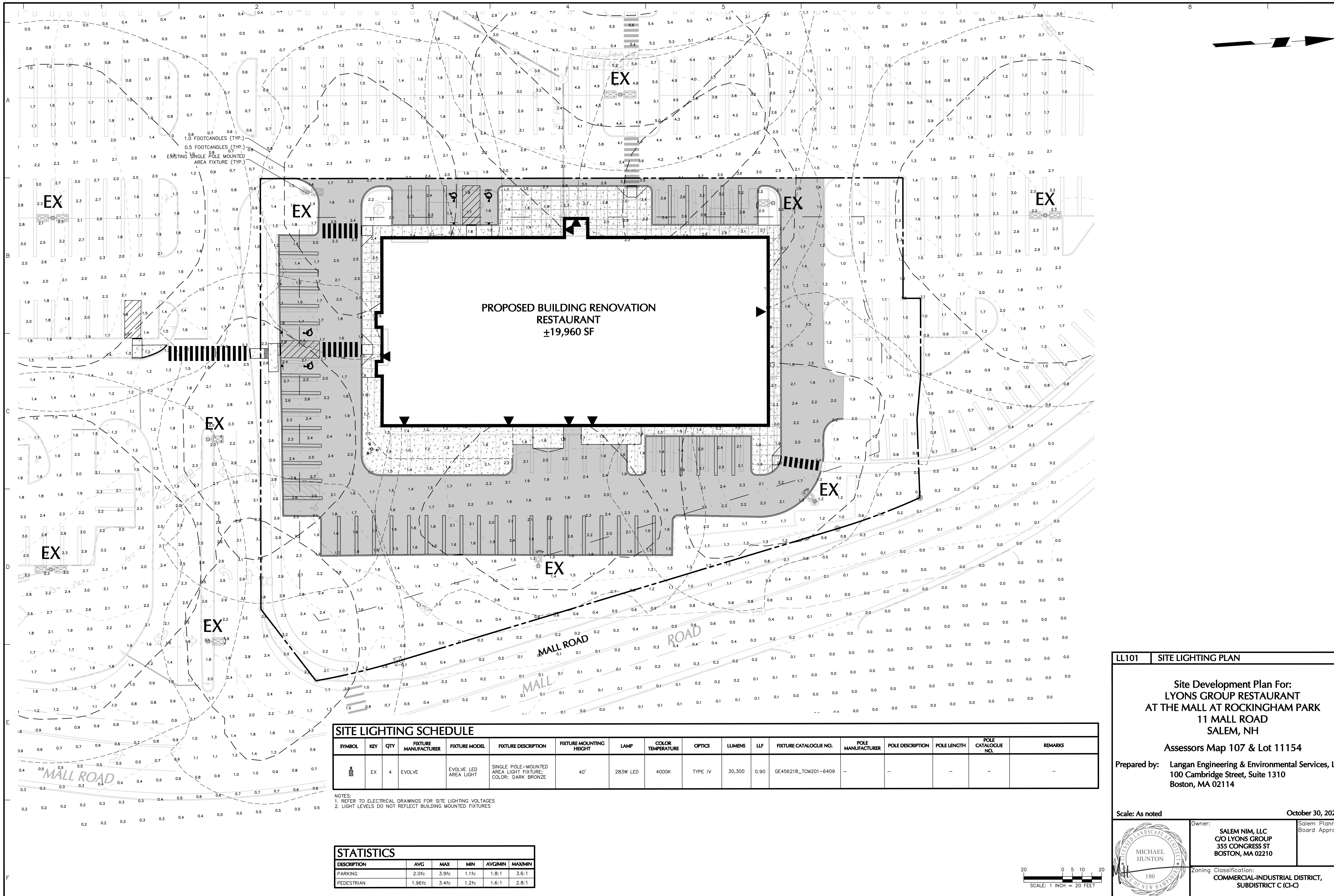
## Assessors Map 107 & Lot 11154

Prepared by: Langan Engineering & Environmental Services, LLC  
100 Cambridge Street, Suite 1310  
Boston, MA 02114

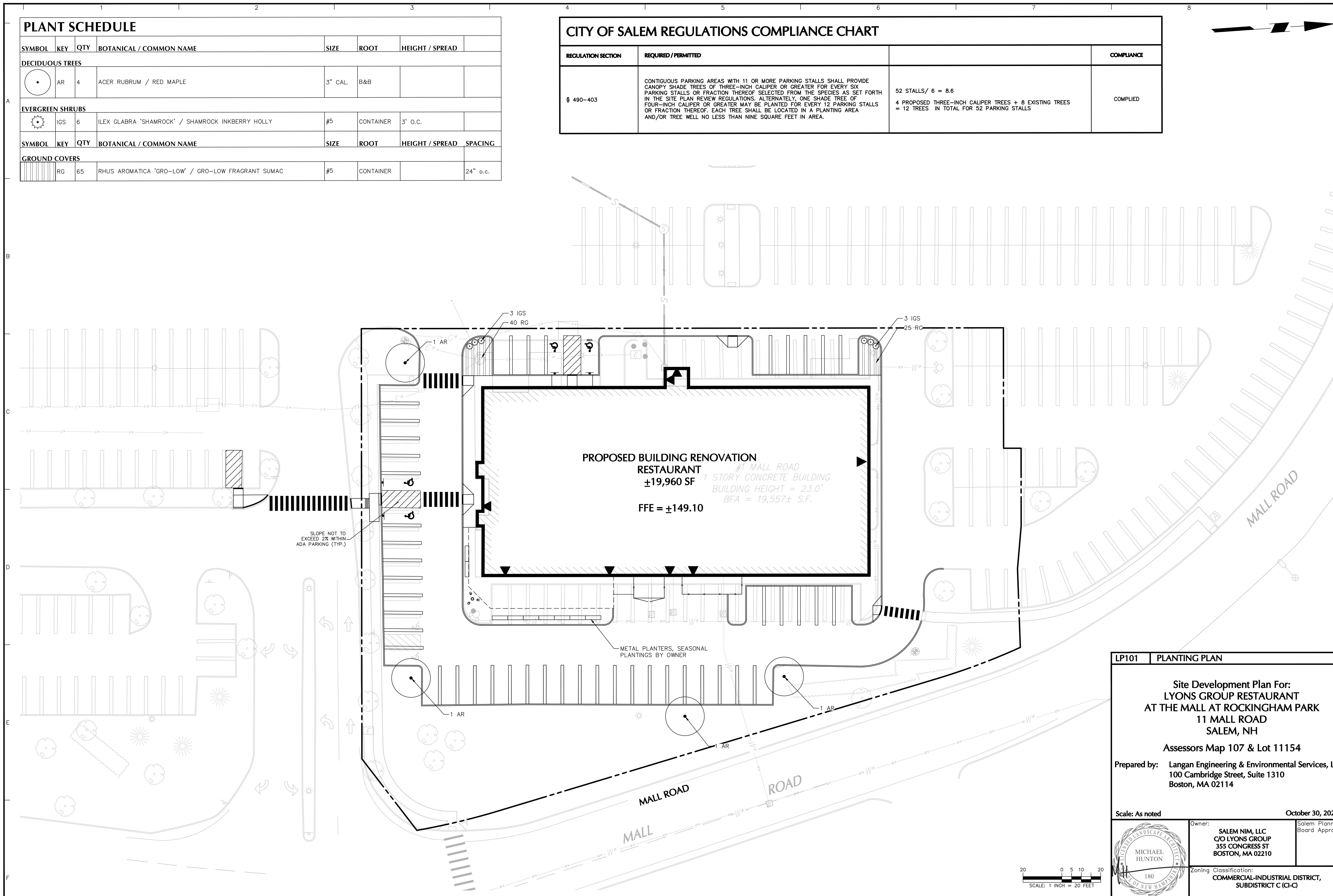
cale: As noted	October 30, 2025	
	<p>Owner:</p> <p><b>SALEM NIM, LLC</b>  <b>C/O LYONS GROUP</b>  <b>355 CONGRESS ST</b>  <b>BOSTON, MA 02210</b></p>	Salem Planning Board Approval
	Zoning Classification:	

Zoning Classification:  
**COMMERCIAL-INDUSTRIAL DISTRICT,  
SUBDISTRICT C (CI-C)**









## GENERAL LANDSCAPE PLANTING NOTES

1. INVOICES FOR ALL PLANTING WITHIN WETLAND AND WETLAND BUFFER SHALL BE PROVIDED AND APPROVED BY CONSERVATION COMMISSION PRIOR TO INSTALLATION.
2. NAMES OF PLANTS AS DESCRIBED ON THIS PLAN CONFORM TO THOSE GIVEN IN "STANDARDIZED PLANT NAMES", 1942 EDITION, PREPARED BY THE AMERICAN JOINT COMMITTEE ON HORTICULTURAL NOMENCLATURE. NAMES OF PLANT VARIETIES NOT INCLUDED THEREIN CONFORM TO NAMES GENERALLY ACCEPTED IN NURSERY TRADE.
3. ALL EXPOSED GROUND SURFACES THAT ARE NOT PAVED WITHIN THE CONTRACT LIMIT LINE, AND THAT ARE NOT COVERED BY LANDSCAPE PLANTING OR SEEDING AS SPECIFIED, SHALL BE COVERED BY A NATURAL MULCH THAT WILL PREVENT SOIL EROSION AND THE EMANATION OF DUST.
4. NO PLANT SHALL BE PUT INTO THE GROUND BEFORE ROUGH GRADING HAS BEEN COMPLETED AND APPROVED BY THE PROJECT LANDSCAPE ARCHITECT OR PROJECT ENGINEER.
5. STANDARDS FOR TYPE, SPREAD, HEIGHT, ROOT BALL AND QUALITY OF NEW PLANT MATERIAL SHALL BE IN ACCORDANCE WITH STANDARDS AS SET FORTH IN THE "AMERICAN STANDARD FOR NURSERY STOCK", PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN. PLANT MATERIAL SHALL HAVE NORMAL HABIT OF GROWTH AND BE HEALTHY, VIGOROUS, AND FREE FROM DISEASES AND INSECT INFESTATION.
6. NEW PLANT MATERIAL SHALL BE NURSERY GROWN UNLESS SPECIFIED OTHERWISE. ALL PLANTS SHALL BE SET PLUMB AND SHALL EARTH THE SAME RELATIONSHIP TO FINISHED GRADE AS THE PLANT'S ORIGINAL GRADE BEFORE DIGGING. PLANT MATERIAL OF THE SAME SPECIES AND AS THE SAME SIZE SHOULD BE SIMILAR IN SHAPE, COLOR, AND HABIT. THE LANDSCAPE ARCHITECT HAS THE RIGHT TO REJECT PLANT MATERIAL THAT DOES NOT CONFORM TO THE TYPICAL OR SPECIFIED HABIT OF THAT SPECIES.
7. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING UNDERGROUND UTILITY AND SEWER LINES PRIOR TO THE START OF EXCAVATION ACTIVITIES. NOTIFY THE PROJECT ENGINEER AND OWNER IMMEDIATELY OF ANY CONFLICTS WITH PROPOSED PLANTING LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE.
8. THE CONTRACTOR SHALL NOT MAKE SUBSTITUTIONS. IF THE SPECIFIED LANDSCAPE MATERIAL IS NOT OBTAINABLE, THE CONTRACTOR SHALL SUBMIT PROOF OF NON-AVAILABILITY TO THE LANDSCAPE ARCHITECT AND OWNER, TOGETHER WITH A WRITTEN PROPOSAL FOR USE OF AN EQUIVALENT MATERIAL.
9. LANDSCAPE CONTRACTOR TO STAKE OUT PLANTING LOCATIONS, FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT AND/OR OWNER BEFORE PLANTING WORK BEGINS. THE LANDSCAPE ARCHITECT AND/OR OWNER SHALL DIRECT THE CONTRACTOR IN THE FINAL PLACEMENT OF ALL PLANT MATERIAL AND LOCATION OF PLANTING BEDS TO ENSURE COMPLIANCE WITH DESIGN INTENT UNLESS OTHERWISE INSTRUCTED.
10. THE LANDSCAPE ARCHITECT MAY REVIEW PLANT MATERIALS AT THE SITE, BEFORE PLANTING, FOR COMPLIANCE WITH REQUIREMENTS FOR GENUS, SPECIES, VARIETY, SIZE, AND QUALITY. THE LANDSCAPE ARCHITECT RETAINS THE RIGHT TO FURTHER REVIEW PLANT MATERIALS FOR SIZE AND CONDITION OF BALLS AND ROOT SYSTEM, INSECTS, INJURIES, AND LATENT DEFECTS, AND TO REJECT UNSATISFACTORY OR DEFECTIVE MATERIAL AT ANY TIME DURING PROGRESS OF WORK. THE CONTRACTOR SHALL REMOVE REJECTED PLANT MATERIAL IMMEDIATELY FROM PROJECT SITE AS DIRECTED BY THE LANDSCAPE ARCHITECT OR OWNER.
11. DELIVERY, STORAGE, AND HANDLING
  - A. PACKAGED MATERIALS: PACKAGED MATERIALS SHALL BE DELIVERED IN CONTAINERS SHOWING WEIGHT, ANALYSIS, AND NAME OF MANUFACTURER. MATERIALS SHALL BE PROTECTED FROM DETERIORATION DURING DELIVERY, AND WHILE STORED AT SITE.
  - B. TREES AND SHRUBS: THE CONTRACTOR SHALL PROVIDE TREES AND SHRUBS DUG FOR THE GROWING SEASON FOR WHICH THEY WILL BE PLANTED. DO NOT PRUNE PRIOR TO DELIVERY UNLESS OTHERWISE DIRECTED BY THE LANDSCAPE ARCHITECT. DO NOT BEND OR BIND-TIE TREES OR SHRUBS IN SUCH A MANNER AS TO DAMAGE BARK, BREAK BRANCHES, OR DESTROY NATURAL SHAPE. PROVIDE PROTECTIVE COVERING DURING TRANSIT. DO NOT DROP BALLED AND BURLAP STOCK DURING DELIVERY OR HANDLING.
  - C. ALL PLANTS SHALL BE SHIPPED IN BURLAP OR BURLAP AND ORGANICALLY GROWN AS SPECIFIED. NO CONTAINER GROWN STOCK WILL BE ACCEPTED IF IT IS POORLY WRAPPED. ALL ROOTBALL WRAPPING AND BINDING MATERIAL MADE OF SYNTHETICS OR PLASTICS SHALL BE REMOVED FROM THE TOP OF THE BALL AT THE TIME OF PLANTING. IF THE PLANT IS SHIPPED WITH A WIRE BASKET AROUND THE ROOT BALL, THE WIRE BASKET SHALL BE CUT AND FOLDED DOWN 8 INCHES INTO THE PLANTING HOLE. WITH CONTAINER GROWN STOCK, THE CONTAINER SHALL BE REMOVED AND THE ROOT BALL SHIPPED IN BURLAP, BAGGED, OR BAGGED AND TIED.
  - D. THE CONTRACTOR SHALL HAVE TREES AND SHRUBS DELIVERED TO SITE AFTER PREPARATIONS FOR PLANTING HAVE BEEN COMPLETED AND PLANT IMMEDIATELY. IF PLANTING IS DELAYED MORE THAN 6 HOURS AFTER DELIVERY, THE CONTRACTOR SHALL SET TREES AND SHRUBS IN SHADE, PROTECT FROM WEATHER AND MECHANICAL DAMAGE AND KEEP ROOTS MOIST BY COVERING WITH MULCH, BURLAP OR OTHER ACCEPTABLE MEANS OF RETAINING MOISTURE.
12. ALL LANDSCAPED AREAS TO BE CLEARED OF ROCKS, STUMPS, TRASH AND OTHER UNSIGHTLY DEBRIS. ALL FINISHED GRADED AREAS SHOULD BE HAND RAKED SMOOTH ELIMINATING ANY CLUMPS AND AND UNLEVEL SURFACES PRIOR TO PLANTING OR MULCHING.
13. NEW PLANT MATERIAL SHALL BE GUARANTEED TO BE ALIVE AND IN VIGOROUS GROWING CONDITION FOR A PERIOD OF TWO YEARS FOLLOWING ACCEPTANCE BY THE OWNER. PLANT MATERIAL FOUND TO BE UNHEALTHY, DYING OR DEAD DURING THIS PERIOD, SHALL BE REMOVED AND REPLACED IN KIND BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER.
14. THE CONTRACTOR SHALL KEEP AREA CLEAN DURING DELIVERY AND INSTALLATION OF PLANT MATERIALS. REMOVE AND DISPOSE OF OFF-SITE ANY ACCUMULATED DEBRIS OR UNUSED MATERIALS. REPAIR DAMAGE TO ADJACENT AREAS CAUSED BY LANDSCAPE INSTALLATION OPERATIONS.
15. ALL PLANTS SHALL BE WATERED THOROUGHLY TWICE DURING THE FIRST 24-HOUR PERIOD AFTER PLANTING. ALL PLANTS SHALL THEN BE WATERED WEEKLY OR AS REQUIRED BY SITE AND WEATHER CONDITIONS TO MAINTAIN VIGOROUS AND HEALTHY PLANT GROWTH.
16. THE BACKFILL MIXTURE AND SOIL MIXES TO BE INSTALLED PER THE SPECIFICATIONS.
17. AFTER PLANT IS PLACED IN TREE PIT LOCATION, ALL TWINE HOLDING ROOT BALL TOGETHER SHOULD BE COMPLETELY REMOVED AND THE BURLAP SHOULD BE PULLED DOWN SO 1/3 OF THE ROOT BALL IS EXPOSED. SYNTHETIC BURLAP SHOULD BE COMPLETELY REMOVED AFTER INSTALLATION.
18. MULCH SHOULD NOT BE PILED UP AROUND THE TRUNK OF ANY PLANT MATERIAL. NO MULCH OR TOPSOIL SHOULD BE TOUCHING THE BASE OF THE TRUNK ABOVE THE ROOT COLLAR.
19. ALL FENCE INSTALLATION SHALL BE COMPLETED PRIOR TO COMMENCEMENT OF ANY LANDSCAPE PLANTING, LAWN AND GRASSES, OR IRRIGATION WORK.
20. FOR ANY DISCREPANCIES BETWEEN THE PLANT SCHEDULE AND PLANTING PLAN THE GRAPHIC QUANTITY SHOWN SHALL GOVERN.
21. PLANT MATERIALS SHALL NOT BE PLANTED UNTIL THE FINISHED GRADING HAS BEEN COMPLETED.
22. ALL PLANT INSTALLATIONS SHALL BE COMPLETED EITHER BETWEEN APRIL 1 – JUNE 15 OR AUGUST 15 – NOVEMBER 1, UNLESS OTHERWISE DIRECTED BY THE PROJECT LANDSCAPE ARCHITECT. SEE LAWN SEEDING DATES IN SEEDING NOTES.

## LANDSCAPE MAINTENANCE NOTES

1. MAINTENANCE OPERATIONS BEFORE APPROVAL:
  - A. PLANT CARE SHALL BEGIN IMMEDIATELY AFTER EACH PLANT IS SATISFACTORILY INSTALLED AND SHALL CONTINUE THROUGHOUT THE LIFE OF THE CONTRACT UNTIL FINAL ACCEPTANCE OF THE PROJECT.
  - B. CARE SHALL INCLUDE, BUT NOT BE LIMITED TO, REPLACING MULCH THAT HAS BEEN DISPLACED BY EROSION OR OTHER MEANS, REPAIRING AND RESHAPING WATER RINGS OR SAUCERS, MAINTAINING STAKES AND GUYS AS ORIGINALLY INSTALLED, WATERING WHEN NEEDED OR DIRECTED, AND PERFORMING ANY OTHER WORK REQUIRED TO KEEP THE PLANTS IN A HEALTHY CONDITION.
  - C. CONTRACTOR SHALL REMOVE AND REPLACE ALL DEAD, DEFECTIVE AND/OR REJECTED PLANTS AS REQUIRED BEFORE FINAL ACCEPTANCE.
2. MAINTENANCE DURING CONSTRUCTION:
  - A. MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER PLANTING. PLANTS SHALL BE WATERED, MULCHED, WEEDED, PRUNED, SPRAYED, FERTILIZED, CULTIVATED, AND OTHERWISE MAINTAINED AND PROTECTED UNTIL PROVISIONAL ACCEPTANCE. SETTLED PLANTS SHALL BE RESET TO PROPER GRADE AND POSITION, PLANTING SAUCER RESTORED AND DEAD MATERIAL REMOVED. STAKES AND WIRES SHALL BE TIGHTENED AND REPAIRED. DEFECTIVE WORK SHALL BE CORRECTED AS SOON AS POSSIBLE AFTER IT BECOMES APPARENT AND WEATHER AND SEASON PERMIT.
  - B. IF A SUBSTANTIAL NUMBER OF PLANTS ARE SICKLY OR DEAD AT THE TIME OF INSPECTION, ACCEPTANCE SHALL NOT BE GRANTED AND THE CONTRACTOR'S RESPONSIBILITY FOR MAINTENANCE OF ALL PLANTS SHALL BE EXTENDED FROM THE TIME REPLACEMENTS ARE MADE OR EXISTING PLANTS ARE DEEMED ACCEPTABLE BY THE LANDSCAPE ARCHITECT.
  - C. ALL REPLACEMENTS SHALL BE PLANTS OF THE SAME KIND AND SIZE SPECIFIED ON THE PLANT LIST OR THAT WHICH WAS TO REMAIN OR BE RELOCATED. THEY SHALL BE FURNISHED AND PLANTED AS SPECIFIED. THE COST SHALL BE BORNE BY THE CONTRACTOR. REPLACEMENTS RESULTING FROM REMOVAL, LOSS, OR DAMAGE DUE TO OCCUPANCY OF THE PROJECT IN ANY PART, VANDALISM, PHYSICAL DAMAGE BY ANIMALS, VEHICLES, ETC., AND LOSSES DUE TO CURTAILMENT OF WATER BY LOCAL AUTHORITIES SHALL BE APPROVED AND PAID FOR BY THE OWNER.
  - D. PLANTS SHALL BE GUARANTEED FOR A PERIOD OF TWO YEARS AFTER INSPECTION AND PROVISIONAL ACCEPTANCE.
  - E. AT THE END OF THE ESTABLISHMENT PERIOD, INSPECTION SHALL BE MADE AGAIN. ANY PLANT REQUIRED UNDER THIS CONTRACT THAT IS DEAD OR UNSATISFACTORY TO THE LANDSCAPE ARCHITECT OR OWNER SHALL BE REMOVED FROM THE SITE AND REPLACED DURING THE NORMAL PLANTING SEASON.
3. LAWN MAINTENANCE:
  - A. BEGIN MAINTENANCE IMMEDIATELY AFTER EACH PORTION OF LAWN IS PLANTED AND CONTINUE FOR 8 WEEKS AFTER ALL LAWN PLANTING IS COMPLETED.
  - B. WATER TO KEEP SURFACE SOIL MOIST, REPAIR WASHED OUT AREAS BY FILLING WITH TOPSOIL, LIMING, FERTILIZING AND RE-SEEDING; MOW TO 2 1/2 – 3 INCHES AFTER GRASS REACHES 3 1/2 INCHES IN HEIGHT, AND MOW FREQUENTLY ENOUGH TO KEEP GRASS FROM EXCEDING 3 1/2 INCHES. WEED BY LOCAL SPOT APPLICATION OF SELECTIVE HERBICIDE ONLY AFTER GRASS IS WELL-ESTABLISHED.
  - C. IN ORDER TO REMOVE WEEDS FROM THE LAWN AREAS, APPLY PRE-EMERGENT HERBICIDE ONCE GRASS IS WELL-ESTABLISHED. APPLY POST-EMERGENT HERBICIDE AS NEEDED. IF WEEDS ARE STILL PRESENT, PER SUPPLIER

## PLANTING SOIL SPECIFICATIONS

1. PLANTING SOIL, ALTERNATELY BE REFERRED TO AS TOPSOIL, SHOULD BE FRIABLE, FERTILE, WELL DRAINED, FREE OF DEBRIS, TOXINS, TRASH AND STONES OVER 1/2" DIA. IT SHOULD HAVE A HIGH ORGANIC CONTENT SUITABLE TO SUSTAIN HEALTHY PLANT GROWTH AND SHOULD LOOK AESTHETICALLY PLEASING HAVING NO NOXIOUS ODORS.
2. PLANTING SOIL:
 

REUSE SURFACE SOILS STOCKPILED ON SITE, VERIFYING COMPLIANCE WITH PLANTING SOIL AND TOPSOIL CRITERIA IN THIS SPECIFICATION THROUGH TESTING. CLEAN SURFACE SOIL OF ALL ROOTS, PLANTS, SOD, AND GRAVEL OVER 1" IN DIAMETER AND DELETERIOUS MATERIALS. IF ON-SITE SOILS ARE TO BE USED FOR PROPOSED PLANTING, THE CONTRACTOR SHALL DEMONSTRATE, THROUGH SOIL TESTING, THAT ON-SITE SOILS MEET THE SAME CRITERIA AS INDICATED IN NOTES PLANS AND SPECIFICATIONS.

SUPPLEMENT WITH IMPORTED OR MANUFACTURED TOPSOIL FROM OFF SITE SOURCES WHEN TOPSOIL AND PLANTING SOIL QUANTITIES ARE INSUFFICIENT. OBTAIN SOIL DISPLACED FROM NATURALLY WELL-DRAINED SITES WHERE TOPSOIL OCCURS AT LEAST 4" DEEP. DO NOT OBTAIN FROM AGRICULTURAL LAND, BOGS, MARSHES OR CONTAMINATED SITES.

CONTRACTOR SHALL TEST SOILS AND FURNISH SAMPLES UPON REQUEST. PACKAGED MATERIALS SHALL BE UNOPENED BAGS OR CONTAINERS, EACH BEARING A NAME, GUARANTEE, AND TRADEMARK OF THE PRODUCER, MATERIAL COMPOSITION, MANUFACTURER'S CERTIFIED ANALYSIS, AND THE WEIGHT OF THE MATERIAL. SOIL OR AMENDMENT MATERIALS SHALL BE STORED ON SITE TEMPORARILY IN STOCKPILES PRIOR TO PLACEMENT AND SHALL BE PROTECTED FROM INTRUSION OF CONTAMINANTS AND EROSION. AFTER MIXING, SOIL MATERIALS SHALL BE COVERED WITH A TARPULIN UNTIL TIME OF ACTUAL USE.
3. STANDARDS FOR TYPE, SPREAD, HEIGHT, ROOT BALL AND QUALITY OF NEW PLANT MATERIAL SHALL BE IN ACCORDANCE WITH STANDARDS AS SET FORTH IN THE "AMERICAN STANDARD FOR NURSERY STOCK", PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN. PLANT MATERIAL SHALL HAVE NORMAL HABIT OF GROWTH AND BE HEALTHY, VIGOROUS, AND FREE FROM DISEASES AND INSECT INFESTATION.
4. NEW PLANT MATERIAL SHALL BE NURSERY GROWN UNLESS SPECIFIED OTHERWISE. ALL PLANTS SHALL BE SET PLUMB AND SHALL EARTH THE SAME RELATIONSHIP TO FINISHED GRADE AS THE PLANT'S ORIGINAL GRADE BEFORE DIGGING. PLANT MATERIAL OF THE SAME SPECIES AND AS THE SAME SIZE SHOULD BE SIMILAR IN SHAPE, COLOR, AND HABIT. THE LANDSCAPE ARCHITECT HAS THE RIGHT TO REJECT PLANT MATERIAL THAT DOES NOT CONFORM TO THE TYPICAL OR SPECIFIED HABIT OF THAT SPECIES.
5. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL EXISTING UNDERGROUND UTILITY AND SEWER LINES PRIOR TO THE START OF EXCAVATION ACTIVITIES. NOTIFY THE PROJECT ENGINEER AND OWNER IMMEDIATELY OF ANY CONFLICTS WITH PROPOSED PLANTING LOCATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE.
6. THE CONTRACTOR SHALL NOT MAKE SUBSTITUTIONS. IF THE SPECIFIED LANDSCAPE MATERIAL IS NOT OBTAINABLE, THE CONTRACTOR SHALL SUBMIT PROOF OF NON-AVAILABILITY TO THE LANDSCAPE ARCHITECT AND OWNER, TOGETHER WITH A WRITTEN PROPOSAL FOR USE OF AN EQUIVALENT MATERIAL.
7. LANDSCAPE CONTRACTOR TO STAKE OUT PLANTING LOCATIONS, FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT AND/OR OWNER BEFORE PLANTING WORK BEGINS. THE LANDSCAPE ARCHITECT RETAINS THE RIGHT TO FURTHER REVIEW PLANT MATERIALS FOR SIZE AND CONDITION OF BALLS AND ROOT SYSTEM, INSECTS, INJURIES, AND LATENT DEFECTS, AND TO REJECT UNSATISFACTORY OR DEFECTIVE MATERIAL AT ANY TIME DURING PROGRESS OF WORK. THE CONTRACTOR SHALL REMOVE REJECTED PLANT MATERIAL IMMEDIATELY FROM PROJECT SITE AS DIRECTED BY THE LANDSCAPE ARCHITECT OR OWNER.
8. DELIVERY, STORAGE, AND HANDLING
  - A. PACKAGED MATERIALS: PACKAGED MATERIALS SHALL BE DELIVERED IN CONTAINERS SHOWING WEIGHT, ANALYSIS, AND NAME OF MANUFACTURER. MATERIALS SHALL BE PROTECTED FROM DETERIORATION DURING DELIVERY, AND WHILE STORED AT SITE.
  - B. TREES AND SHRUBS: THE CONTRACTOR SHALL PROVIDE TREES AND SHRUBS DUG FOR THE GROWING SEASON FOR WHICH THEY WILL BE PLANTED. DO NOT PRUNE PRIOR TO DELIVERY UNLESS OTHERWISE DIRECTED BY THE LANDSCAPE ARCHITECT. DO NOT BEND OR BIND-TIE TREES OR SHRUBS IN SUCH A MANNER AS TO DAMAGE BARK, BREAK BRANCHES, OR DESTROY NATURAL SHAPE. PROVIDE PROTECTIVE COVERING DURING TRANSIT. DO NOT DROP BALLED AND BURLAP STOCK DURING DELIVERY OR HANDLING.
  - C. ALL PLANTS SHALL BE SHIPPED IN BURLAP OR BURLAP AND ORGANICALLY GROWN AS SPECIFIED. NO CONTAINER GROWN STOCK WILL BE ACCEPTED IF IT IS POORLY WRAPPED. ALL ROOTBALL WRAPPING AND BINDING MATERIAL MADE OF SYNTHETICS OR PLASTICS SHALL BE REMOVED FROM THE TOP OF THE BALL AT THE TIME OF PLANTING. IF THE PLANT IS SHIPPED WITH A WIRE BASKET AROUND THE ROOT BALL, THE WIRE BASKET SHALL BE CUT AND FOLDED DOWN 8 INCHES INTO THE PLANTING HOLE. WITH CONTAINER GROWN STOCK, THE CONTAINER SHALL BE REMOVED AND THE ROOT BALL SHIPPED IN BURLAP, BAGGED, OR BAGGED AND TIED.
  - D. THE CONTRACTOR SHALL HAVE TREES AND SHRUBS DELIVERED TO SITE AFTER PREPARATIONS FOR PLANTING HAVE BEEN COMPLETED AND PLANT IMMEDIATELY. IF PLANTING IS DELAYED MORE THAN 6 HOURS AFTER DELIVERY, THE CONTRACTOR SHALL SET TREES AND SHRUBS IN SHADE, PROTECT FROM WEATHER AND MECHANICAL DAMAGE AND KEEP ROOTS MOIST BY COVERING WITH MULCH, BURLAP OR OTHER ACCEPTABLE MEANS OF RETAINING MOISTURE.
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10. THE LANDSCAPE ARCHITECT MAY REVIEW PLANT MATERIALS AT THE SITE, BEFORE PLANTING, FOR COMPLIANCE WITH REQUIREMENTS FOR GENUS, SPECIES, VARIETY, SIZE, AND QUALITY. THE LANDSCAPE ARCHITECT RETAINS THE RIGHT TO FURTHER REVIEW PLANT MATERIALS FOR SIZE AND CONDITION OF BALLS AND ROOT SYSTEM, INSECTS, INJURIES, AND LATENT DEFECTS, AND TO REJECT UNSATISFACTORY OR DEFECTIVE MATERIAL AT ANY TIME DURING PROGRESS OF WORK. THE CONTRACTOR SHALL REMOVE REJECTED PLANT MATERIAL IMMEDIATELY FROM PROJECT SITE AS DIRECTED BY THE LANDSCAPE ARCHITECT OR OWNER.
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  - A. PACKAGED MATERIALS: PACKAGED MATERIALS SHALL BE DELIVERED IN CONTAINERS SHOWING WEIGHT, ANALYSIS, AND NAME OF MANUFACTURER. MATERIALS SHALL BE PROTECTED FROM DETERIORATION DURING DELIVERY, AND WHILE STORED AT SITE.
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  - D. THE CONTRACTOR SHALL HAVE TREES AND SHRUBS DELIVERED TO SITE AFTER PREPARATIONS FOR PLANTING HAVE BEEN COMPLETED AND PLANT IMMEDIATELY. IF PLANTING IS DELAYED MORE THAN 6 HOURS AFTER DELIVERY, THE CONTRACTOR SHALL SET TREES AND SHRUBS IN SHADE, PROTECT FROM WEATHER AND MECHANICAL DAMAGE AND KEEP ROOTS MOIST BY COVERING WITH MULCH, BURLAP OR OTHER ACCEPTABLE MEANS OF RETAINING MOISTURE.
12. ALL PLANT MATERIAL SHALL BE INSTALLED AS PER DETAILS, NOTES AND CONTRACT SPECIFICATIONS. THE LANDSCAPE ARCHITECT MAY REVIEW INSTALLATION AND MAINTENANCE PROCEDURES.
13. NEW PLANT MATERIAL SHALL BE GUARANTEED TO BE ALIVE AND IN VIGOROUS GROWING CONDITION FOR A PERIOD OF TWO YEARS FOLLOWING ACCEPTANCE BY THE OWNER. PLANT MATERIAL FOUND TO BE UNHEALTHY, DYING OR DEAD DURING THIS PERIOD, SHALL BE REMOVED AND REPLACED IN KIND BY THE CONTRACTOR AT NO EXPENSE TO THE OWNER.
14. THE CONTRACTOR SHALL KEEP AREA CLEAN DURING DELIVERY AND INSTALLATION OF PLANT MATERIALS. REMOVE AND DISPOSE OF OFF-SITE ANY ACCUMULATED DEBRIS OR UNUSED MATERIALS. REPAIR DAMAGE TO ADJACENT AREAS CAUSED BY LANDSCAPE INSTALLATION OPERATIONS.
15. ALL PLANTS SHALL BE WATERED THOROUGHLY TWICE DURING THE FIRST 24-HOUR PERIOD AFTER PLANTING. ALL PLANTS SHALL THEN BE WATERED WEEKLY OR AS REQUIRED BY SITE AND WEATHER CONDITIONS TO MAINTAIN VIGOROUS AND HEALTHY PLANT GROWTH.
16. THE BACKFILL MIXTURE AND SOIL MIXES TO BE INSTALLED PER THE SPECIFICATIONS.
17. AFTER PLANT IS PLACED IN TREE PIT LOCATION, ALL TWINE HOLDING ROOT BALL TOGETHER SHOULD BE COMPLETELY REMOVED AND THE BURLAP SHOULD BE PULLED DOWN SO 1/3 OF THE ROOT BALL IS EXPOSED. SYNTHETIC BURLAP SHOULD BE COMPLETELY REMOVED AFTER INSTALLATION.
18. MULCH SHOULD NOT BE PILED UP AROUND THE TRUNK OF ANY PLANT MATERIAL. NO MULCH OR TOPSOIL SHOULD BE TOUCHING THE BASE OF THE TRUNK ABOVE THE ROOT COLLAR.
19. ALL FENCE INSTALLATION SHALL BE COMPLETED PRIOR TO COMMENCEMENT OF ANY LANDSCAPE PLANTING, LAWN AND GRASSES, OR IRRIGATION WORK.
20. FOR ANY DISCREPANCIES BETWEEN THE PLANT SCHEDULE AND PLANTING PLAN THE GRAPHIC QUANTITY SHOWN SHALL GOVERN.
21. PLANT MATERIALS SHALL NOT BE PLANTED UNTIL THE FINISHED GRADING HAS BEEN COMPLETED.
22. ALL PLANT INSTALLATIONS SHALL BE COMPLETED EITHER BETWEEN APRIL 1 – JUNE 15 OR AUGUST 15 – NOVEMBER 1, UNLESS OTHERWISE DIRECTED BY THE PROJECT LANDSCAPE ARCHITECT. SEE LAWN SEEDING DATES IN SEEDING NOTES.

## IRRIGATION NOTES:

1. THE IRRIGATION CONTRACTOR SHALL PROVIDE SHOP DRAWINGS OF THE IRRIGATION INSTALLATION PLAN AND CUT-SHEETS FOR ALL COMPONENTS FOR REVIEW AND APPROVAL BY THE PROJECT LANDSCAPE ARCHITECT OR OWNER'S REPRESENTATIVE. THE CONTRACTOR SHALL COORDINATE WITH THE LANDSCAPE ARCHITECT TO DETERMINE THE PLACEMENT AND DESIGNATION OF WATER LINES AND WATER USES IN GALLONS PER MINUTE PER ZONE, RUN TIME, SCHEDULE, LEGEND OF COMPONENTS AND PLAN GRAPHICS WITH QUANTITIES, MINIMUM SYSTEM REQUIREMENTS INCLUDING STATIC PRESSURE AT THE WATER CONNECTION POINT, ESTIMATED WATER BUDGET, CONSTRUCTION DETAILS AND IRRIGATION NOTES. THE PLAN SHALL ALSO INCLUDE LOCATIONS OF ALL PROPOSED SLEEVES AND THEIR SIZES, LOCATIONS OF ALL LATERALS, SIZE STANDARDS WITH SIZE INDICATIONS, LOCATION OF ALL SOIL MOISTURE SENSORS, CONTROL VALVES AND ALL OTHER COMPONENTS NECESSARY FOR THE SYSTEMS OPERATION.
2. LANDSCAPE AREAS SHALL BE IRRIGATED WITH POP-UP SPRAY AND ROTARY IRRIGATION HEADS IN SUFFICIENT DENSITY TO COVER THE ENTIRE AREA.
3. CONTRACTOR TO AVOID DISTURBANCE OF EXISTING PLANT MATERIAL WHEN LOCATING VALVES AND PIPE LINES. ANY PLANT MATERIAL DAMAGED AS A RESULT OF IRRIGATION INSTALLATION SHALL BE REPLACED AT NO ADDITIONAL COST.
4. ALL EXCAVATION MATERIAL SHALL BE PLACED BACK IN TRENCHES.
5. ALL DISTURBED LANDSCAPE AND PAVED AREAS SHALL BE RESTORED TO THE CONDITION FOUND PRIOR TO START OF INSTALLATION.
6. DEPTH OF TRENCHES SHALL BE SUFFICIENT OR PROVIDE A MINIMUM COVER ABOVE THE TOP OF PIPE AS FOLLOWS:
  - 12" OVER NON-PRESSURE LATERAL LINES
  - 18" OVER PRESSURE LATERAL LINES UNDER PAVING
  - 18" OVER CONTROL WIRES
  - 24" OVER MAIN LINE
7. THE IRRIGATION CONTRACTOR IS RESPONSIBLE FOR COORDINATING THE LOCATION OF THE PLUMBING TIE-INS, SLEEVES UNDER PAVEMENTS (AS NECESSARY), AND CONTROL DEVICES WITH THE GENERAL CONTRACTOR, OWNER, AND OWNER'S REPRESENTATIVE.
8. CONTRACTOR TO COORDINATE INSTALLATION OF IRRIGATION SYSTEM WITH EXISTING AND PROPOSED UTILITIES, SITE DRAWDOWN SYSTEMS, AND OTHER CONTRACTORS.
9. CONTRACTOR SHALL PROMPTLY NOTIFY THE OWNER'S REPRESENTATIVE SHOULD ANY UTILITIES, NOT SHOWN ON THE PLANS, BE FOUND DURING INSTALLATION WORK.
10. WATERPROOF ALL WIRE CONNECTORS USING 3M "D/B" WATERPROOF CONNECTORS OR EQUIVALENT.
11. DRAINAGE LINES ARE TO BE PROVIDED AT SUFFICIENT INTERVALS TO PROVIDE COMPLETE DRAINAGE OF ALL PIPING.
12. COORDINATE THE LOCATION OF CONTROLS, IRRIGATION CONTROLLER, AND SOIL MOISTURE SENSORS WITH THE PROJECT TEAM.
13. IRRIGATION CONTRACTOR SHALL MAKE FIELD ADJUSTMENTS TO IRRIGATION DESIGN WHERE REQUIRED TO PROVIDE 100% COVERAGE OF ALL LANDSCAPE AREAS, AS DESIGNATED ON THIS PLAN.
14. INSTALLATION MUST COMPLY WITH ALL LOCAL CODES AND CONDITIONS.
15. ALL IRRIGATION WORK SHALL BE GUARANTEED FOR 1 YEAR AFTER COMPLETION OF ALL WORK.
16. CONTRACTOR TO PROVIDE THREE (3) COPIES OF AS-BUILTS, SERVICE MANUALS AND INSTRUCTIONS TO THE OWNER OR OWNER'S REPRESENTATIVE.
17. ALL SPRINKLER HEADS SHALL BE SET BACK 4" MINIMUM FROM BACK OF ALL CURBS.
18. CONTRACTOR MAY SUBMIT ALTERNATE EQUIVALENT MATERIALS FOR REVIEW AND APPROVAL BY OWNER'S REPRESENTATIVE OR PROJECT LANDSCAPE ARCHITECT.

### GENERAL NOTE:

DUE TO GENERAL CONSTRUCTION ACTIVITIES AND ADJACENT SITE COMPACTION REQUIREMENTS, SUBGRADE SOILS WITHIN PROPOSED PLANTING AREAS TEND TO BECOME HIGHLY COMPACTION. IN ORDER TO CREATE A HEALTHY GROWTH MEDIUM TO ALLOW PROPOSED PLANTINGS TO ESTABLISH A VIGOROUS ROOT MASS, THIS SUBGRADE SOIL MUST UNDERGO A RESTORATION PROCESS. IN ADDITION, IMPORTED OR AMENDED EXISTING SOILS SHALL BE MIXED WITH SUBGRADE SOILS WHERE THEY MEET IN ORDER TO CREATE A TRANSITIONAL GRADIENT TO ALLOW FOR PROPER DRAINAGE.

6" IMPORTED PLANTING SOIL (OR AMENDED EXISTING PLANTING SOIL) SHALL BE ROTO-TILLED INTO SUBGRADE TO A DEPTH OF 12".

1. CONTRACTOR IS RESPONSIBLE TO SEND SAMPLES OF EXISTING SOILS INTENDED FOR USE IN PLANTING AREAS (1 PER 500 CY) TO TESTING LABORATORY OR UNIVERSITY COOPERATIVE EXTENSION FOR TESTING. ALL TESTING COSTS ARE AT THE CONTRACTOR'S EXPENSE.

2. RECYCLED CRUSHED CONCRETE AND ASPHALT MILLINGS SHALL NOT BE PLACED WITHIN 2"-6" OF FINISH GRADE IN PROPOSED LANDSCAPE AREAS.

3. IMPORTED FILL SHALL CONTAIN NO CONTAMINATION IN EXCEDENCE OF THE APPLICABLE STATE

27 August 2025, 18 November 2025  
Revised 29 December 2025

Jacob LaFontaine  
Planning Director  
Salem Planning Division  
33 Geremonty Drive  
Salem, NH 03079

Re: Traffic & Parking Utilization Study  
Proposed Reuse of Former Sears Auto Center  
The Mall at Rockingham Park  
Salem, NH  
Langan Project No.: 151064501

Dear Mr. LaFontaine:

Langan Engineering and Environmental Services, Inc. (Langan) has prepared this traffic and parking assessment for the proposed reuse of the former Sears Auto Center at the Mall at Rockingham Park ("the Mall") located at 1 Mall Road in Salem, New Hampshire. The Lyons Group proposes to convert the former auto center into a "Game On!" adult entertainment center and a "Loretta's Last Call", a country music themed restaurant, bar and dancing venue.

The analysis presented herein shows a reduction in expected traffic volumes when compared to the previously approved development plan. There is also ample parking capacity in the north lots to accommodate the planned development. Parking observations are consistent with those previously reported in the 2017 *Parking Utilization Study for the Redevelopment/Expansion of The Mall at Rockingham Park*<sup>1</sup> and the 2023 *Parking Utilization Study for the Dick's House of Sport at The Mall at Rockingham Park*<sup>2</sup>. This letter report presents the parking demand for the surface lots located at the north end of the mall and the anticipated changes in traffic compared to previous approvals.

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<sup>1</sup> *Parking Utilization Study for Redevelopment/Expansion of The Mall at Rockingham Park*, Rockingham Park Boulevard, Salem, New Hampshire, Langan Engineering and Environmental Services, May 2017

<sup>2</sup> *Parking Utilization Study for Dick's House of Sport at The Mall at Rockingham Park*, Rockingham Park Boulevard, Salem, New Hampshire, Langan Engineering and Environmental Services, May 2023, Revised July 2023



**Figure 1: Site Aerial Photograph**

## PROJECT DESCRIPTION

Lyons Group plans to repurpose and renovate the approximately 19,600 square foot former Sears Auto Center in the north parking lot the Mall to include a  $\pm 14,158$  SF, 143 seat "Game On!" adult entertainment center and a  $\pm 5,343$  SF, 194 seat (that includes 59 seasonal outdoor patio seats) "Loretta's Last Call" restaurant/bar/dance venue. This space was previously approved as 19,600 SF of restaurant and retail space as part of the Cinemark movie theater project in 2018. The proposed renovation of the Lyons Group includes full interior remodeling and exterior improvements such as landscaping and seasonal outdoor seating. The  $\pm 1.67$ -acre lot presently includes 84 parking spaces. The proposed site plan eliminates 16 parking spaces in lot C (See Figure 2) to accommodate the restaurant's entryway and patio seating, which would leave 1,070 parking spaces in lot C.

"Game On!" is expected to operate seven days a week and open from 11:30 AM to 2:00 AM on weekends and 11:30 AM to 12:00 AM on weekdays. "Loretta's Last Call" is expected to operate seven days a week and open from 4:00 PM to 2:00 AM on weekends and 4:00 PM to 1:00 AM on weekdays. "Loretta's" will likely open for brunch on Saturdays and Sundays from 10:30 AM to 4:00 PM. The project is expected to be completed by summer/fall 2026.

The overall mall provides approximately 4,563 spaces, which includes the proposed parking lot renovations. An additional 500 spaces are available during peak shopping periods under an easement with Tuscan Village bringing the overall total to 5,063 during peak parking demand periods.

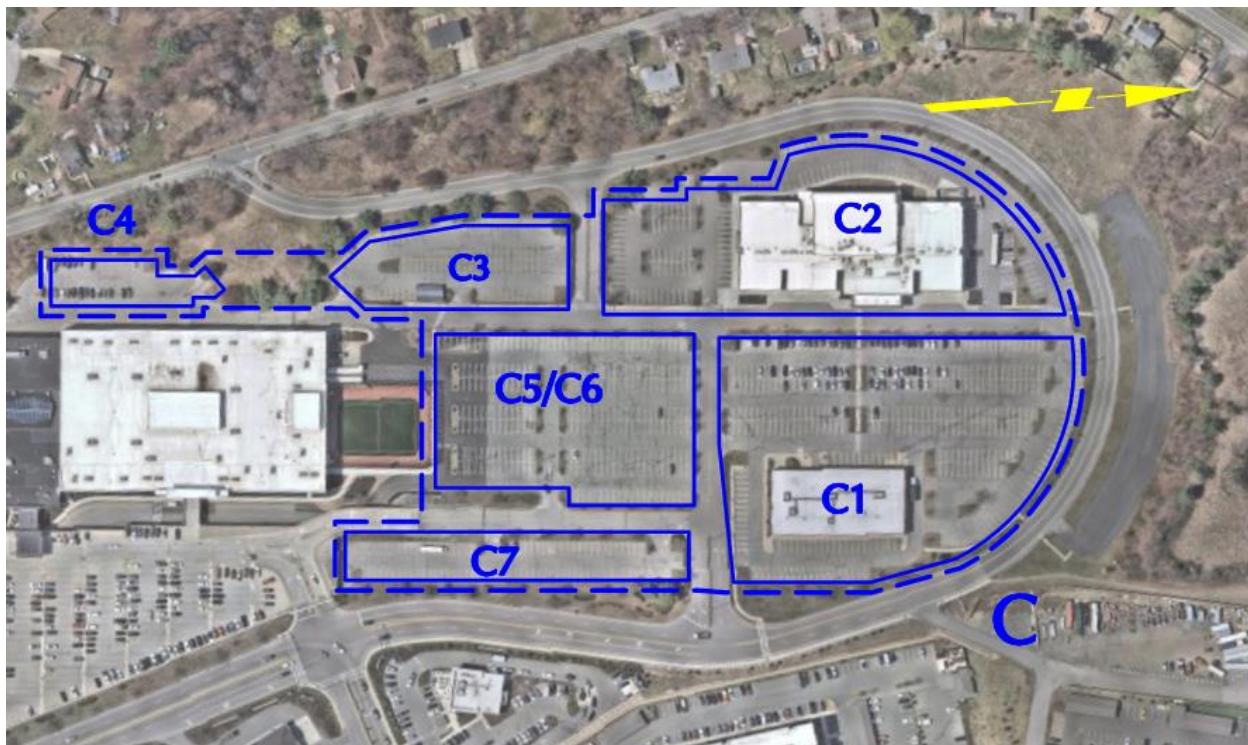


Figure 2: Parking Utilization Study Area

## BACKGROUND

Langan previously prepared an overall parking utilization study entitled *Parking Utilization Study for Redevelopment/Expansion of The Mall at Rockingham Park* during the 2016 Christmas shopping season. The study evaluated the feasibility of adding retail space at the mall and collected parking counts for the entire mall during the peak holiday season on Black Friday, the busiest shopping day of the year, two Saturdays – one in early December and one in mid-January<sup>3</sup>. The study found that the mall has sufficient parking spaces to serve the parking demand with the proposed redevelopment with exception of Black Friday, a once-a-year event, when parking was at capacity. Table 2 from the original parking study showing both peak and non-peak parking supply and demand are provided in the Appendix. The addition of the cinema and retail uses resulted in the issuance of a Conditional Use Permit decreasing the required Mall parking from 5,123 spaces to 4,645 spaces.

<sup>3</sup> Parking utilization was recorded on Friday, November 25, (Black Friday), Saturday, December 10, 2016 and Saturday January 14, 2017

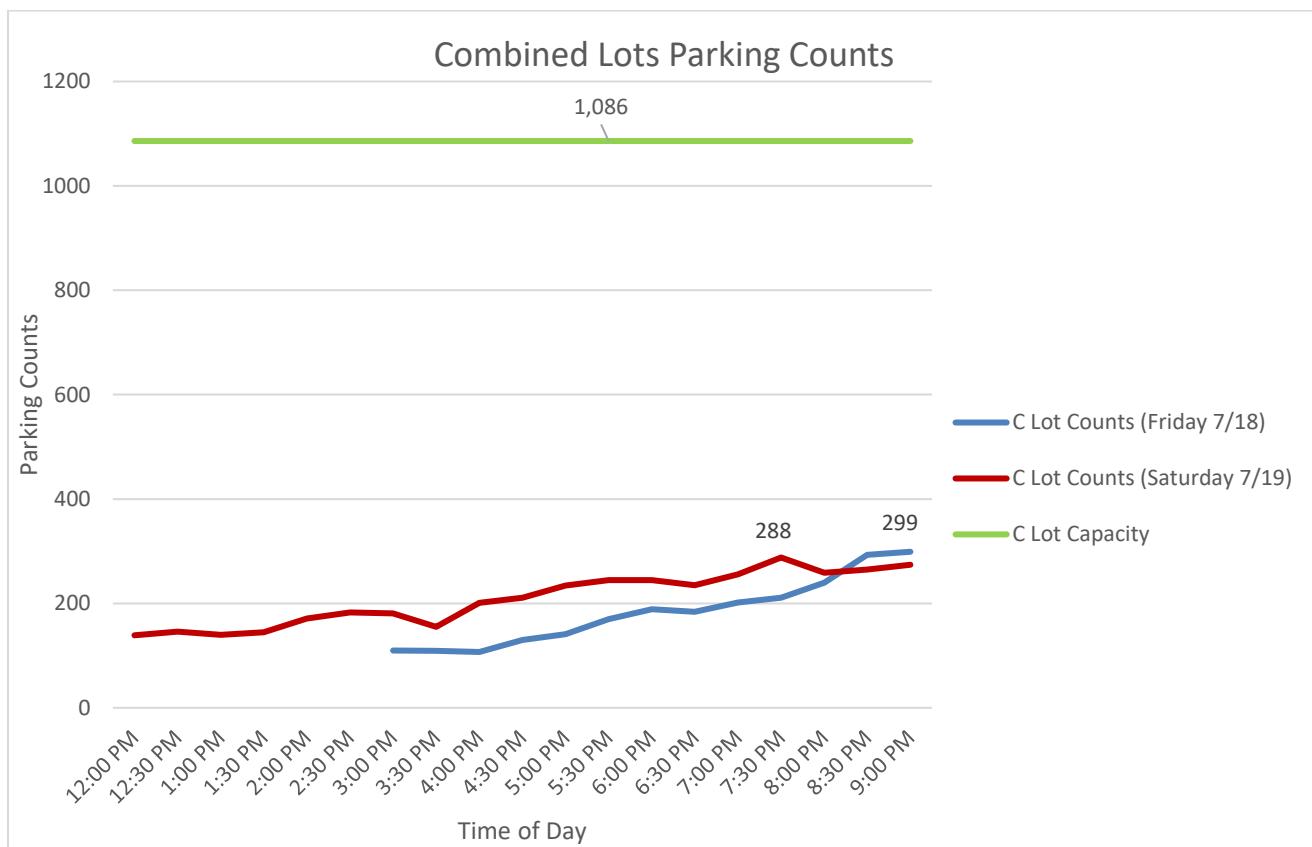
In 2023, Langan updated the 2017 study finding for Dick's Sporting Goods. Dick's renovated the former Sears store at the north end of the Mall and added an outdoor demonstration field. The addition of the field resulted in the loss of 58 spaces in Parking Lot C of the Mall, bringing the Lot C parking to 1,086. A Conditional Use Permit was granted by the Salem Planning Board further reducing the required mall parking from 4,645 to 4,587 overall spaces.

In 2024, Langan updated the 2017 and 2023 study findings for the Live! charitable gaming facility that was approved for the former Lord & Taylor store at the southwest end of the Mall. These renovations reduced the parking count by 8 spaces in Parking Lot A to add accessible spaces, bringing the Lot C parking to 1,486. A Conditional Use Permit was granted by the Salem Planning Board further reducing the required mall parking from 4,587 to 4,579.

## **PARKING UTILIZATION STUDY**

We performed manual parking counts every 30 minutes on Friday, July 18<sup>th</sup> between 3:00 PM and 9:00 PM and Saturday July 19<sup>th</sup> between 12:00 PM and 9:00 PM to document parking utilization. We performed parking counts in the northern C lots as the parking demand associated with the reuse will be isolated almost exclusively there. The C lots presently contain 1,086 striped spaces broken down into seven separate lots, C1 through C7, as shown in Figure 2. Demand in each of the sub lots was recorded separately. The raw parking counts for the two days surveyed are included in Appendix A.

Based on the data collected we found an average utilization rate of 17 percent on Friday July 18<sup>th</sup> and 19 percent on Saturday July 19<sup>th</sup>. The highest peak utilization on Friday occurred at 9:00 PM with a 28 percent occupancy (299 parked vehicles) with the final count of the evening. The highest peak on the Saturday counts occurred between 7:30 – 8:00 PM with a 27 percent occupancy (288 parked vehicles). Figure 3 below shows the parking utilization by time of day and compared that to the existing parking supply in the C lots.



**Figure 3: Parking Utilization by Time of Day**

#### Anticipated Parking Demand

The 2017 study was conducted to demonstrate the feasibility of additional development at the north end of the mall that consisted of the Cinemark movie theater, 15,000 SF of restaurants and 4,500 SF of generic retail space. Only one of the proposed uses outlined in that study, the movie theatre, has been constructed since the parking study was submitted to the Town in 2017. Since then, Dick's House of Sport has occupied the Sears anchor space and constructed an outdoor turf field in lot C5.

The remaining development projected in the 2017 study was a mix of restaurant/retail in the former Sears Auto Center. The ITE *Parking Generation Manual*<sup>4</sup>, 6<sup>th</sup> Edition was used to estimate the proposed renovation's peak parking demand. Under the renovation as part of this current application, this building will be entirely a restaurant/entertainment use. Lyons Group proposed two restaurants within the existing structure, "Game On!" and "Loretta's Last Call". "Game On!" contains 10 bowling lanes, pool tables, arcade space and a bar area. "Loretta's Last Call" is a western-themed restaurant and bar with a dance floor.

<sup>4</sup> Parking Generation Manual, 6<sup>th</sup> Edition published by the Institution of Transportation Engineers (ITE)

### ITE Land Use Code

Choosing a representative ITE land use code (LUC) to predict parking and traffic demand included consideration of a few land uses in that data base. There are codes for uses such as bowling alley and bar/tavern uses (LUC 437 and LUC 975, respectively). Data points are limited in both cases where daily trip estimates or parking demand are not available. Bowling alley as a use also generated parking demand that is significantly lower than expected for this type of mixed use facility. Land Use Code (LUC) 932 – High-Turnover (Sit-Down) Restaurant was found to be a use that could capture parking demand for both “Game On!” and “Loretta’s Last Call,” as both developments will serve food and have table seating for patrons. Seats were chosen to be the most appropriate independent variable for LUC 932, as “Game On!” has a large floor area space dedicated to bowling lanes, which is likely not typical of the standard High-Turnover (Sit-Down) Restaurant use. As the space for bowling lanes are not occupiable, using square footage as an independent variable will produce a significantly inflated parking demand. Additionally, estimating parking demand for “Loretta’s Last Call” resulted in lower anticipated parking demand compared to seats. A full land use description, data graphs, and a table with anticipated average and peak parking demand for “Loretta’s Last Call” using square footage as the independent variable are included in the appendix.

### Traffic & Parking Generation

The parking demand generation for the proposed renovations was estimated using parking generation data contained in the ITE *Parking Generation Manual*, 6<sup>th</sup> Edition <sup>5</sup>. **Table 1** below shows the resulting average parking demand, and **Table 2** shows the peak parking demand estimates for the proposed restaurants.

**TABLE 1**  
**ANTICIPATED AVERAGE PARKING DEMAND - SALEM NH LYONS**

USE	LAND USE CODE <sup>1</sup>	WEEKDAY (MON - THURS)		FRIDAY		SATURDAY	
		Average Parking Rate <sup>2</sup>	Anticipated Average Parking Demand	Average Parking Rate <sup>2</sup>	Anticipated Average Parking Demand	Average Parking Rate <sup>2</sup>	Anticipated Average Parking Demand
Loretta's Last Call (194 Seats)	932	0.28 spaces/seat	54	0.47 spaces/seat	91	0.38 spaces/seat	74
Game On! (143 Seats)	932	0.28 spaces/seat	40	0.47 spaces/seat	67	0.38 spaces/seat	54
<b>Total Average Parking Demand</b>		<b>0.28 spaces/seat</b>	<b>94</b>	<b>0.47 spaces/seat</b>	<b>158</b>	<b>0.38 spaces/seat</b>	<b>128</b>

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

<sup>5</sup> Parking Generation Manual, 6<sup>th</sup> Edition published by the Institution of Transportation Engineers (ITE).

TABLE 2 ANTICIPATED PEAK (85TH PERCENTILE) PARKING DEMAND - SALEM NH LYONS							
USE	LAND USE CODE <sup>1</sup>	WEEKDAY (MON - THURS)		FRIDAY		SATURDAY	
		Peak Parking Rate <sup>2</sup>	Anticipated Peak Parking Demand	Peak Parking Rate <sup>2</sup>	Anticipated Peak Parking Demand	Peak Parking Rate <sup>2</sup>	Anticipated Peak Parking Demand
Loretta's Last Call (194 Seats)	932	0.48 spaces/seat	93	0.79 spaces/seat	153	0.56 spaces/seat	109
Game On! (143 Seats)	932	0.48 spaces/seat	69	0.79 spaces/seat	113	0.56 spaces/seat	80
<b>Total Average Parking Demand</b>		<b>0.48 spaces/seat</b>	<b>162</b>	<b>0.79 spaces/seat</b>	<b>266</b>	<b>0.56 spaces/seat</b>	<b>189</b>

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

The proposed renovations are expected to generate an average demand for 158 parking spaces on a Friday and 128 parking spaces on a Saturday, and a peak demand for 266 parking spaces on a Friday and 189 spaces on a Saturday during the off-peak season in the C parking section. The Parking Generation Manual does not provide a peak season/off-peak season parking generation differential for LUC 932.

This analysis simply overlays the estimated parking demand using ITE Parking Generation Manual with parking demands measured at the in the field at the Mall at Rockingham Park. We did not take into account shared usage with the traditional retail components of the mall. Demand at Cinemark and the proposed restaurants is lowest when the rest of the mall is most active and peaks later in the evening when retail traffic in the mall is diminishing. This combined with the fact that the existing parking utilization plus peak future restaurant parking demand in Lot C would result in 53% utilization in Lot C during off-peak seasons suggest that the results of this analysis are conservative and general day to day operations will be less impactful.

The trip generation for the proposed renovations was estimated using trip generation data contained in the ITE *Trip Generation Manual*, 11<sup>th</sup> Edition <sup>6</sup>. **Table 3** below shows the resulting trip generation estimates for the proposed restaurants using seats as the independent variable. The proposed renovation estimates 132 evening peak hour trips and 1,473 weekday daily trips.

<sup>6</sup> Trip Generation Manual, 11<sup>th</sup> Edition published by the Institution of Transportation Engineers (ITE).

**TABLE 3**  
**ANTICIPATED TRIP GENERATION - SALEM NH LYONS**

USE	LAND USE CODE <sup>1</sup>	PM PEAK HOUR			WEEKDAY DAILY TOTAL	SAT PEAK HOUR			SATURDAY DAILY TOTAL
		ENTER	EXIT	TOTAL		ENTER	EXIT	TOTAL	
Loretta's Last Call (194 Seats)	932	43	33	76	848	54	49	103	1086
Game On! (143 Seats)	932	32	24	56	625	40	36	76	801
<b>Total New Trips</b>		<b>75</b>	<b>57</b>	<b>132</b>	<b>1473</b>	<b>94</b>	<b>85</b>	<b>179</b>	<b>1887</b>
Previously Approved Restaurant/Retail (19,600 SF)	-	94	64	158	2022	121	106	227	2508
<b>Net Change in Trips</b>		<b>-19</b>	<b>-7</b>	<b>-26</b>	<b>-549</b>	<b>-27</b>	<b>-21</b>	<b>-48</b>	<b>-621</b>

<sup>1</sup> Land Use Codes based on ITE Trip Generation Manual 11th Edition

<sup>2</sup> Volume based on ITE Trip Generation Manual 11th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

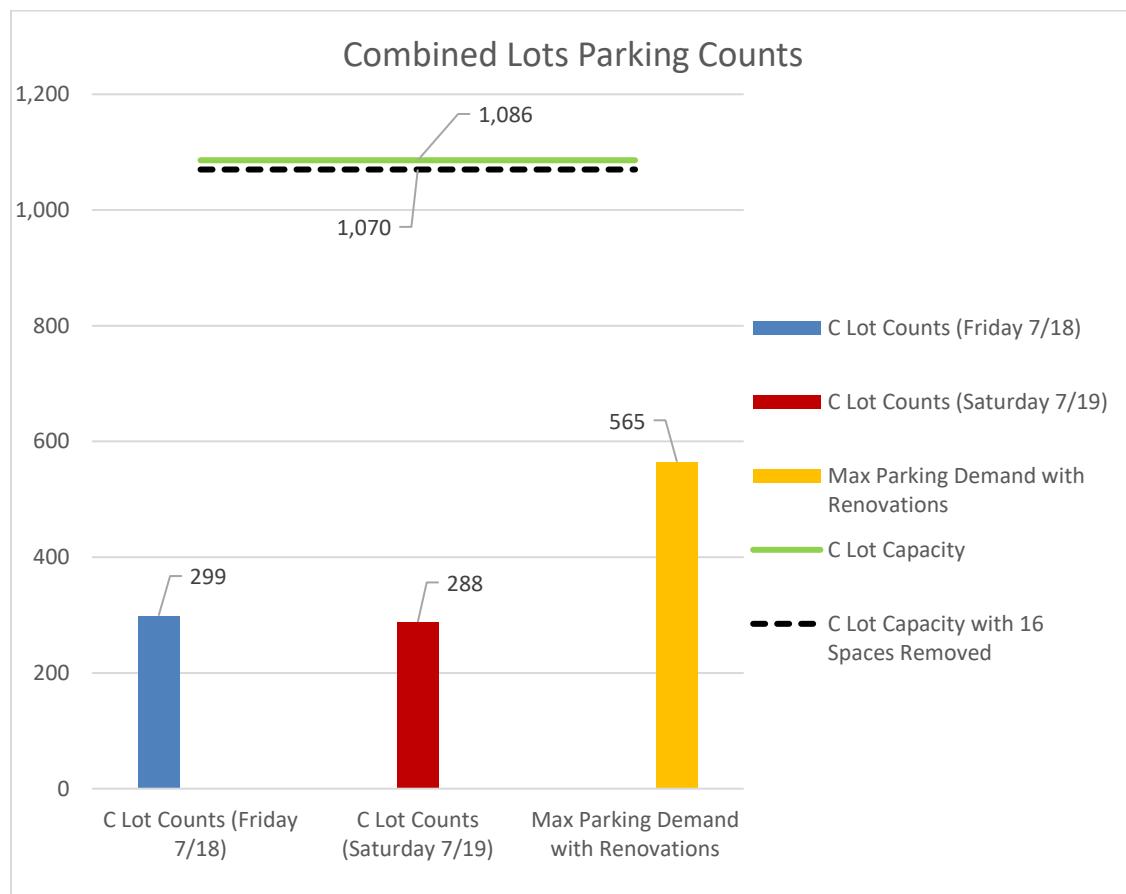
<sup>3</sup> Volume based on report titled "Traffic Impact Study - Redevelopment/Expansion of the Mall at Rockingham Park" by Langan dated June 2017.

The previously approved trip generation for this site (former Sears Auto Center) is also shown in **Table 3**. The proposed renovation proposes a reduction in 26 weekday and 48 Saturday evening peak hour trips, and 549 weekday and 621 Saturday daily trips than the previous approvals.

Similar to the parking demand generation, a trip generation estimate was created using square footage as the independent variable for "Loretta's Last Call," and it was found that this generates less traffic than using seats as the independent variable. A table with anticipated trip generation for "Loretta's Last Call" using square footage as the independent variable is included in the appendix.

### Peak Parking Demand

Figure 4 shows the 2025 observed parking counts with the addition of the max peak daily parking demand (266) for the renovated restaurants.



**Figure 4: Parking Utilization by Time of Day with Renovations Added**

As shown in Figure 4, there will be a surplus of 505 parking spaces during the off-peak season in parking lot C. Therefore, with the addition of the proposed restaurants and the existing parking demand, parking lot C can easily accommodate the new development. The graph also clearly shows that the parking demand for the uses in parking lot C is complimentary to each other and to the traditional retail uses in the mall. Demand at Cinemark and the proposed restaurants is lowest when the rest of the mall is most active and peaks later in the evening when retail traffic in the mall is diminishing.

Parking demand at the Mall at Rockingham Park increases during the holiday season in late November and December. The 2023 Dick's parking study data was collected in Lot C on Black Friday and the following Saturday, both considered to be peak dates for the Mall. Black Friday peak parking was 645 occupied spaces and Saturday peaked with 175 occupied spaces. The peak Lot C parking demand with the proposed restaurant renovations on a Friday during the holiday

season is expected to be 911 occupied spaces out of 1,070 available<sup>7</sup>. This peak parking demand assumes that the peak retail parking utilization overlaps with the peak restaurant utilization, which is unlikely to coincide. This data indicates that during the holiday season, a considerable number of vacant spaces will be available in the overall mall parking supply.

#### Additional Parking Capacity

The Mall at Rockingham Park has an easement for an additional 500 spaces available during peak shopping periods at the Tuscan Village. The analysis presented herein suggest that implementation of this additional capacity is not necessary, as the Mall will be available to increase the capacity of the overall mall parking supply if needed.

#### **CONDITIONAL USE PERMIT**

The proponent will require an amended conditional use permit (CUP) for the overall mall parking requirement to account for the loss of 16 spaces around the former Sears Auto Center. The overall parking requirement under zoning for the Mall as per the restaurant renovations site plan submittal is 5,573 spaces. The estimated parking requirement under zoning in the November 2024 CUP was 5,123 spaces. The CUP must be amended to request a total 1,010 space reduction consisting of 478 (Cinemark), 58 (Dick's), 458 (Casino), and 16 (Lyons Group) for the proposed restaurant renovations. The total required parking will be reduced to 4,563 parking spaces. The analysis presented herein demonstrates that this small reduction in the overall parking count is inconsequential given the significant surplus in parking spaces in the north lots.

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<sup>7</sup> The addition of the restaurant's Friday parking demand (158) to Black Friday's peak peaking demand (645)

## CONCLUSION

Langan evaluated the parking impact and trip generation of the proposed renovations of the former Sears Auto Center to a restaurant use at the Mall at Rockingham Park. We combined field observations of parking in Area C adjacent to Cinemark and Dick's with ITE Parking Generation estimates. Key findings discussed herein are:

- Existing parking demand in Parking Lot C peaks at 28% utilization on Friday and 27% on Saturday in the evenings.
- The combination of existing parking utilization and anticipated parking demand would result in a 53% utilization rate of Parking Lot C during off-peak season.
- The Mall will require an amended Conditional Use Permit (CUP) to reduce the required parking to 4,563 to account for the removal of 16 parking spaces for the proposed restaurants.
- The estimated trip generation for the proposed restaurants is 26 less evening peak hour trips and 549 daily trips less than the previous approvals.

In conclusion, our analysis indicated that the proposed renovation/reuse of the former Sears Auto Center building by proposed restaurants can be accommodated under typical operating conditions based on the estimated parking requirement.

Sincerely,  
**Langan Engineering and Environmental Services, Inc.**



Christopher McLean, P.E.  
Project Engineer



Maximo Polanco, P.E.  
Senior Project Engineer

## Appendix A - Parking Count Data

In-House			ROCKINGHAM PARK		PARKING COUNTS			FRIDAY 7/18/2025	
Time	C1	C2	C3	C4	C5	C6	C7	Total	Utilization
3:00 PM	73	19	1	17	0	0	0	110	10%
3:30 PM	73	18	1	17	0	0	0	109	10%
4:00 PM	70	17	1	17	0	2	0	107	10%
4:30 PM	87	24	1	17	0	1	0	130	12%
5:00 PM	92	30	1	17	0	1	0	141	13%
5:30 PM	118	31	2	17	0	2	0	170	16%
6:00 PM	133	35	1	17	0	3	0	189	17%
6:30 PM	124	35	3	17	0	5	0	184	17%
7:00 PM	142	35	3	17	0	5	0	202	19%
7:30 PM	152	33	2	17	0	6	1	211	19%
8:00 PM	177	33	2	17	0	11	0	240	22%
8:30 PM	222	38	1	17	0	15	0	293	27%
9:00 PM	228	37	1	17	0	16	0	299	28%
Capacity	<b>389</b>	<b>191</b>	<b>106</b>	<b>43</b>	<b>259</b>		<b>98</b>	<b>1086</b>	
Average	<b>130</b>	<b>30</b>	<b>2</b>	<b>17</b>	<b>5</b>		<b>0</b>	<b>184</b>	<b>17%</b>

NDS		ROCKINGHAM PARK			PARKING COUNTS			SATURDAY 7/19/2025		
Time	C1	C2	C3	C4	C5	C6	C7	Total	Utilization	
12:00 PM	95	22	0	17	3		2	139	13%	
12:30 PM	98	22	4	17	3		2	146	13%	
1:00 PM	89	27	0	17	6		1	140	13%	
1:30 PM	92	28	0	17	7		1	145	13%	
2:00 PM	117	30	0	17	6		1	171	16%	
2:30 PM	121	34	1	17	9		1	183	17%	
3:00 PM	118	34	1	17	8		3	181	17%	
3:30 PM	109	25	0	17	3		1	155	14%	
4:00 PM	154	26	0	17	3		1	201	19%	
4:30 PM	158	27	0	17	7		2	211	19%	
5:00 PM	173	30	1	17	12		1	234	22%	
5:30 PM	183	32	2	17	10		1	245	23%	
6:00 PM	180	36	0	17	11		1	245	23%	
6:30 PM	171	33	1	17	12		1	235	22%	
7:00 PM	191	28	1	17	18		1	256	24%	
7:30 PM	230	23	1	17	16		1	288	27%	
8:00 PM	208	22	0	17	11		1	259	24%	
8:30 PM	218	14	0	17	15		1	265	24%	
9:00 PM	225	17	0	17	14		1	274	25%	
<b>Capacity</b>	<b>389</b>	<b>191</b>	<b>106</b>	<b>43</b>	<b>259</b>		<b>98</b>	<b>1086</b>		
<b>Average</b>	<b>154</b>	<b>27</b>	<b>1</b>	<b>17</b>	<b>9</b>		<b>1</b>	<b>209</b>	<b>19%</b>	

# Land Use: 932 High-Turnover (Sit-Down) Restaurant

## Description

A high-turnover (sit-down) restaurant is full-service eating establishment with a typical duration of stay of 60 minutes or less. This type of restaurant is usually moderately priced, frequently belongs to a restaurant chain, and is commonly referred to as casual dining. Generally, these restaurants serve lunch and dinner; they may also be open for breakfast and are sometimes open 24 hours a day. These restaurants typically do not accept reservations. A patron commonly waits to be seated, is served by wait staff, orders from a menu, and pays after the meal. Some facilities offer carry-out for a small proportion of their customers. Some facilities within this land use may also contain a bar area for serving food and alcoholic drinks.

## Land Use Subcategory

Data are separated into two subcategories for this land use:

- Restaurants that serve breakfast
- Restaurants that do not serve breakfast

The “serves breakfast” subcategory includes restaurants that serve customers during breakfast, lunch, and dinner; during breakfast and lunch; and during breakfast only. The “does not serve breakfast” subcategory includes restaurants that serve customers during lunch and dinner, during dinner only, and during lunch only.

## Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a weekday (Monday–Thursday) at restaurants that serve breakfast, lunch, and dinner (10 study sites) and at restaurants that serve only lunch and dinner (25 sites). The following table also presents a time-of-day distribution of parking demand on a Saturday at restaurants that serve breakfast, lunch, and dinner (nine study sites) and at restaurants that serve only lunch and dinner (six sites).

Hour Beginning	Percent of Monday–Thursday Peak Parking Demand		Percent of Saturday Peak Parking Demand	
	Serving Breakfast, Lunch, and Dinner	Serving Lunch and Dinner	Serving Breakfast, Lunch, and Dinner	Serving Lunch and Dinner
12:00–4:00 a.m.	–	–	–	–
5:00 a.m.	–	–	–	–
6:00 a.m.	–	–	–	–
7:00 a.m.	–	–	–	–
8:00 a.m.	64	–	55	–
9:00 a.m.	74	–	76	–
10:00 a.m.	82	–	91	–
11:00 a.m.	89	28	100	33
12:00 p.m.	100	96	97	56
1:00 p.m.	86	100	91	69
2:00 p.m.	57	51	73	58
3:00 p.m.	44	37	51	49
4:00 p.m.	39	34	43	63
5:00 p.m.	62	56	57	77
6:00 p.m.	73	87	66	100
7:00 p.m.	95	91	80	100
8:00 p.m.	76	73	62	85
9:00 p.m.	–	–	–	55
10:00 p.m.	–	–	–	35
11:00 p.m.	–	–	–	–

## **Additional Data**

If the restaurant has outdoor seating, its area is not included in the overall gross floor area. For a restaurant that has significant outdoor seating, the number of seats may have a more direct relationship to site-generated parking demand than GFA.

The average parking supply ratio for the 55 study sites with parking supply information in a general urban/suburban setting is 16 spaces per 1,000 square feet GFA. The average parking supply ratio for the five study sites with parking supply information in a dense multi-use urban setting is 11 spaces per 1,000 square feet GFA. The average peak parking occupancy is 74 percent at the general urban/suburban sites and 88 percent at the dense multi-use urban sites.

The sites were surveyed in the 1990s, the 2000s, the 2010s, and the 2020s in Arizona, British Columbia (CAN), California, Florida, Illinois, Indiana, Maine, Maryland, Massachusetts, Minnesota, New Jersey, New York, North Carolina, Oregon, Pennsylvania, Texas, Virginia, Washington, and Wisconsin.

## **Source Numbers**

168, 218, 274, 276, 299, 527, 531, 556, 557, 567, 568, 618, 619, 620, 622, 626, 628, 637

# High-Turnover (Sit Down) Restaurant Does Not Serve Breakfast (932)

Peak Period Parking Demand vs: Seats

On a: Weekday (Monday - Thursday)

Setting/Location: General Urban/Suburban

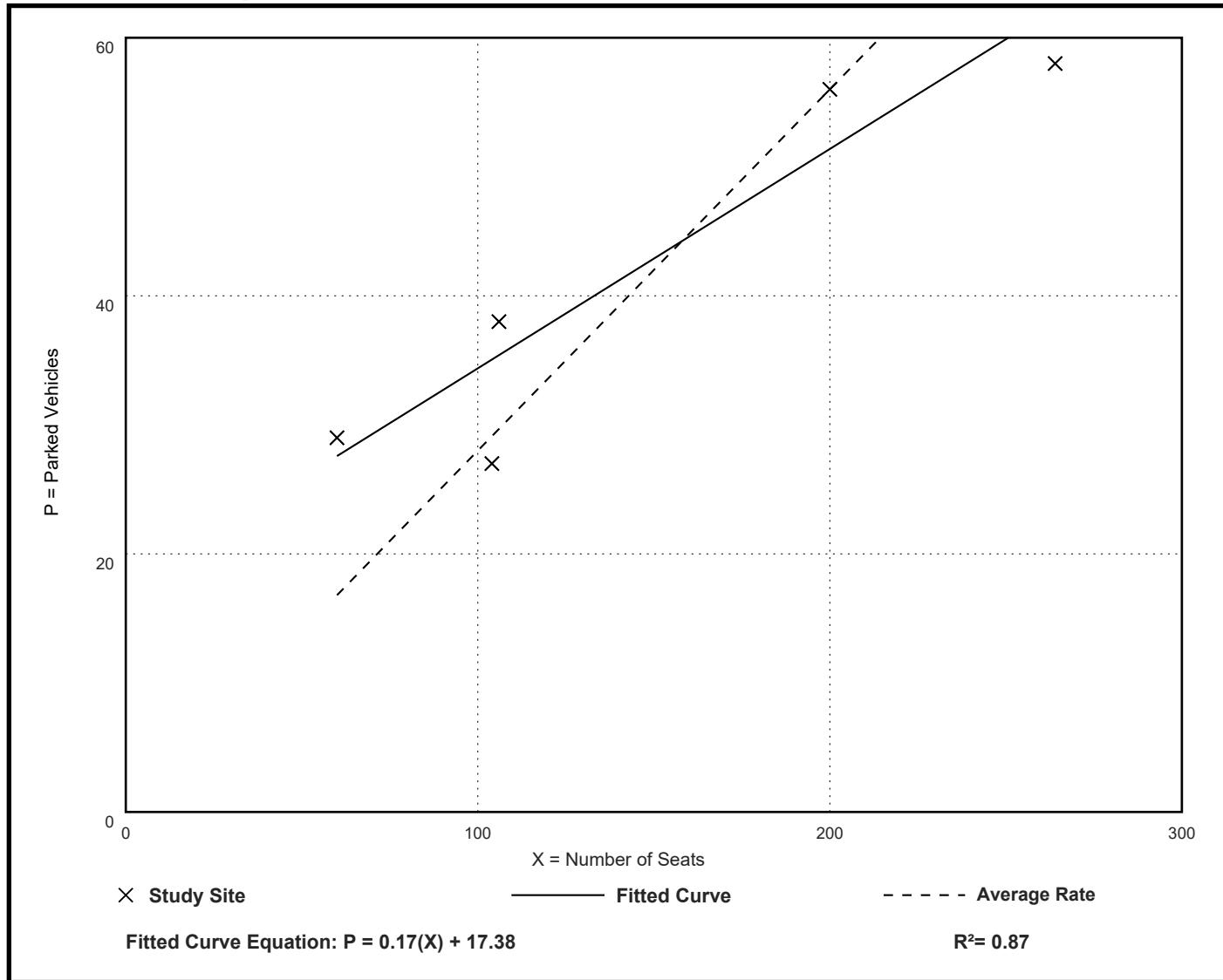
Number of Studies: 5

Avg. Num. of Seats: 147

## Peak Period Parking Demand per Seat

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
0.28	0.22 - 0.48	0.26 / 0.48	***	0.08 ( 29% )

## Data Plot and Equation



# High-Turnover (Sit Down) Restaurant Does Not Serve Breakfast (932)

Peak Period Parking Demand vs: Seats

On a: Friday

Setting/Location: General Urban/Suburban

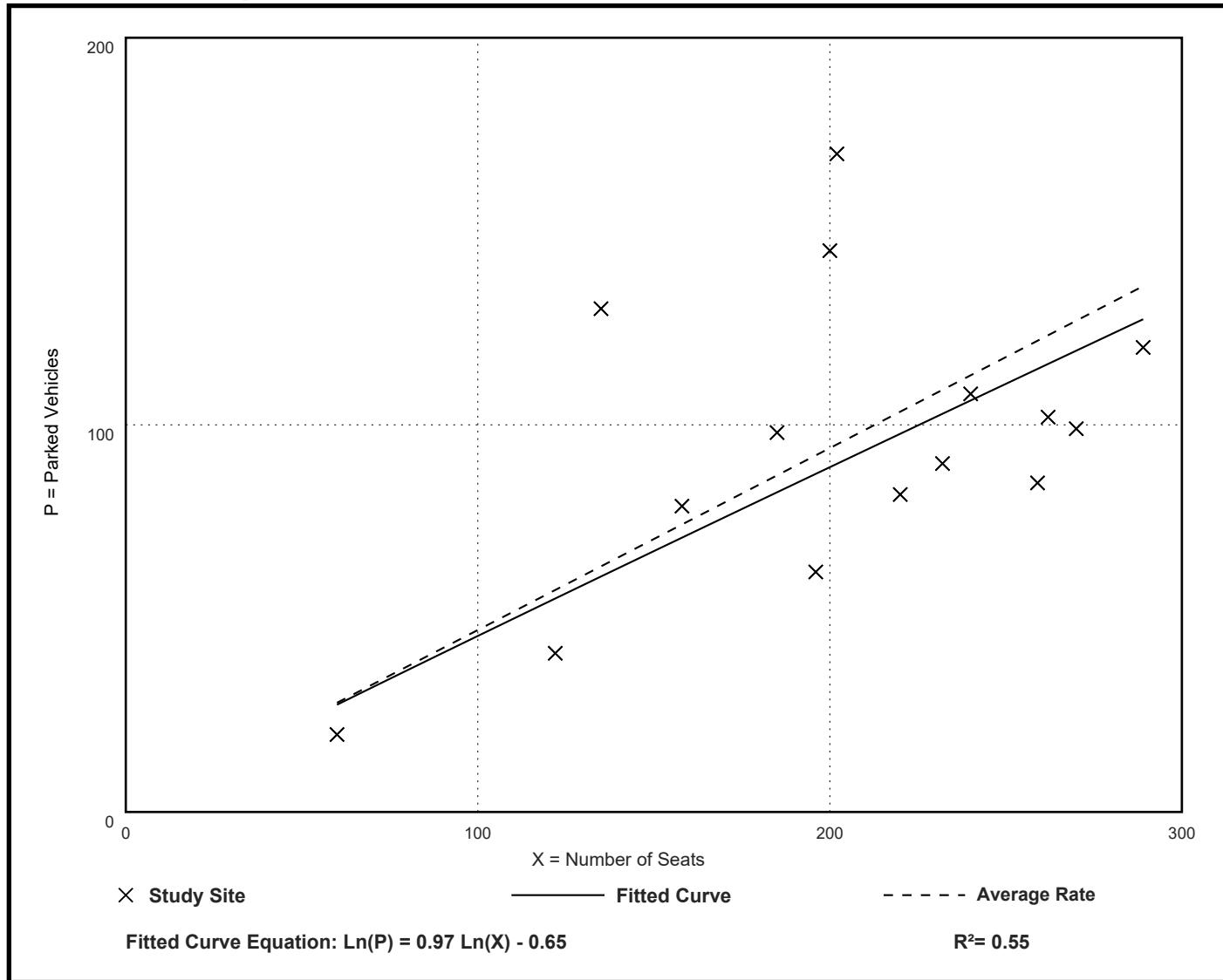
Number of Studies: 15

Avg. Num. of Seats: 202

## Peak Period Parking Demand per Seat

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
0.47	0.32 - 0.96	0.37 / 0.79	***	0.19 ( 40% )

## Data Plot and Equation



# High-Turnover (Sit Down) Restaurant Does Not Serve Breakfast (932)

Peak Period Parking Demand vs: Seats

On a: Saturday

Setting/Location: General Urban/Suburban

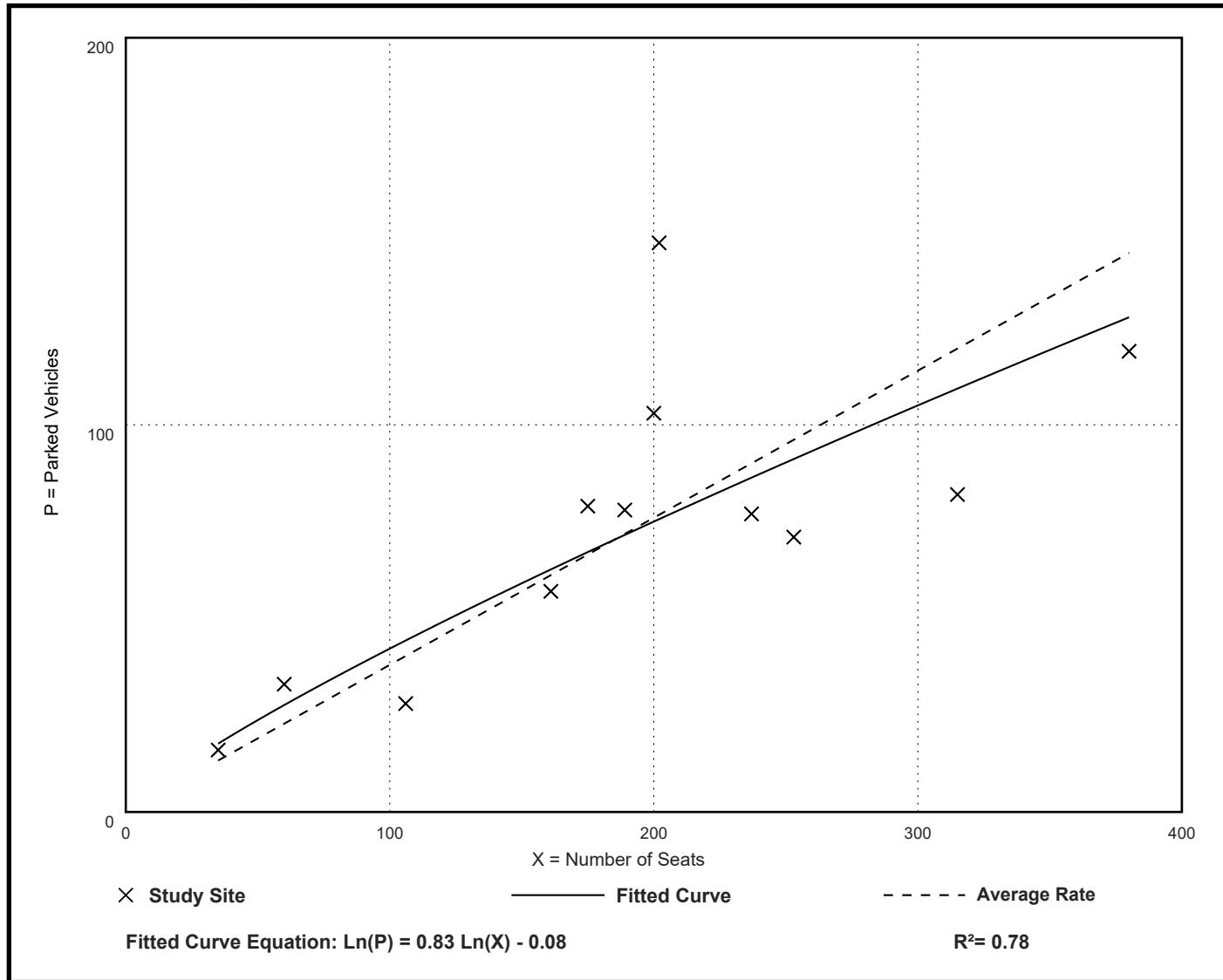
Number of Studies: 12

Avg. Num. of Seats: 193

## Peak Period Parking Demand per Seat

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
0.38	0.26 - 0.73	0.32 / 0.56	***	0.14 ( 37% )

## Data Plot and Equation



# Land Use: 437 Bowling Alley

## Description

A bowling alley is a recreational facility that includes bowling lanes. A small lounge, restaurant and/or snack bar, video games, and billiard tables may also be available.

## Time-of-Day Distribution for Parking Demand

The following table presents a time-of-day distribution of parking demand on a weekday at five study sites.

Hour Beginning	Percent of Weekday Peak Parking Demand
12:00–4:00 a.m.	—
5:00 a.m.	—
6:00 a.m.	—
7:00 a.m.	—
8:00 a.m.	—
9:00 a.m.	—
10:00 a.m.	—
11:00 a.m.	—
12:00 p.m.	—
1:00 p.m.	—
2:00 p.m.	—
3:00 p.m.	—
4:00 p.m.	—
5:00 p.m.	45
6:00 p.m.	87
7:00 p.m.	99
8:00 p.m.	100
9:00 p.m.	82
10:00 p.m.	—
11:00 p.m.	—

## **Additional Data**

Factors such as the extent of ancillary activities (for example, billiards, lounge) and transitions between activities (when one bowling league had not yet ended and the next was about to begin) may have affected the reported parking demand.

The average parking supply ratio for the four study sites with parking supply information is 5.6 spaces per lane. The average peak parking occupancy at these four sites is 56 percent.

The sites were surveyed in the 1990s in Oregon.

## **Source Number**

275

# Bowling Alley (437)

Peak Period Parking Demand vs: Lanes

On a: Weekday (Monday - Friday)

Setting/Location: General Urban/Suburban

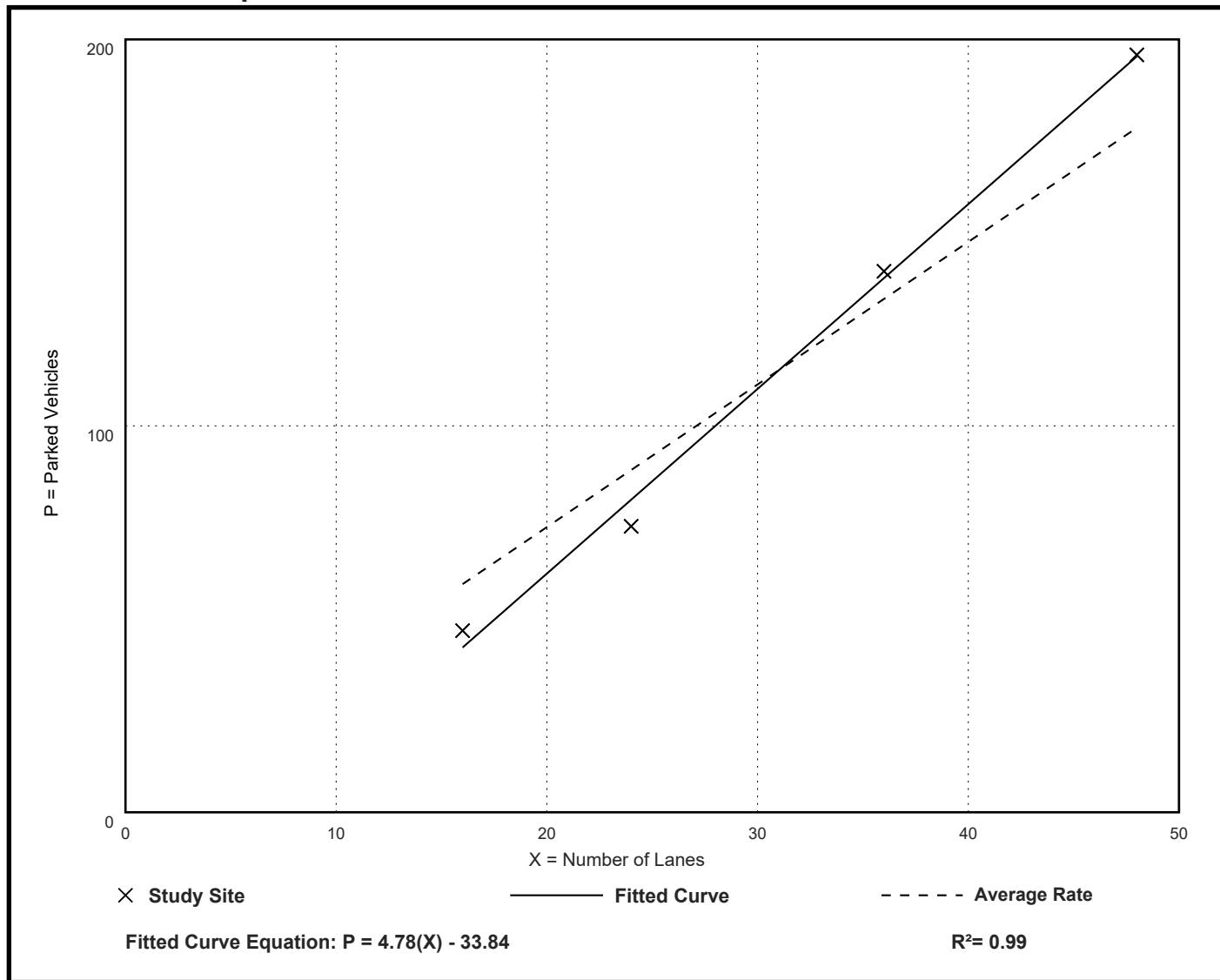
Number of Studies: 4

Avg. Num. of Lanes: 31

## Peak Period Parking Demand per Lane

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
3.69	2.94 - 4.08	3.03 / 4.08	***	0.54 ( 15% )

## Data Plot and Equation



# Bowling Alley (437)

Peak Period Parking Demand vs: Lanes

On a: Weekday (Monday - Friday)

Setting/Location: Dense Multi-Use Urban

Number of Studies: 1

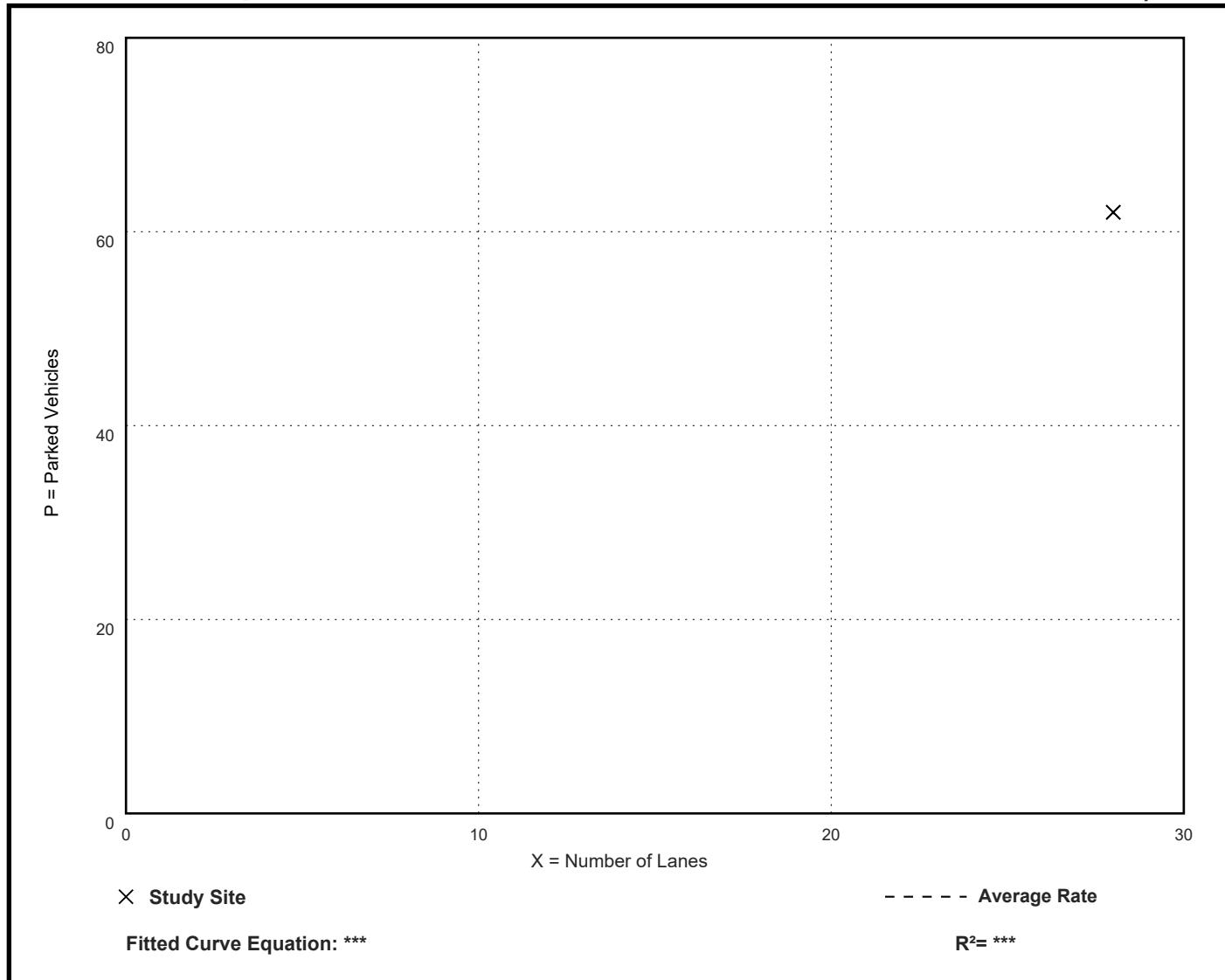
Avg. Num. of Lanes: 28

## Peak Period Parking Demand per Lane

Average Rate	Range of Rates	33rd / 85th Percentile	95% Confidence Interval	Standard Deviation (Coeff. of Variation)
2.21	2.21 - 2.21	*** / ***	***	*** ( *** )

## Data Plot and Equation

*Caution – Small Sample Size*



# Land Use: 932

## High-Turnover (Sit-Down) Restaurant

---

### Description

This land use consists of sit-down, full-service eating establishments with a typical duration of stay of 60 minutes or less. This type of restaurant is usually moderately priced, frequently belongs to a restaurant chain, and is commonly referred to as casual dining. Generally, these restaurants serve lunch and dinner; they may also be open for breakfast and are sometimes open 24 hours a day. These restaurants typically do not accept reservations. A patron commonly waits to be seated, is served by wait staff, orders from a menu, and pays after the meal.

Some facilities offer carry-out for a small proportion of its customers. Some facilities within this land use may also contain a bar area for serving food and alcoholic drinks.

Fast casual restaurant (Land Use 930), fine dining restaurant (Land Use 931), fast-food restaurant without drive-through window (Land Use 933), and fast-food restaurant with drive-through window (Land Use 934) are related uses.

### Additional Data

***Users should exercise caution when applying statistics during the AM peak periods, as the sites contained in the database for this land use may or may not be open for breakfast. In cases where it was confirmed that the sites were not open for breakfast, data for the AM peak hour of the adjacent street traffic were removed from the database.***

If the restaurant has outdoor seating, its area is not included in the overall gross floor area. For a restaurant that has significant outdoor seating, the number of seats may be more reliable than GFA as an independent variable on which to establish a trip generation rate.

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Florida, Georgia, Indiana, Kentucky, Massachusetts, Minnesota, New Hampshire, New Jersey, New York, Ohio, Oklahoma, Oregon, Pennsylvania, South Carolina, South Dakota, Texas, Vermont, and Wisconsin.

### Source Numbers

126, 269, 275, 280, 300, 301, 305, 338, 340, 341, 358, 384, 424, 432, 437, 438, 444, 507, 555, 577, 589, 617, 618, 728, 868, 884, 885, 903, 927, 939, 944, 961, 962, 977, 1048

# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: Seats  
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Seats: 148

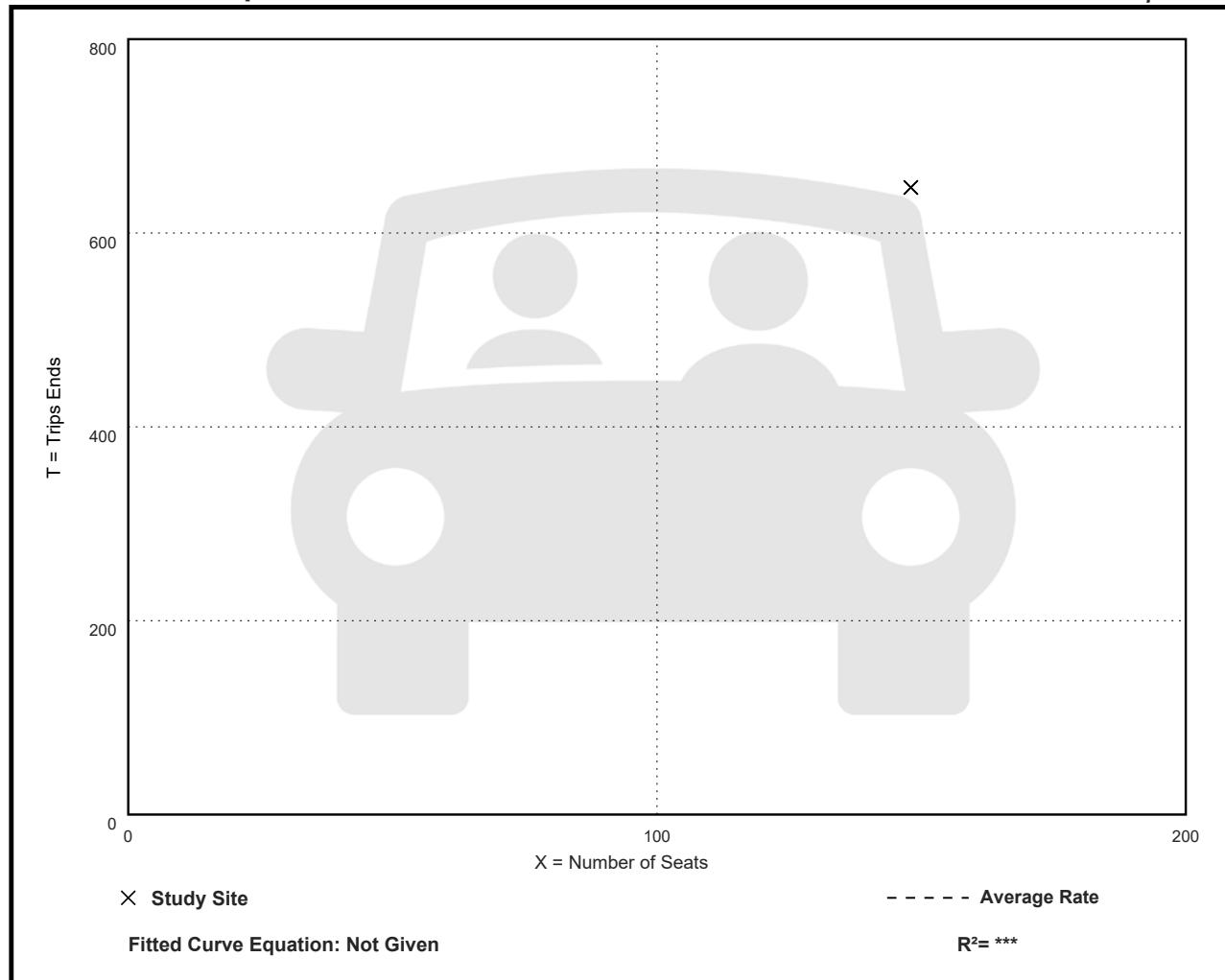
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Seat

Average Rate	Range of Rates	Standard Deviation
4.37	4.37 - 4.37	***

## Data Plot and Equation

*Caution – Small Sample Size*



# High-Turnover (Sit-Down) Restaurant (932)

## Vehicle Trip Ends vs: Seats

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 7

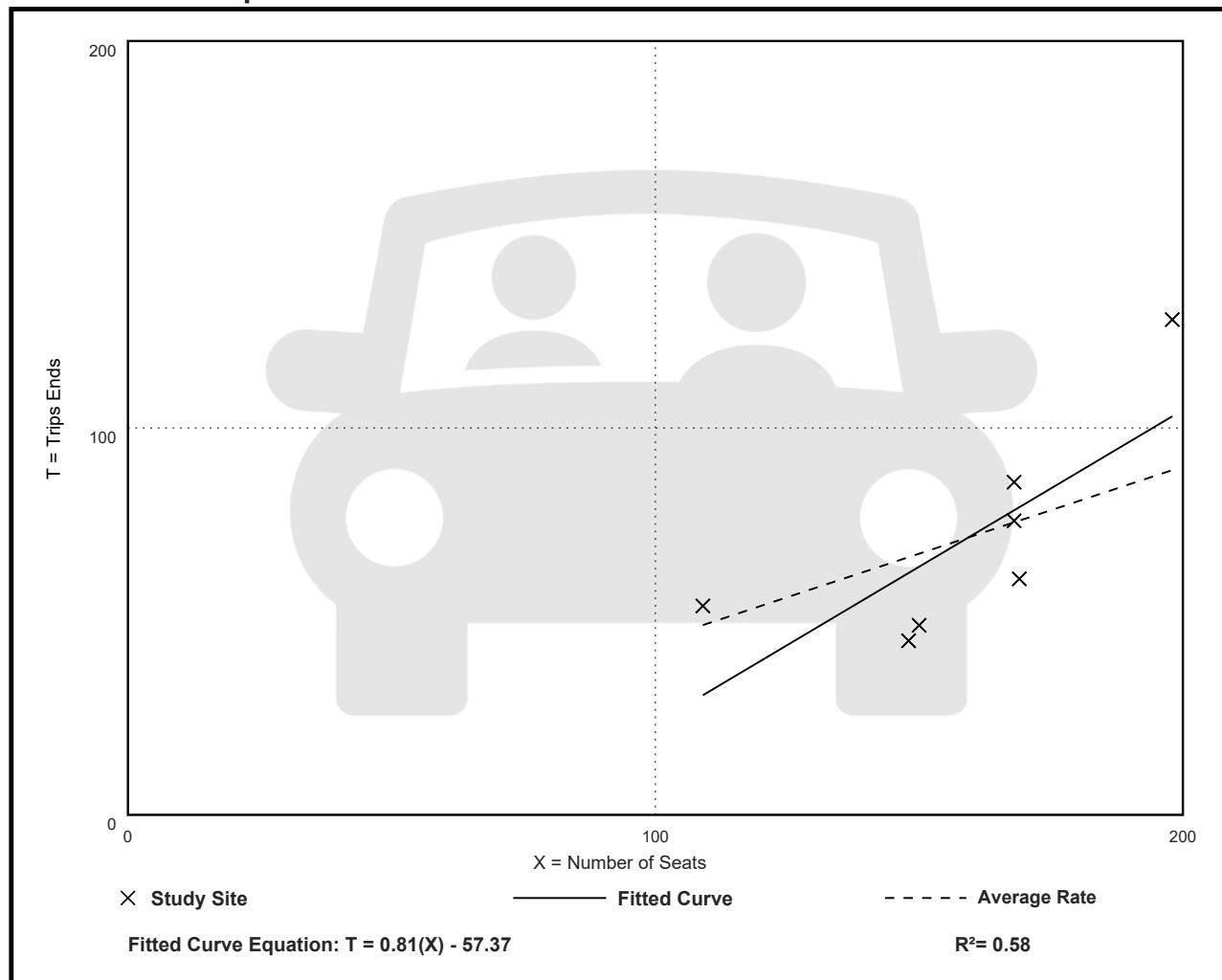
Avg. Num. of Seats: 159

Directional Distribution: 52% entering, 48% exiting

## Vehicle Trip Generation per Seat

Average Rate	Range of Rates	Standard Deviation
0.45	0.30 - 0.65	0.13

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

## Vehicle Trip Ends vs: Seats

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 14

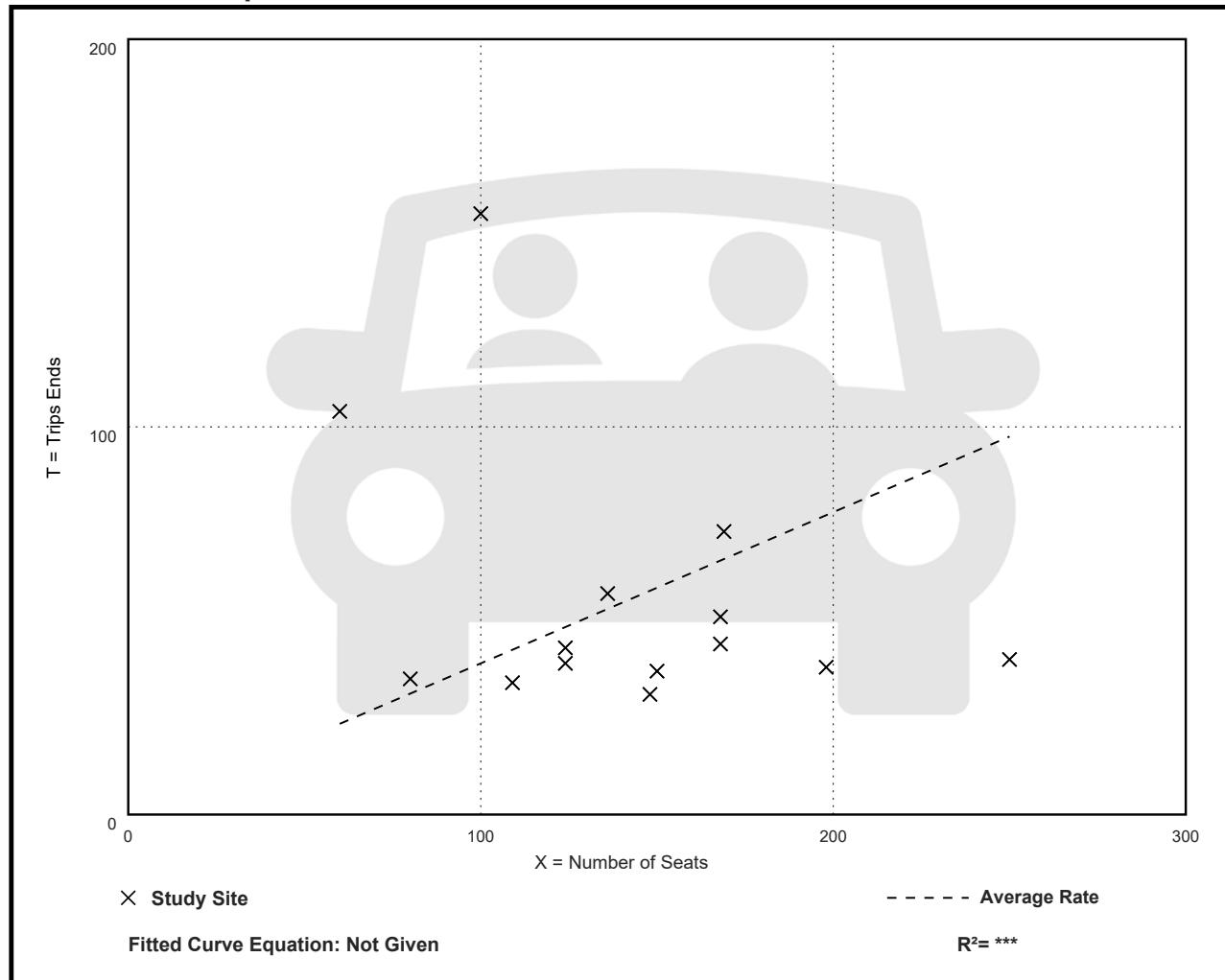
Avg. Num. of Seats: 142

Directional Distribution: 57% entering, 43% exiting

## Vehicle Trip Generation per Seat

Average Rate	Range of Rates	Standard Deviation
0.39	0.16 - 1.73	0.39

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: Seats

On a: Weekday,

AM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 5

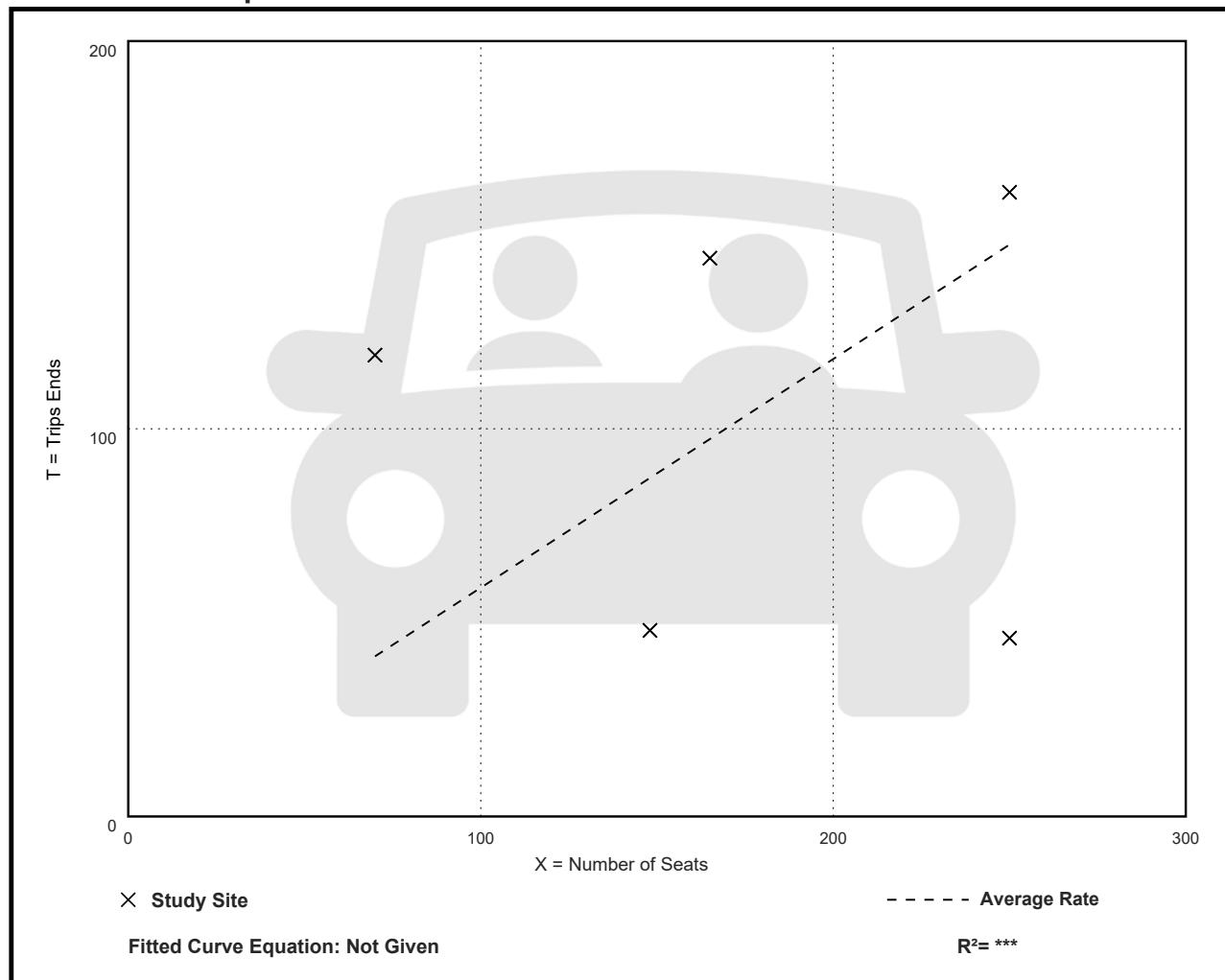
Avg. Num. of Seats: 177

Directional Distribution: 60% entering, 40% exiting

## Vehicle Trip Generation per Seat

Average Rate	Range of Rates	Standard Deviation
0.59	0.18 - 1.70	0.46

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: Seats

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 10

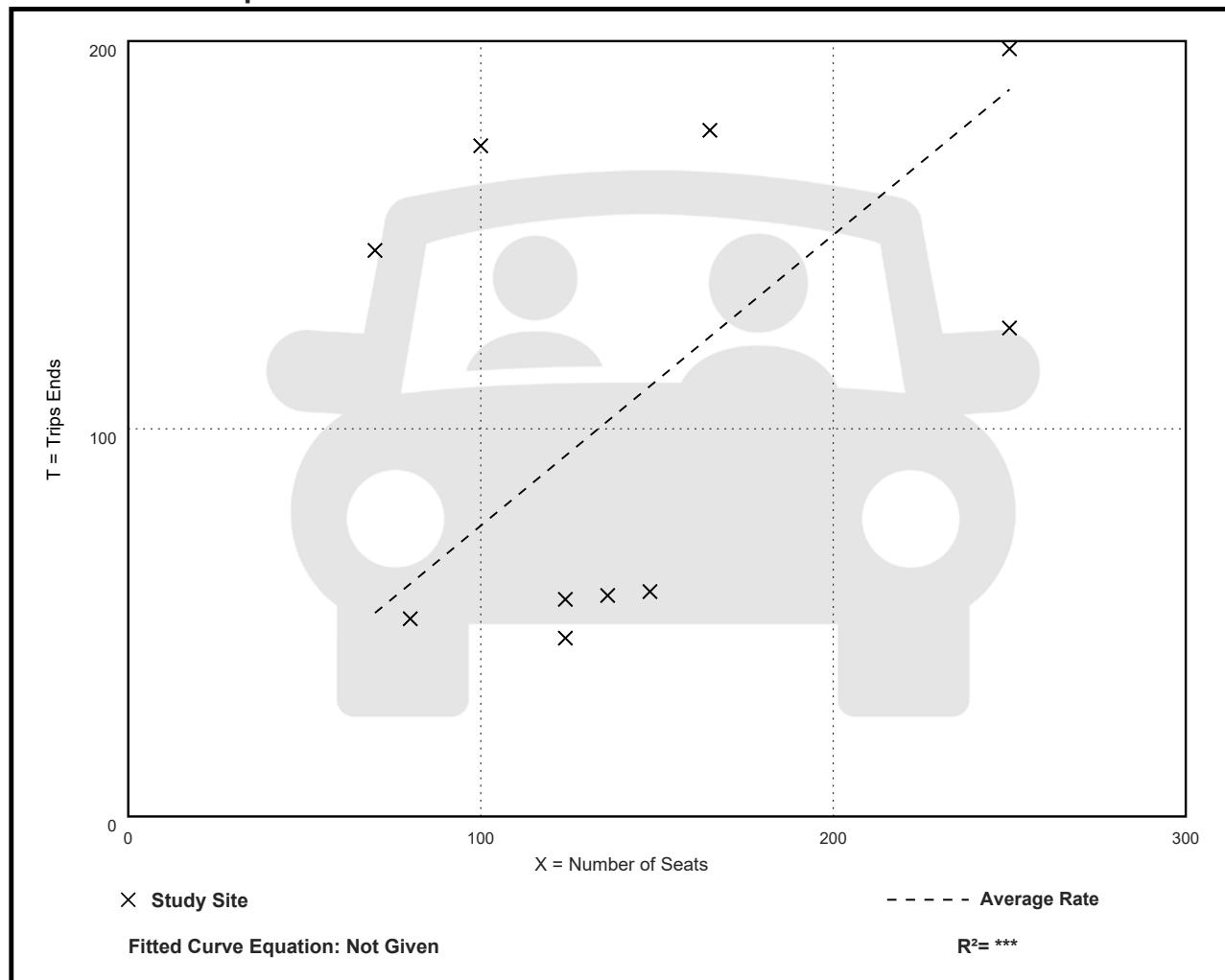
Avg. Num. of Seats: 145

Directional Distribution: 52% entering, 48% exiting

## Vehicle Trip Generation per Seat

Average Rate	Range of Rates	Standard Deviation
0.75	0.37 - 2.09	0.49

## Data Plot and Equation



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: Seats  
On a: Saturday

**Setting/Location: General Urban/Suburban**

Number of Studies: 1

Avg. Num. of Seats: 148

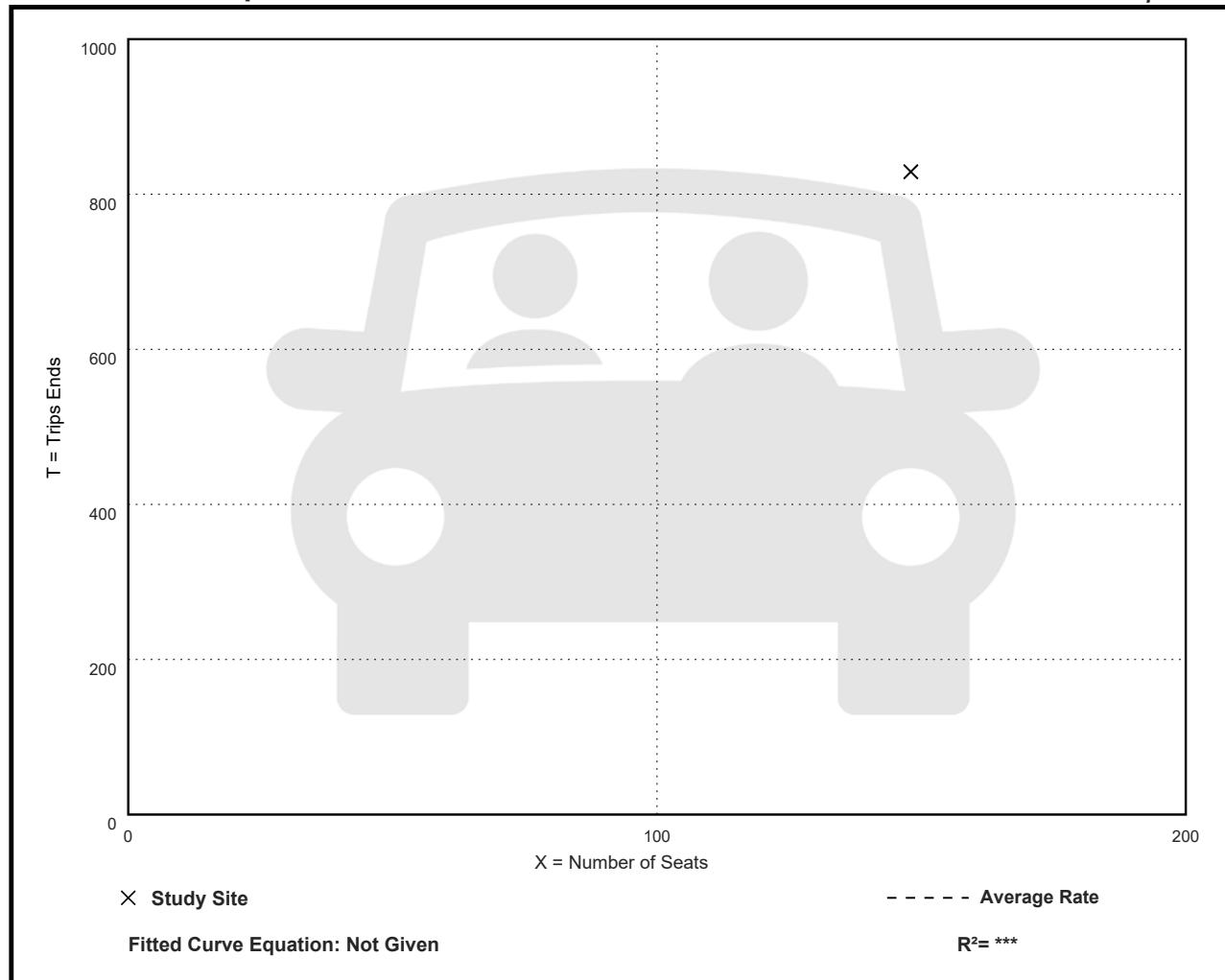
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Seat

Average Rate	Range of Rates	Standard Deviation
5.60	5.60 - 5.60	***

## Data Plot and Equation

*Caution – Small Sample Size*



# High-Turnover (Sit-Down) Restaurant (932)

Vehicle Trip Ends vs: Seats

On a: Saturday, Peak Hour of Generator

**Setting/Location: General Urban/Suburban**

Number of Studies: 8

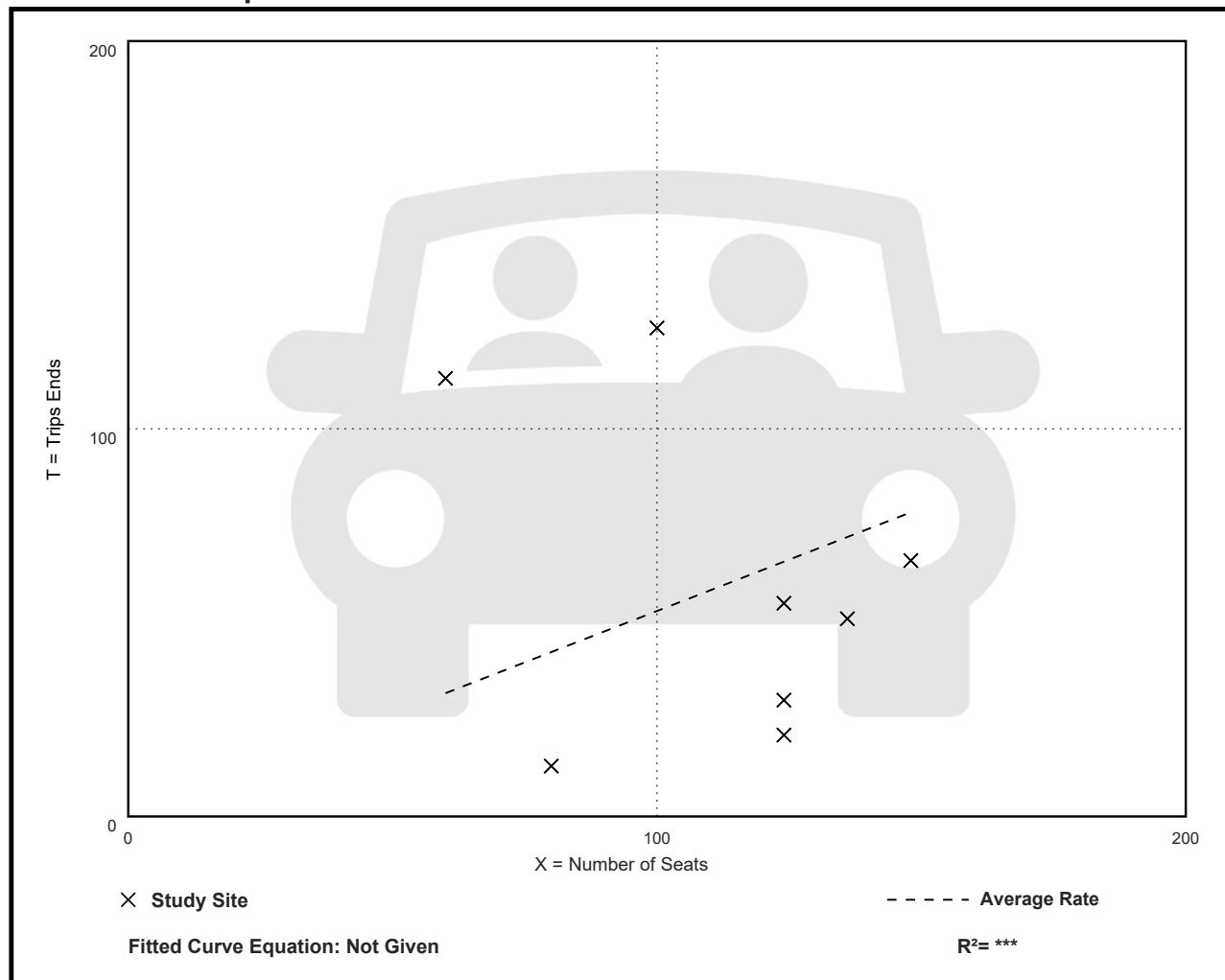
Avg. Num. of Seats: 112

Directional Distribution: 53% entering, 47% exiting

## Vehicle Trip Generation per Seat

Average Rate	Range of Rates	Standard Deviation
0.53	0.16 - 1.88	0.51

## Data Plot and Equation



## Land Use: 437 Bowling Alley

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### Description

A bowling alley is a recreational facility that includes bowling lanes. A small lounge, restaurant and/or snack bar, video games, and pool tables may also be available.

### Additional Data

The sites were surveyed in the 1990s, the 2000s, and the 2010s in Connecticut, Florida, and Texas.

### Source Numbers

400, 721, 945

# Bowling Alley (437)

## Vehicle Trip Ends vs: Bowling Lanes

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Bowling Lanes: 40

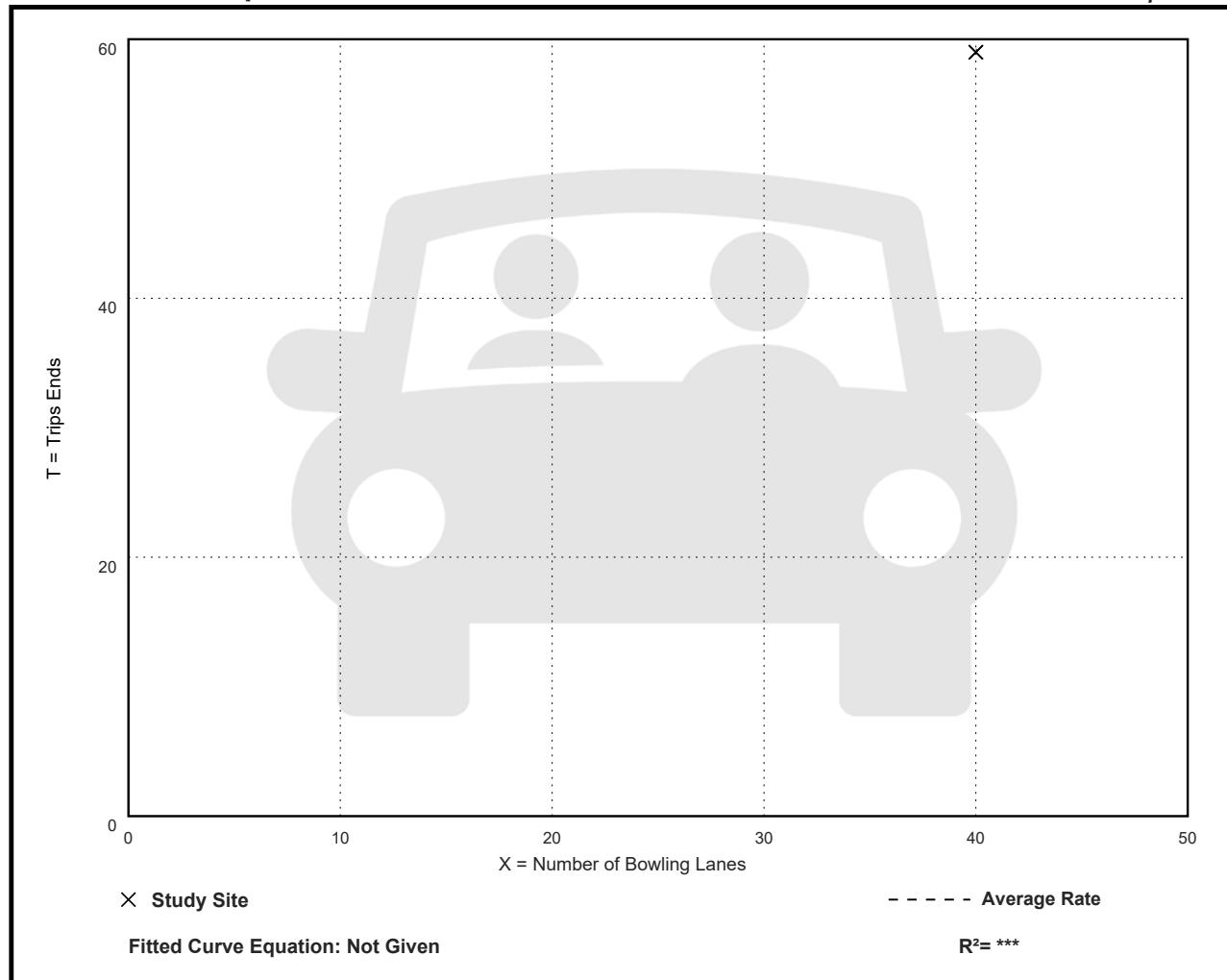
Directional Distribution: 95% entering, 5% exiting

## Vehicle Trip Generation per Bowling Lane

Average Rate	Range of Rates	Standard Deviation
1.48	1.48 - 1.48	***

## Data Plot and Equation

*Caution – Small Sample Size*



# Bowling Alley (437)

## Vehicle Trip Ends vs: Bowling Lanes

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

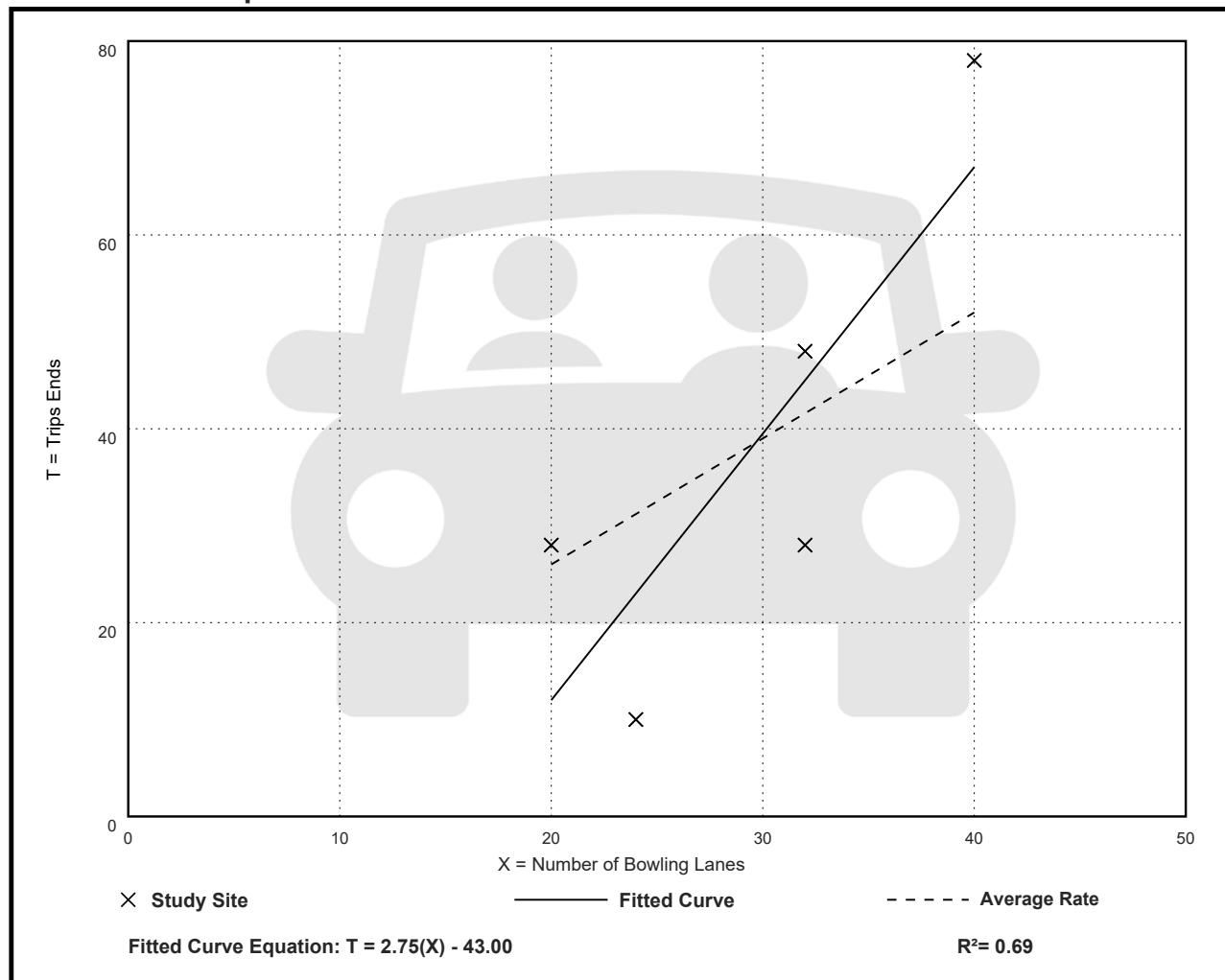
Avg. Num. of Bowling Lanes: 30

Directional Distribution: 65% entering, 35% exiting

## Vehicle Trip Generation per Bowling Lane

Average Rate	Range of Rates	Standard Deviation
1.30	0.42 - 1.95	0.60

## Data Plot and Equation



# Bowling Alley (437)

Vehicle Trip Ends vs: Bowling Lanes

On a: Weekday,

PM Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Bowling Lanes: 32

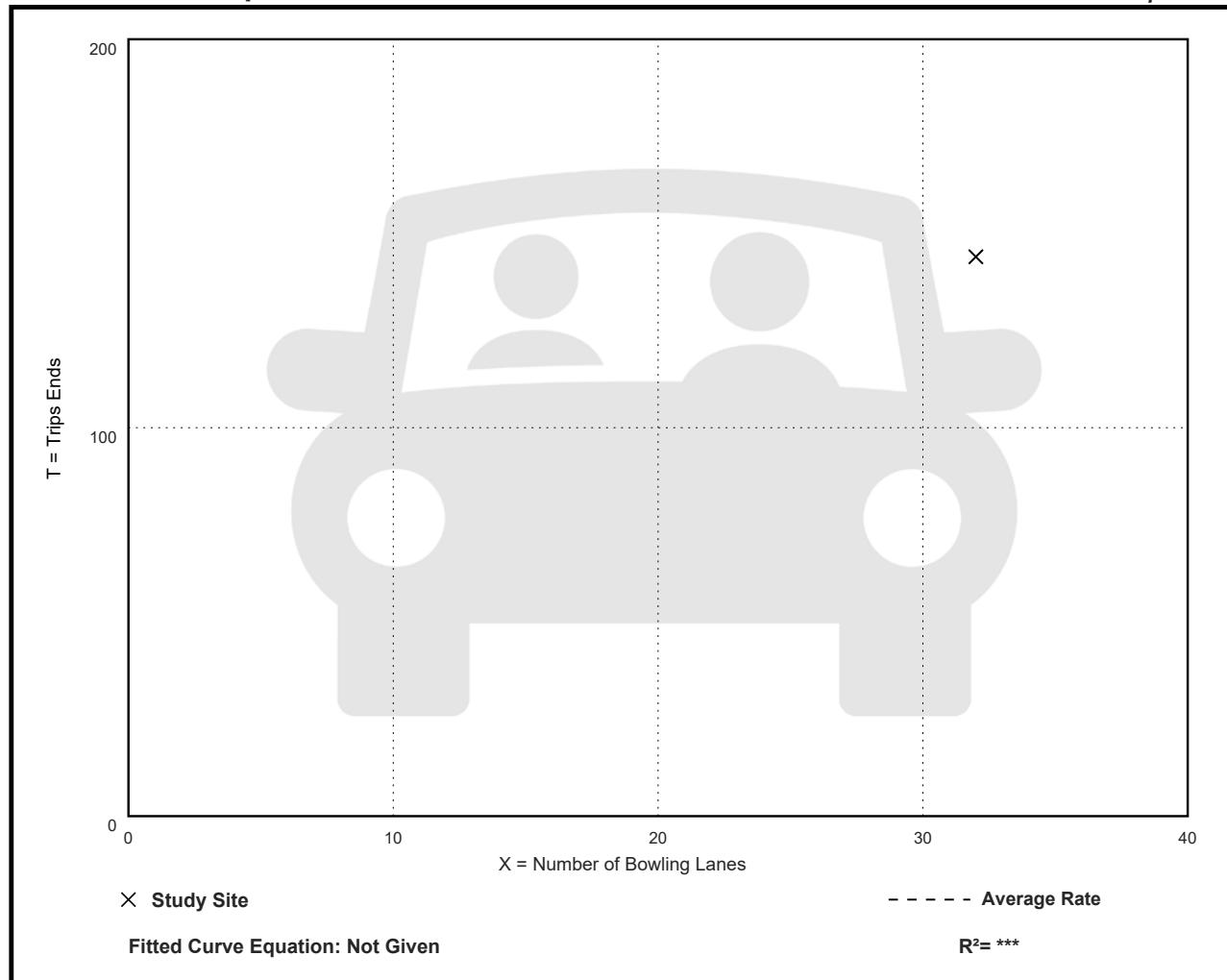
Directional Distribution: 83% entering, 17% exiting

## Vehicle Trip Generation per Bowling Lane

Average Rate	Range of Rates	Standard Deviation
4.50	4.50 - 4.50	***

## Data Plot and Equation

*Caution – Small Sample Size*



# Bowling Alley (437)

Vehicle Trip Ends vs: Bowling Lanes

On a: Saturday, Peak Hour of Generator

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. Num. of Bowling Lanes: 32

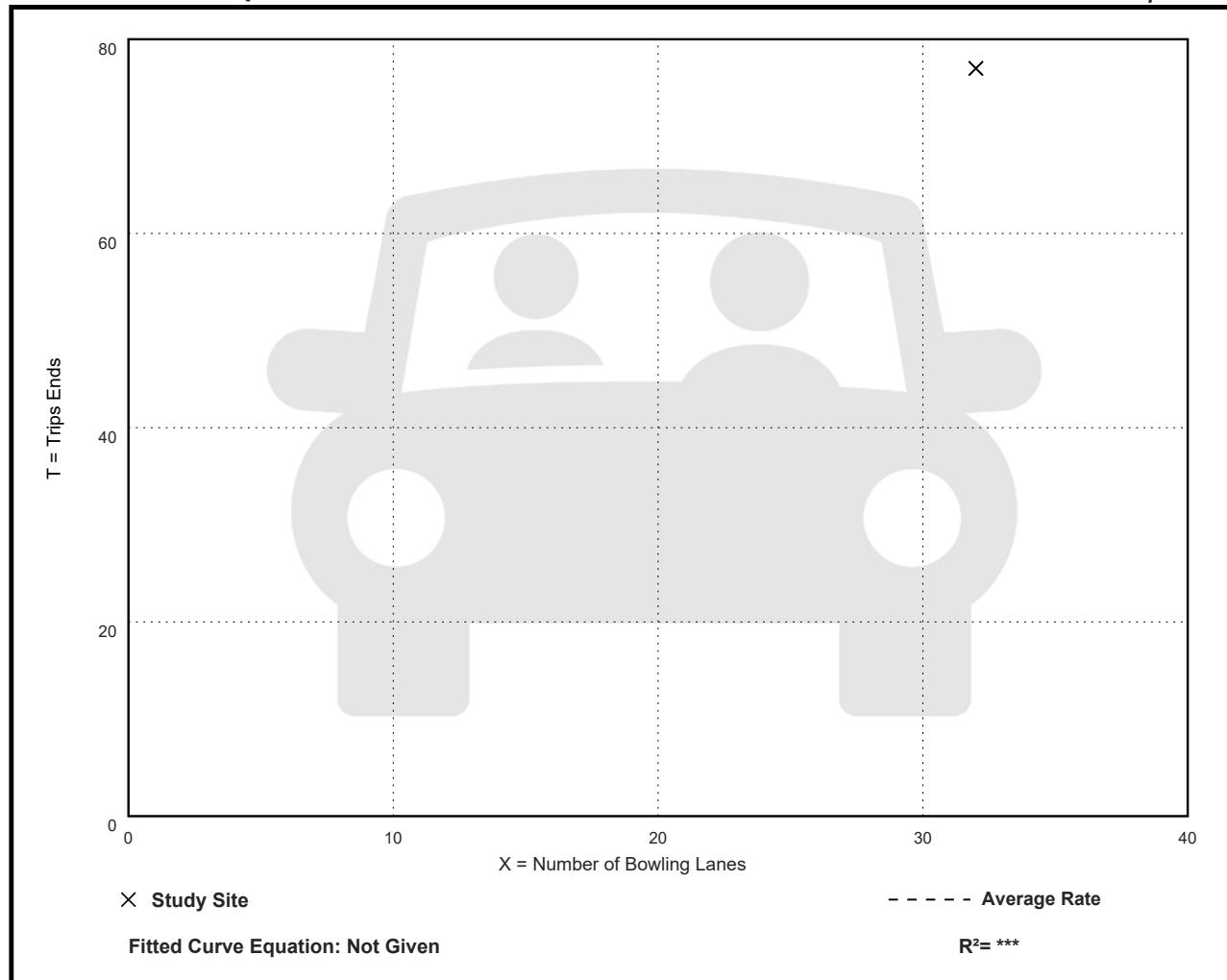
Directional Distribution: 39% entering, 61% exiting

## Vehicle Trip Generation per Bowling Lane

Average Rate	Range of Rates	Standard Deviation
2.41	2.41 - 2.41	***

## Data Plot and Equation

*Caution – Small Sample Size*



# Bowling Alley (437)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 1

Avg. 1000 Sq. Ft. GFA: 73

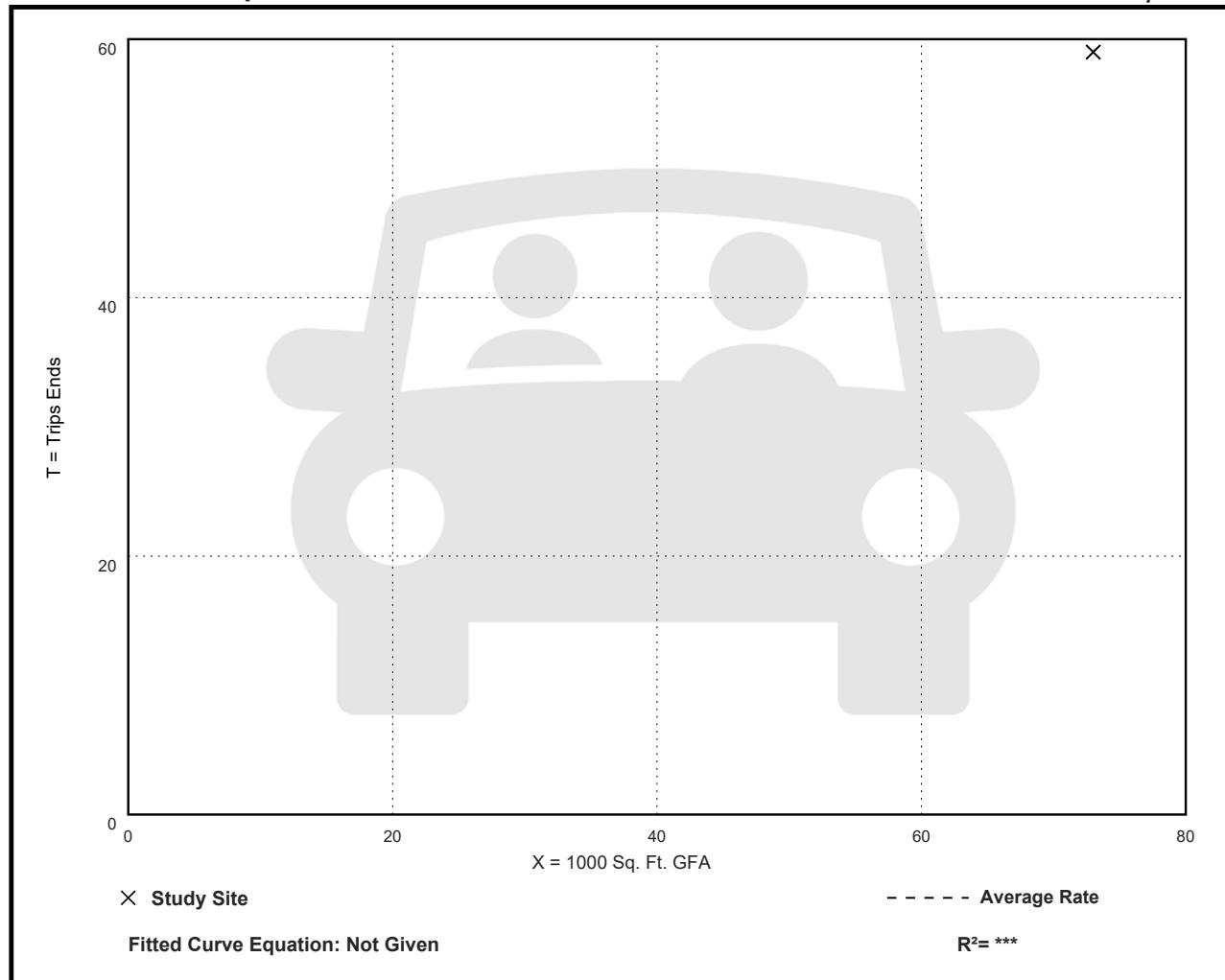
Directional Distribution: 95% entering, 5% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.81	0.81 - 0.81	***

## Data Plot and Equation

*Caution – Small Sample Size*



# Bowling Alley (437)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

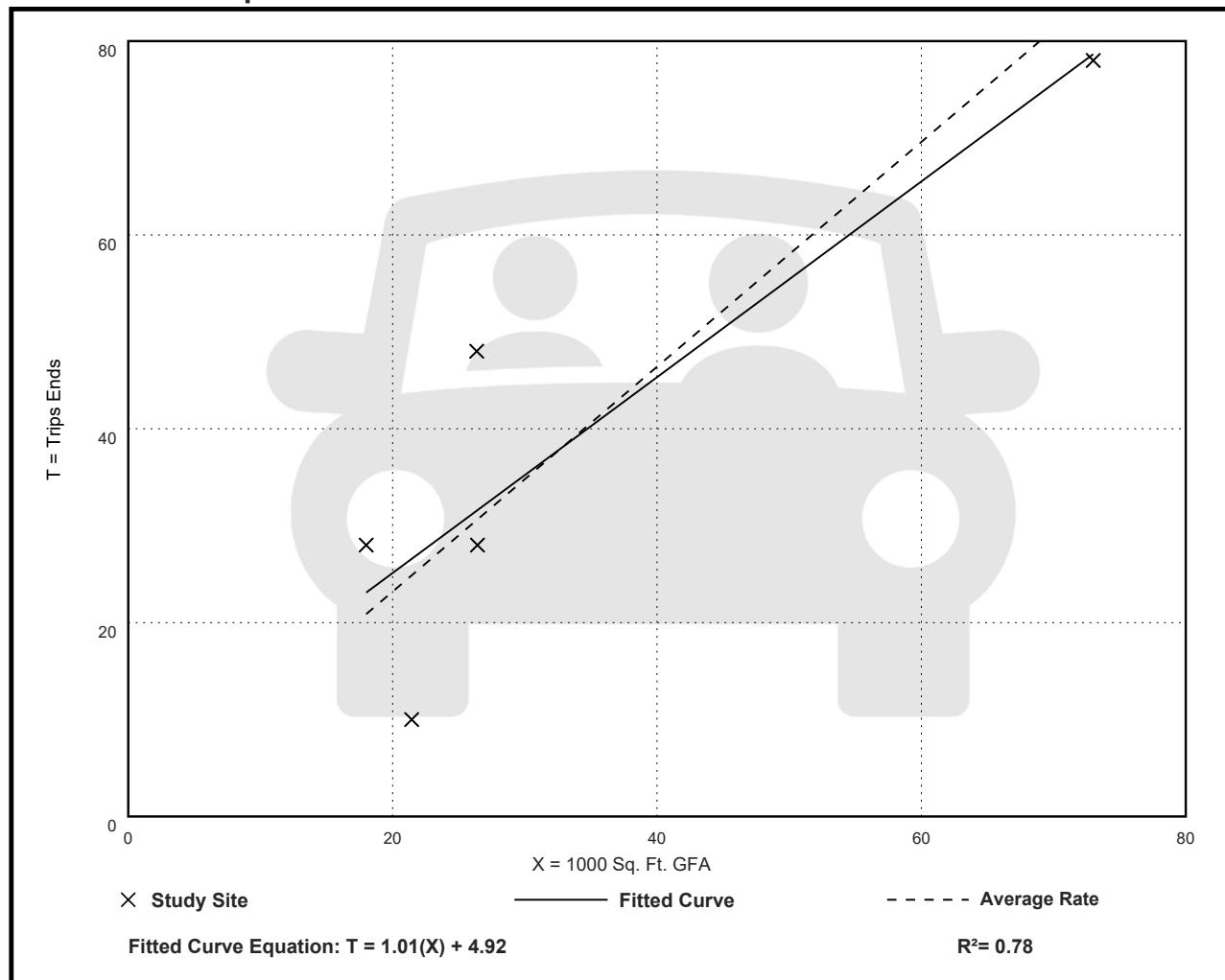
Avg. 1000 Sq. Ft. GFA: 33

Directional Distribution: 65% entering, 35% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
1.16	0.47 - 1.82	0.44

## Data Plot and Equation



# Land Use: 975 Drinking Place

---

## Description

A drinking place contains a bar, where alcoholic beverages and food are sold, and possibly some type of entertainment, such as music, television screens, video games, or pool tables. Establishments that specialize in serving food but also have bars are not included in this land use.

## Additional Data

All data for this land use were collected on Mondays through Thursdays.

The sites were surveyed in the 1980s, the 1990s, and the 2010s in Colorado, Florida, Oregon, Pennsylvania, and South Dakota.

## Source Numbers

291, 358, 583, 1020, 1053

# Drinking Place (975)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 12

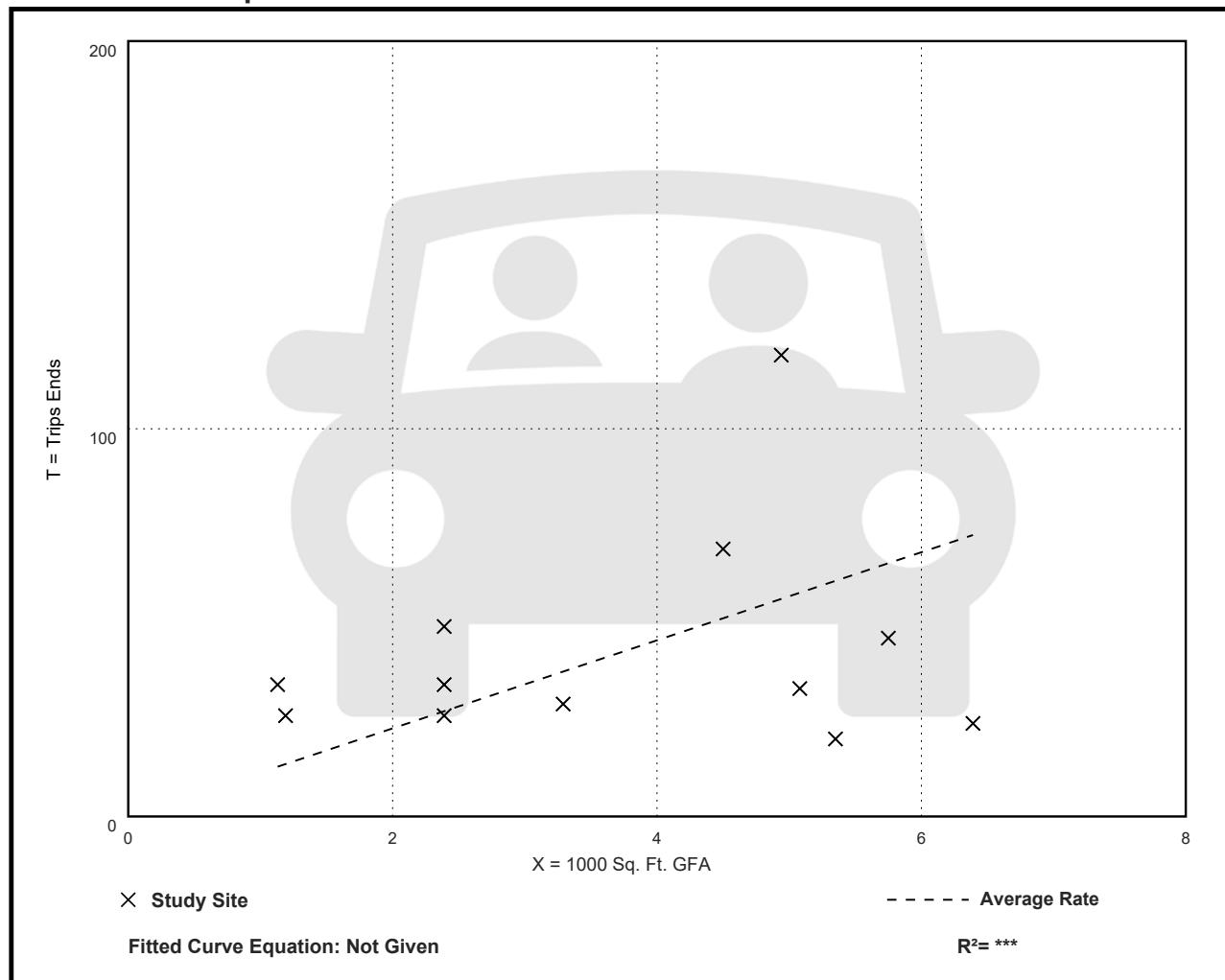
Avg. 1000 Sq. Ft. GFA: 4

Directional Distribution: 66% entering, 34% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
11.36	3.74 - 30.09	7.81

## Data Plot and Equation



# Drinking Place (975)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

PM Peak Hour of Generator

**Setting/Location: General Urban/Suburban**

Number of Studies: 8

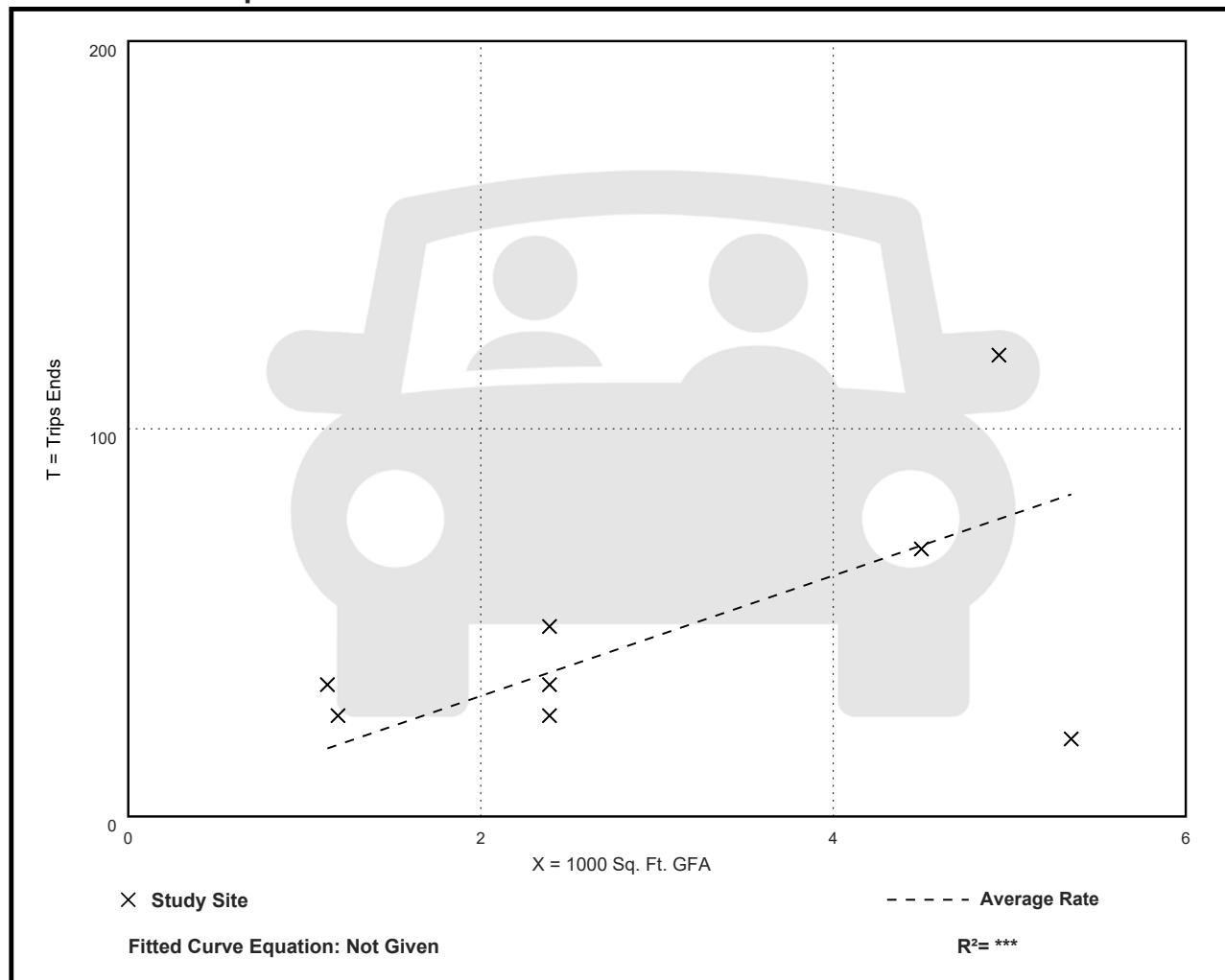
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 68% entering, 32% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
15.53	3.74 - 30.09	8.42

## Data Plot and Equation



# Drinking Place (975)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 5

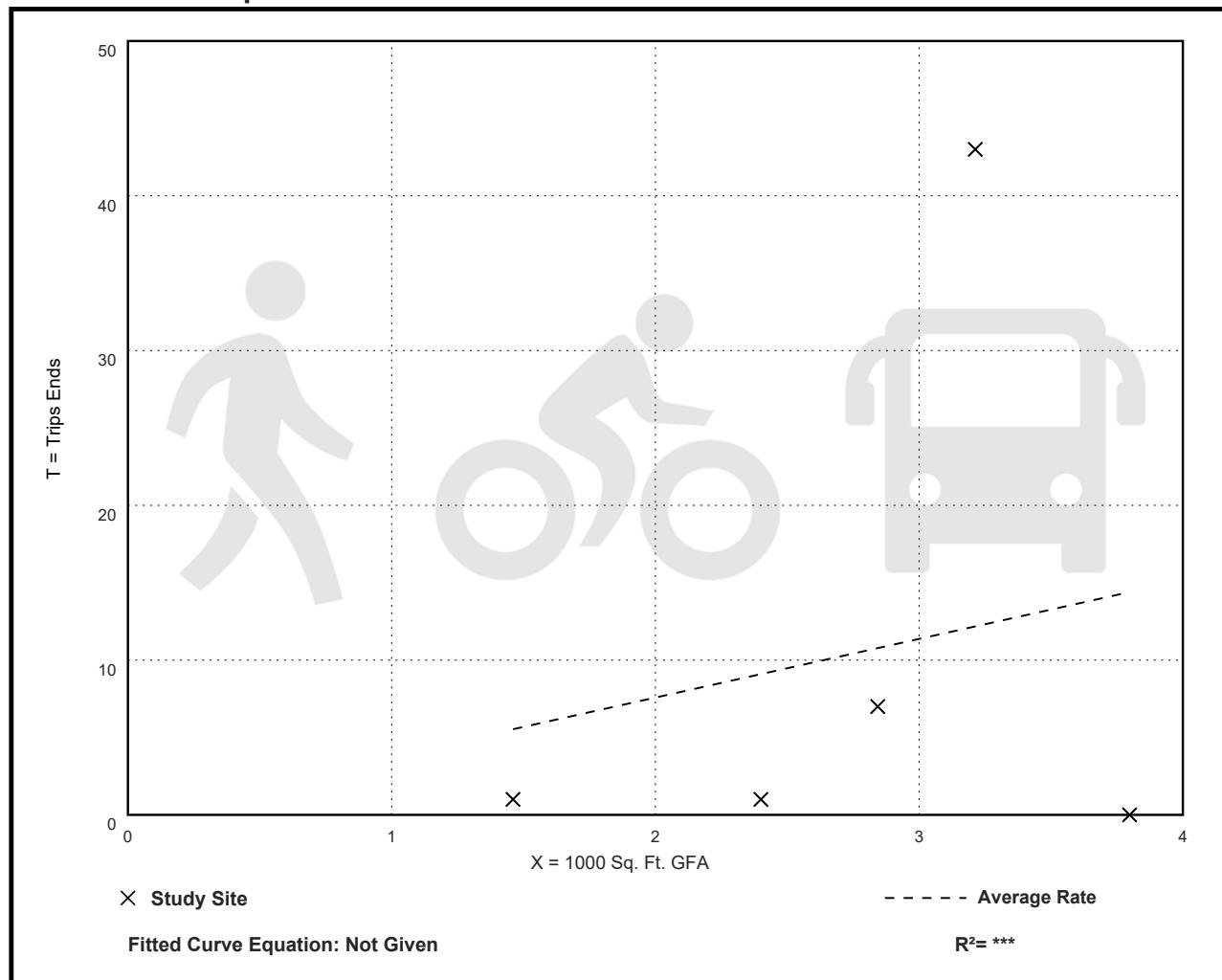
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 65% entering, 35% exiting

## Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
3.79	0.00 - 13.38	6.01

## Data Plot and Equation



# Drinking Place (975)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

PM Peak Hour of Generator

**Setting/Location: General Urban/Suburban**

Number of Studies: 5

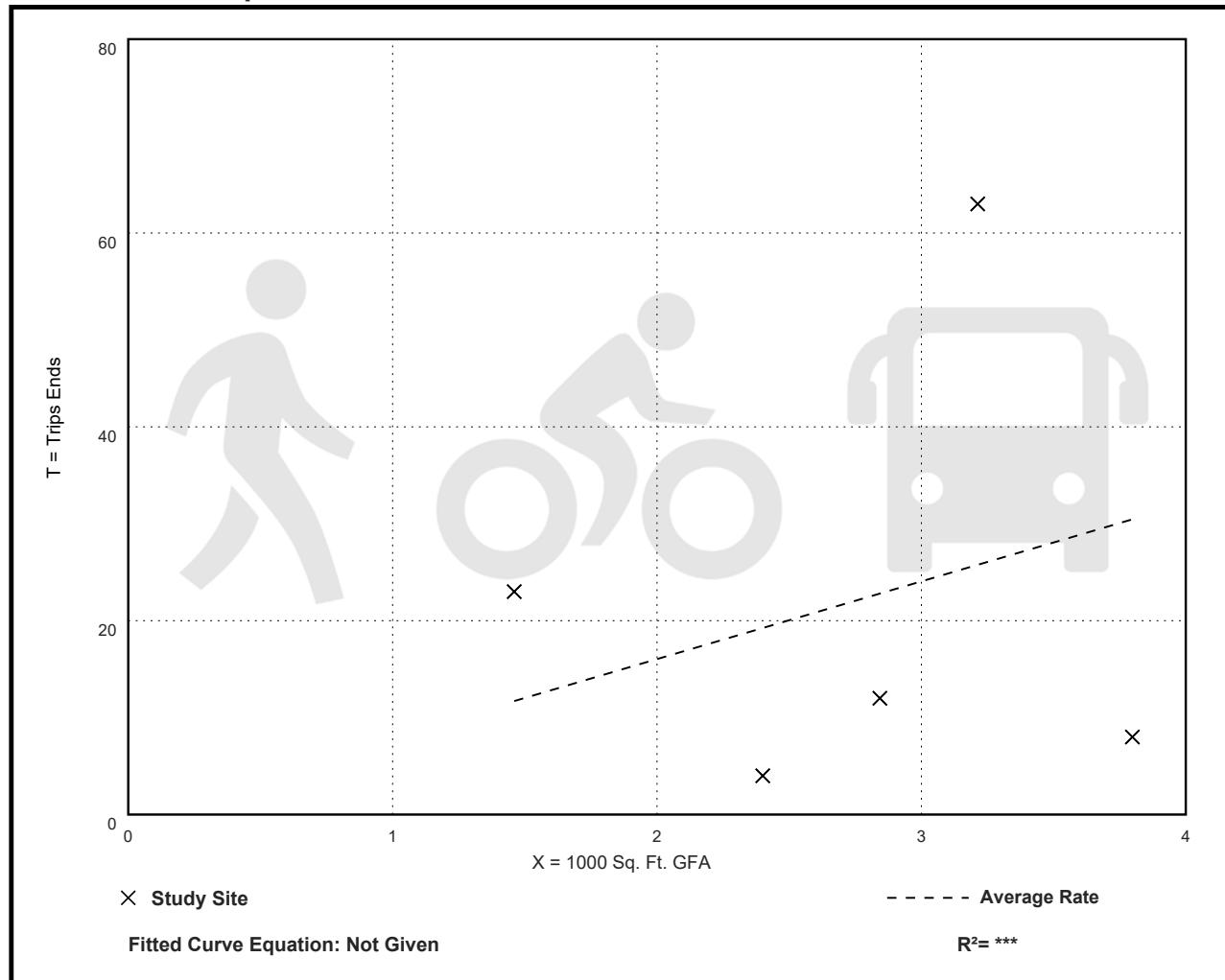
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 47% entering, 53% exiting

## Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
8.02	1.67 - 19.61	8.48

## Data Plot and Equation



# Drinking Place (975)

Walk+Bike+Transit Trip Ends vs: 1000 Sq. Ft. GFA

On a: Saturday, Peak Hour of Generator

**Setting/Location: General Urban/Suburban**

Number of Studies: 5

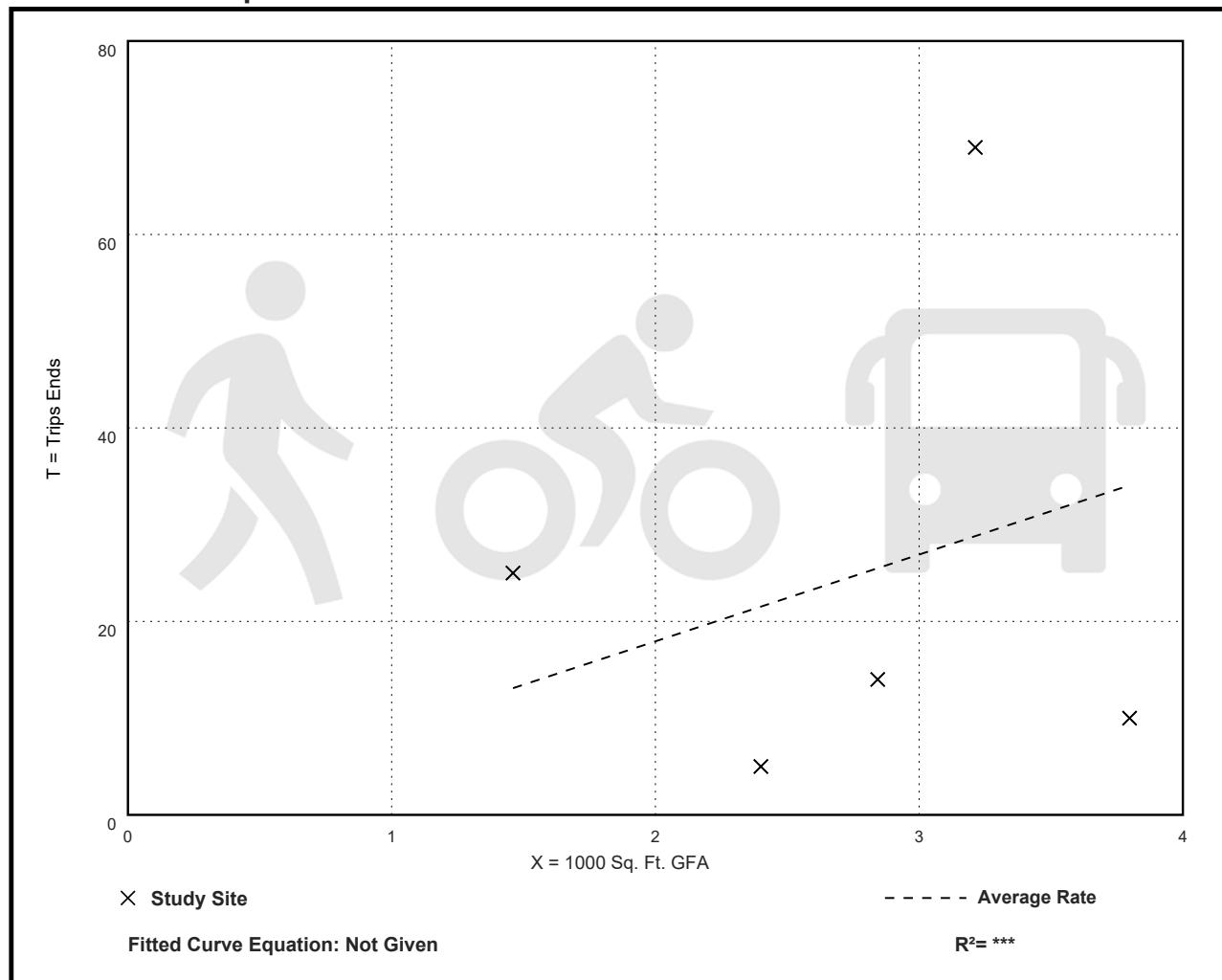
Avg. 1000 Sq. Ft. GFA: 3

Directional Distribution: 54% entering, 46% exiting

## Walk+Bike+Transit Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
8.97	2.08 - 21.48	9.12

## Data Plot and Equation



parking spaces and 854 parking deck spaces). Parking Area C is located north of Sears and Dick's Sporting Goods and includes 1,359 surface parking spaces.

#### Existing Parking Zoning Requirements

The current zoning code requires that the mall provide 4,545 parking spaces. The breakdown of parking requirements based on Chapter 490-701 of the Salem Zoning Code is found in Table 1.

<b>TABLE 1</b> <b>SALEM ZONING CODE –PARKING REQUIREMENTS</b>			
<b>Use</b>	<b>Parking Demand Variable</b>	<b>Parking Requirement</b>	<b>Spaces Required</b>
First Floor Retail GFA	606,600 SF	1 space / 200 SF GFA	<b>3,033</b>
Upper Floor Retail GFA	604,878 SF	1 space / 400 SF GFA	<b>1,512</b>
<b>Total</b>			<b>4,545</b>

### **3.0 EXISTING PARKING DEMAND**

Parking data was collected during the peak holiday shopping season of 2016 and in January 2017 in order to determine existing parking demand under various scenarios. Langan met with the town of Salem and its peer reviewer to develop the methodology of this study. Parking counts were conducted in half-hour intervals on Black Friday, November 25, 2016 from 7:00 a.m. to 4:00 p.m., on Saturday, December 10, 2016 from 9:00 a.m. to 3:00 p.m., and on Saturday, January 14, 2017 from 10:00 a.m. to 4:00 p.m. These were the anticipated peak shopping times for these dates. See Appendix B for detailed parking count spreadsheets.

Parking demand ratio is defined as the actual number of parked vehicles per 1,000 square-feet of gross floor area (GFA). Based on the existing parking observed, a peak parking demand ratio of 3.82 was observed on Black Friday, 3.12 was observed on Saturday, December 10 and 2.04 was observed on Saturday, January 14. Table 2 summarizes the existing peak parking demands for the entire mall.

<b>TABLE 2</b> <b>EXISTING PARKING DEMANDS FOR ENTIRE MALL</b>				
<b>Peak Period</b>	<b>Maximum Number of Occupied Spaces</b>	<b>Parking Provided</b>	<b>Percentage of Occupied Parking Spaces</b>	<b>Observed Peak Parking Demand Ratio</b>
Friday, November 25, 2016 @ 1:30 P.M.	4,623	4,682 <sup>1</sup>	<b>98.87</b>	<b>3.82</b>
Saturday, December 10, 2016 @ 2:30 P.M.	3,781	4,740	<b>79.8%</b>	<b>3.12</b>
Saturday, January 14, 2017 @ 2:30 P.M.	2,472	4,740	<b>52.2%</b>	<b>2.04</b>

<sup>1</sup> Available parking reduced by the 58 spaces blocked off by the Sears Auto Center on Black Friday

**TABLE 1**  
**ANTICIPATED AVERAGE PARKING DEMAND - SALEM NH LYONS**

USE	LAND USE CODE <sup>1</sup>	WEEKDAY (MON - THURS)		FRIDAY		SATURDAY	
		Average Parking Rate <sup>2</sup>	Anticipated Average Parking Demand	Average Parking Rate <sup>2</sup>	Anticipated Parking Demand	Average Parking Rate <sup>2</sup>	Anticipated Parking Demand
Loretta's Last Call (5,343 SF)	932	8.97 spaces/KSF	48	14.27 spaces/KSF	76	11.53 spaces/KSF	62
Game On! (143 Seats)	932	0.28 spaces/seat	40	0.47 spaces/seat	67	0.38 spaces/seat	54
<b>Total Average Parking Demand</b>		<b>0.28 spaces/seat</b>	<b>88</b>	<b>0.47 spaces/seat</b>	<b>143</b>	<b>0.38 spaces/seat</b>	<b>116</b>

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

**TABLE 2**  
**ANTICIPATED PEAK (85TH PERCENTILE) PARKING DEMAND - SALEM NH LYONS**

USE	LAND USE CODE <sup>1</sup>	WEEKDAY (MON - THURS)		FRIDAY		SATURDAY	
		Peak Parking Rate <sup>2</sup>	Anticipated Peak Parking Demand	Peak Parking Rate <sup>2</sup>	Anticipated Peak Parking Demand	Peak Parking Rate <sup>2</sup>	Anticipated Peak Parking Demand
Loretta's Last Call (5,343 SF)	932	13.44 spaces/KSF	72	20.24 spaces/KSF	108	16.37 spaces/KSF	87
Game On! (143 Seats)	932	0.48 spaces/seat	69	0.79 spaces/seat	113	0.56 spaces/seat	80
<b>Total Average Parking Demand</b>		<b>0.28 spaces/seat</b>	<b>141</b>	<b>0.47 spaces/seat</b>	<b>221</b>	<b>0.38 spaces/seat</b>	<b>167</b>

<sup>1</sup> Land Use Codes based on ITE Parking Generation Manual 6th Edition

<sup>2</sup> Parking rates based on ITE Parking Generation Manual 6th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

**TABLE 3**  
**ANTICIPATED TRIP GENERATION - SALEM NH LYONS**

USE	LAND USE CODE <sup>1</sup>	PM PEAK HOUR			WEEKDAY DAILY TOTAL	SAT PEAK HOUR			SATURDAY DAILY TOTAL
		ENTER	EXIT	TOTAL		ENTER	EXIT	TOTAL	
Loretta's Last Call (5,343 SF)	932	28	23	51	572	30	30	60	654
Game On! (143 Seats)	932	32	24	56	625	40	36	76	801
<b>Total New Trips</b>		<b>60</b>	<b>47</b>	<b>107</b>	<b>1197</b>	<b>70</b>	<b>66</b>	<b>136</b>	<b>1455</b>
Previously Approved Restaurant/Retail (19,600 SF)	-	94	64	158	2022	121	106	227	2508
<b>Net Change in Trips</b>		<b>-34</b>	<b>-17</b>	<b>-51</b>	<b>-825</b>	<b>-51</b>	<b>-40</b>	<b>-91</b>	<b>-1053</b>

<sup>1</sup> Land Use Codes based on ITE Trip Generation Manual 11th Edition

<sup>2</sup> Volume based on ITE Trip Generation Manual 11th Edition: Land Use Code 932: High-Turnover (Sit-Down) Restaurant

<sup>3</sup> Volume based on report titled "Traffic Impact Study - Redevelopment/Expansion of the Mall at Rockingham Park" by Langan dated June 2017.

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**100 Cambridge Street, Suite 1310 Boston, MA 02114 T: 617.824.9100 F: 617.824.9101**

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**To:** Jacob LaFontaine – Salem Planning Director

**From:** Richard Schlichting

**Date:** December 29, 2025

**Re:** Conditional Use Permit Request - Signage  
Lyons Group Restaurant at The Mall at Rockingham Park  
Salem, New Hampshire  
Langan Project No.: 151064501

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In support of Lyons Group Restaurant request for a conditional use permit, Langan Engineering & Environmental Services, Inc. in conjunction with Salem NIM, LLC has prepared the following discussion of the conditional use permit criteria.

Lyons Group Restaurant seeks a Conditional Use Permit pursuant to Article VII, Section 490-702(D)(2), the business is allowed one wall sign not exceeding 16 square feet in size. The requested Conditional Use Permit requests eighteen signs surrounding the building.

On the West side of the building is the Game On main entrance. There are the following five signs.

Sign 1 is a 32 ft wide, 20 ft tall sign. It is a painted wall graphic and colored Red: #BA212A and Blue #183E6F. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 2 is the wording "DINE" DRINK PLAY" and WIN", all 1 ft 8 in. tall and 2 ft, 4 ft 3 in. and 2 ft wide, respectfully. It is an inner lit acrylic colored Red: #BA212A. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 3 is a 3 ft wide, 13 ft 2 in. tall, 1 ft wide sign. It is a lighted blade, inner lit acrylic and colored Red: #BA212A, Blue #183E6F, and White. It sits perpendicular to the building and has Game On's name and logo. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 12 is a 12 ft wide, 8 in. tall sign. It is a inner lit acrylic and colored Red: #BA212A. It sits on windows of the building and reads "Home of the Goat Fan". There are two of these signs on this side of the building and it is the only two on the building itself.

# MEMO

Conditional Use Permit Request - Signage  
Lyons Group Restaurant at the Mall at Rockingham Park  
December 29, 2025- Page 2 of 4

On the South side of the building is the Loretta's main entrance. There are the following four signs.

Sign 6 is a 14 ft 8 in. wide, 5 ft 6 in. tall sign. It is an LED inner lit sign and colored Pink: #D0679 and Maroon #5F100A. The sign is "Loretta's Last Call" logo. There is one of these signs on this side of the building and it is one of two on the building.

Sign 7 is a 14 ft 8 in. wide, 5 ft 6 in. tall sign. It is a back lit mounted decor and colored Brown: #593419. This sign is a Loretta's Star. There is one of these signs on this side of the building and it is one of three on the building.

Sign 8 is a 12 ft 11 in. wide, 22 ft 3 in. tall sign. It is a painted guitar graphic with LED strip lights on guitar strings and colored Red: #EE1816, Yellow: #E3DA8D, Green: #84B99A, Purple: #C84BC9, and Orange #D5673D. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 9 is a 3 ft 7 in. wide, 6 ft tall sign. It is an LED lit marquee sign colored Red: #FE030F. There is one of these signs on this side of the building and it is the only one on the building itself.

On the East side of the building is part of the Loretta's seating area. There are the following four signs.

The first sign is second Sign 6.

The second and third sign is Sign 7.

The final sign is Sign 5. Sign 5 is a 15 ft 11.5 in. wide, 7 ft 7 in. tall sign. It is an inner lit acrylic sign and colored Red: #BA212A and Blue #183E6F. The sign says Game On and has the logo. There is one of these signs on this side of the building and it is the only one on the building itself.

On the North side of the building is the back of the building. There are the following five signs.

Sign 4 is a 9 ft wide, 6 ft 5 in. tall sign. It is an inner lit sign and colored Pink: #D0679 and Maroon #5F100A. The sign is "Loretta's Last Call, Live Music" logo. There is one of these signs on this side of the building and it is the only one on the building itself.

Sign 10 is a 4 ft wide, 11 ft tall sign. It is a wall mounted fabric blade sign, perpendicular to the building. They will be a variety of colors. There are four of these signs on this side of the building and they are the only ones on the building.

# MEMO

Conditional Use Permit Request - Signage  
Lyons Group Restaurant at the Mall at Rockingham Park  
December 29, 2025- Page 3 of 4

(a) The modification complies with the purposes of the sign regulations noted in Subsection A.

**Response: The Town of Salem zoning regulations Subsection A of 490-702 "Signs", states the purpose of the section is "to regulate the erection of signs for the purpose of providing information and advertising in an orderly, effective and safe manner. Restrictions on type, location and size of signs protect the public from hazardous and distracting displays and create an attractive environment which is conducive to business, industry, and tourism".**

**The conditional use permit request pertains to the installation of additional building-mounted signage on all four sides of the structure. These signs are intended to improve business identification for customers approaching from multiple directions. Because the building is situated within a plaza and bordered by a ring road, it is visible from all angles. Therefore, each sign is strategically positioned to address a specific viewing corridor and ensure that drivers can identify the business safely and in advance.**

(b) The applicant's particular situation justifies a modification to the requirements.

**Response: Given the layout of the building and entrance to the mall and plaza, this modification is justifiable. It is imperative for both the success of the business as well as the pedestrian and vehicular safety that customers can easily identify their location and take direct routes to their destination.**

(c) The site is suitable for the proposed modification.

**Response: Given the layout of the plaza, this request to include additional wall signs is a suitable modification. The signage is proposed to face the proposed ring road and the adjacent surface parking. This will help vehicular and pedestrian traffic throughout the area.**

(d) There will be no adverse impact on neighboring properties.

**Response: This additional sign is proposed on a building façade that faces an internal parking area and other similar commercial uses. Because the sign is located within the site and oriented internally and towards a ring road, it will not create any adverse impacts on neighboring properties.**

(e) There will be no adverse impact on traffic or pedestrian safety.

**Response: The proposed signage informs motorists and pedestrians of the main business of the building. The sign will not be hazardous to traffic or pedestrian safety.**

(f) The aesthetic character of the site and the surrounding area will not be adversely affected.

**Response: The proposed sign will be consistent with the typical signage associated with this type of use and will align with the existing architectural style and character of the surrounding area.**

# MEMO

Conditional Use Permit Request - Signage  
Lyons Group Restaurant at the Mall at Rockingham Park  
December 29, 2025- Page 4 of 4

(g) The modification will be consistent with the spirit and intent of this chapter and the Master Plan.

**Response: The proposed sign serves the Master Plan's goal of minimizing traffic impact on Salem streets and neighborhoods by clearly directing motorists directly to the proposed field.**

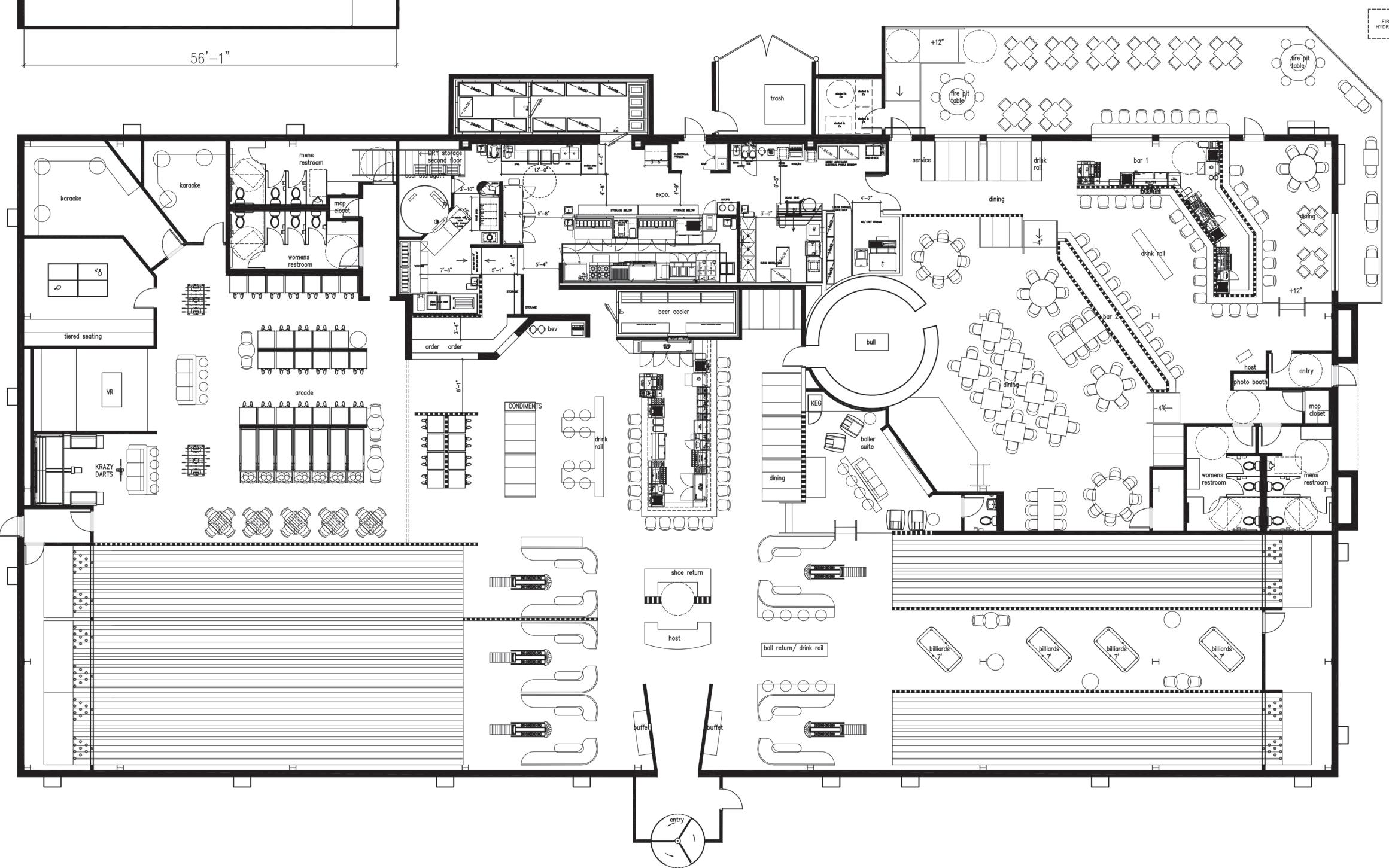
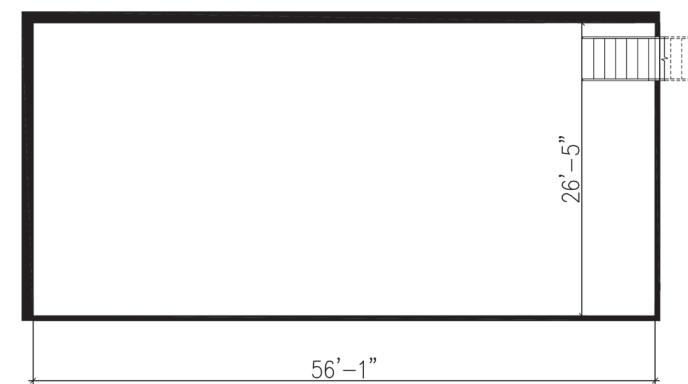
It is the opinion of the applicant and the project team that application meets all seven criteria necessary to be considered for a conditional use permit. We look forward to discussing this application with you at the next Planning Board hearing.

\langan.com\data\bos\data5\151064501\project\_data\discipline\site\_civil\permit\_apps\site\_plan\_submission\parts\lyons\_group conditional use permit request - signage.docx

**GAME ON! / LORETTA'S LAST CALL  
EXTERIOR SIGNAGE PACKAGE**

**12.22.2025**

# FLOOR PLAN



## GAME ON! COUNTS

DINING BAR SEATS: 36  
 DINING ARCADE SEATS: 26  
 BAR SEATS: 31  
 PRIVATE SUITE: 10 +/-

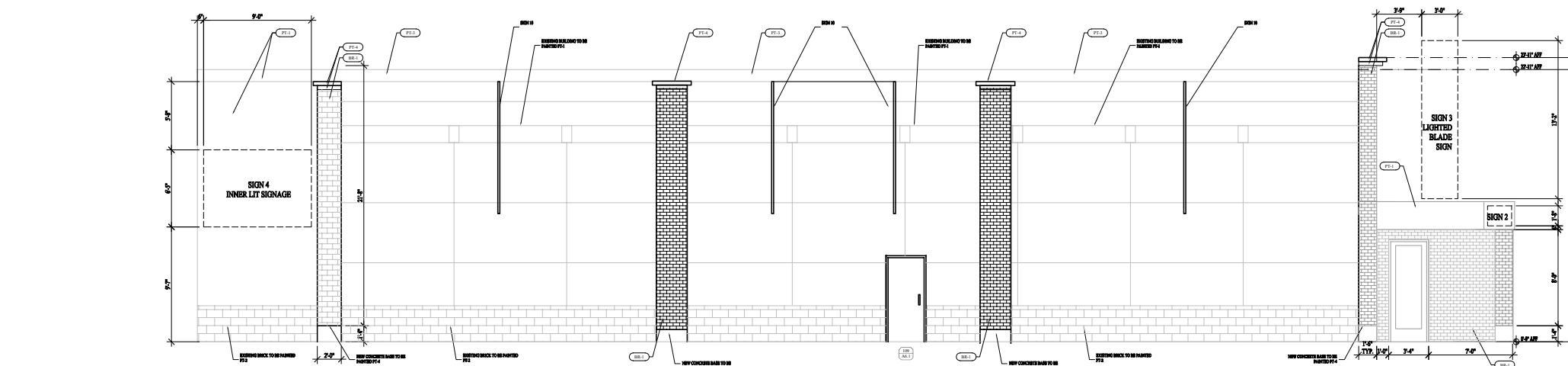
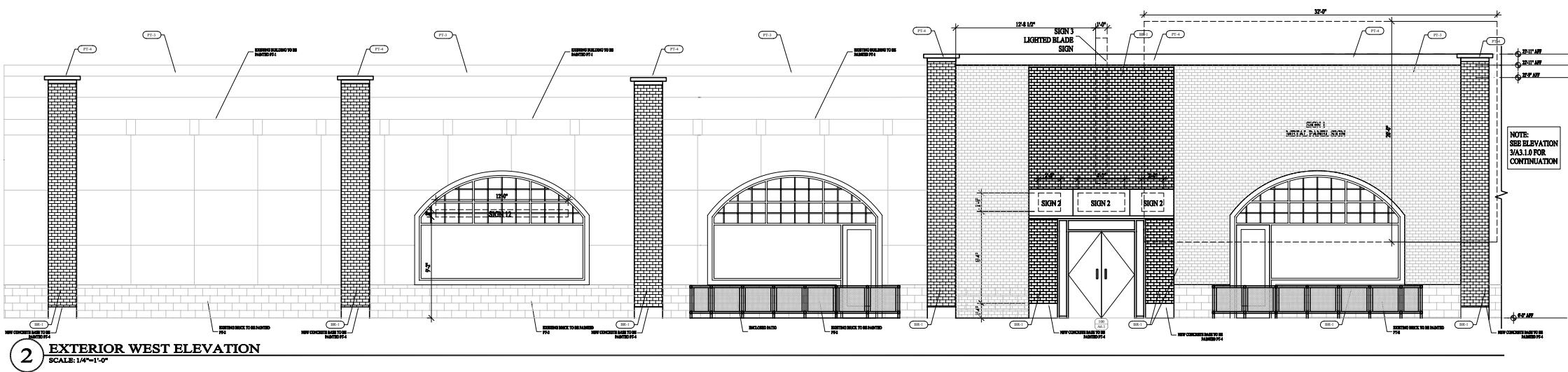
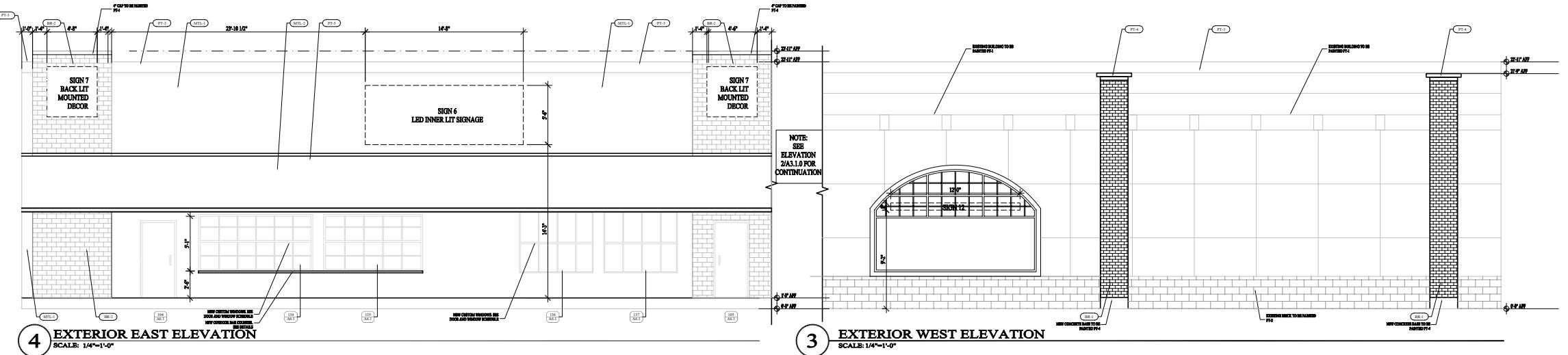
BOWLING LANES: 10 LANES  
 INTERIOR SF: 12,556 +/-  
 KITCHEN SF: 1,895 +/-  
 OUTSIDE COOLER STORAGE: 332 +/-  
**UPSTAIRS STORAGE SF: 1464 +/-**  
**OVERALL SF: 16,247 +/-**

## LORETTA'S LAST CALL COUNTS

DINING SEATS: 104  
 BAR 1 SEATS: 21  
 BAR 2 SEATS: 10  
 PATIO SEATS: 52  
 PATIO BAR: 7

INTERIOR SF: 4045 +/-  
 DANCE FLOOR SF: 1184 TOTAL  
 LINE DANCING SF: 818  
**EXTERIOR PATIO SF: 1298 +/-**  
**OVERALL SF: 5343 +/-**

# EXTERIOR ELEVATIONS



<p><b>DESIGN FIRM:</b> Gravity Architecture &amp; Design, LLC 733 W Smith Street Second Floor Orlando, Florida 32804</p> <p><b>ARCHITECT:</b> Raymond R. Schaefer License #: 001.015384 Phone #: 407.832.0949</p>

<b>GAME ON</b>	<b>LORETTA'S LAST</b>
77 ROCKINGHAM PARK BLVD	SALEM, HN 03079

RECORD	RECORD
No.	Description
0	DESIGN INTENT DRAWINGS
	Date
	08/11/2025
DATE	
SCALE	AS INDICATED

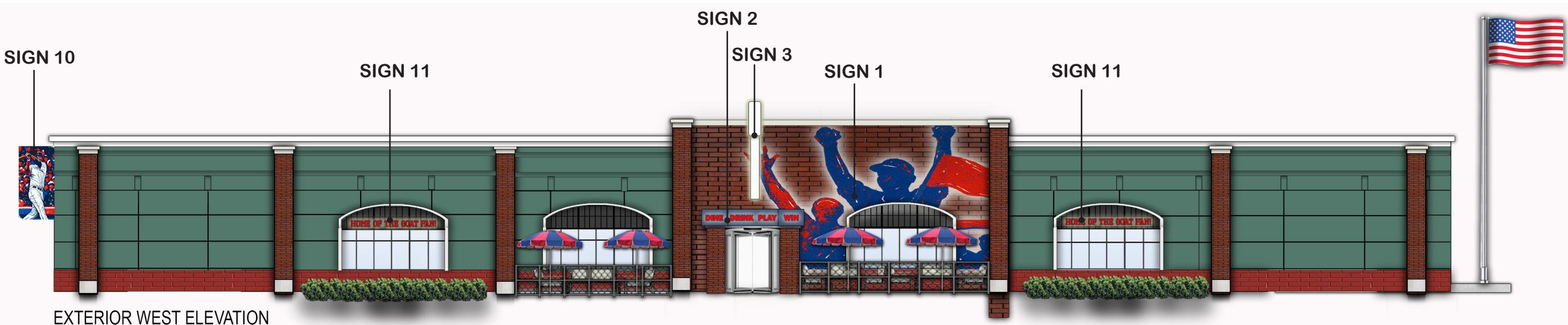
Exterior  
Elevations

A3.1.0

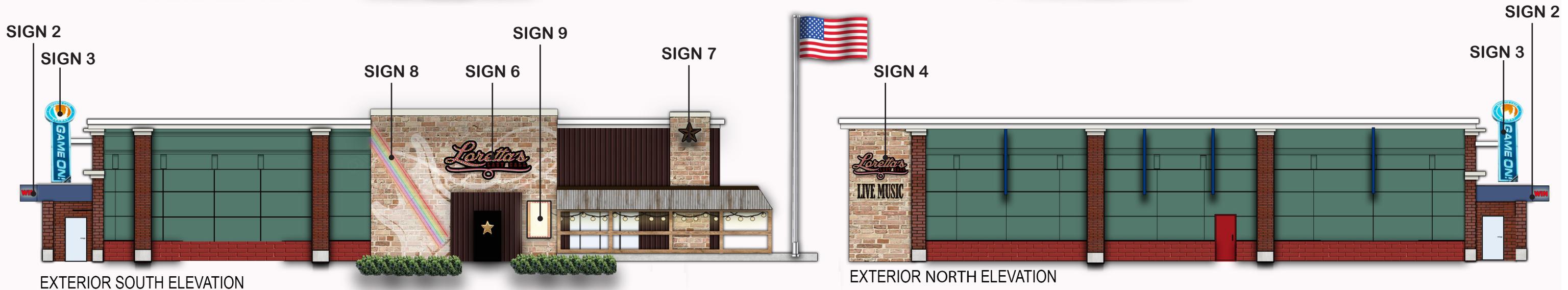
Drawing No.



# COLORED EXTERIOR ELEVATIONS

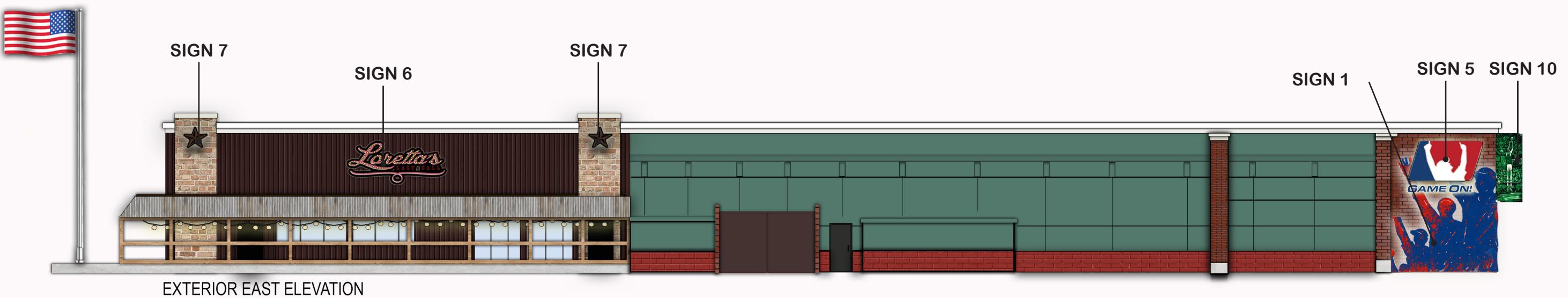


EXTERIOR WEST ELEVATION



EXTERIOR SOUTH ELEVATION

EXTERIOR NORTH ELEVATION



EXTERIOR EAST ELEVATION

# SIGNAGE INFORMATION

## SIGN1:



RED: #BA212A R: 186 G:33 B:42

BLUE: #183E6F R: 24 G:62 B:111

MATERIAL: PAINTED WALL GRAPHIC

APPROXIMATE ROUGH DIMENSION:  
15'-11 1/2"W X 17'-0"H

## SIGN 2:



RED: #BA212A R: 186 G:33 B:42

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
2'-0" W X 1'-8" H

## SIGN 3:



RED: #BA212A R: 186 G:33 B:42

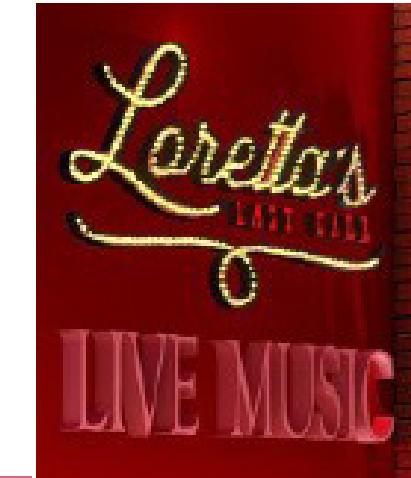
BLUE: #183E6F R: 24 G:62 B:111

WHITE: R: 255 G:255 B:255

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
1'-0" W X 13'-0" H

## SIGN 4:



PINK: #D06779 R: 205 G:106 B:124

MAROON: #5F100A R: 95 G:16 B:10

MATERIAL: REVERSE CHANNEL WITH LED BULBS AND INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
9'-0"W X 6'-5"H

# SIGNAGE INFORMATION

## SIGN 5:



RED: #BA212A R: 186 G:33 B:42

BLUE: #183E6F R: 24 G:62 B:111

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:

14'-1" W X 7'-7" H

## SIGN 6



PINK: #D06779 R: 205 G:106 B:124

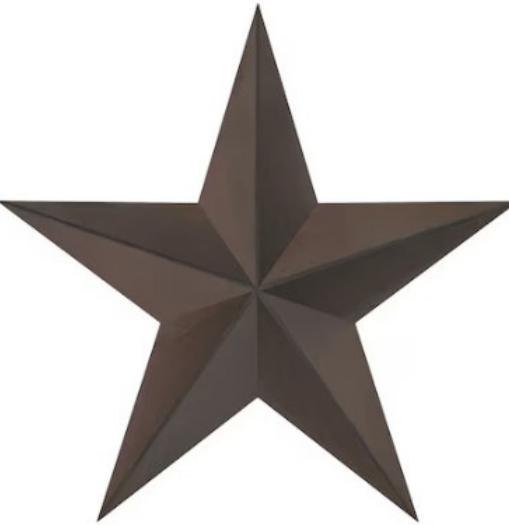
MAROON: #5F100A R: 95 G:16 B:10

MATERIAL: REVERSE CHANNEL WITH LED BUBS

APPROXIMATE ROUGH DIMENSION:

14'-8" W X 5'-0" H

## SIGN 7:



BROWN: #593419  
R: 89 G:52 B:26

MATERIAL: BACKLIT

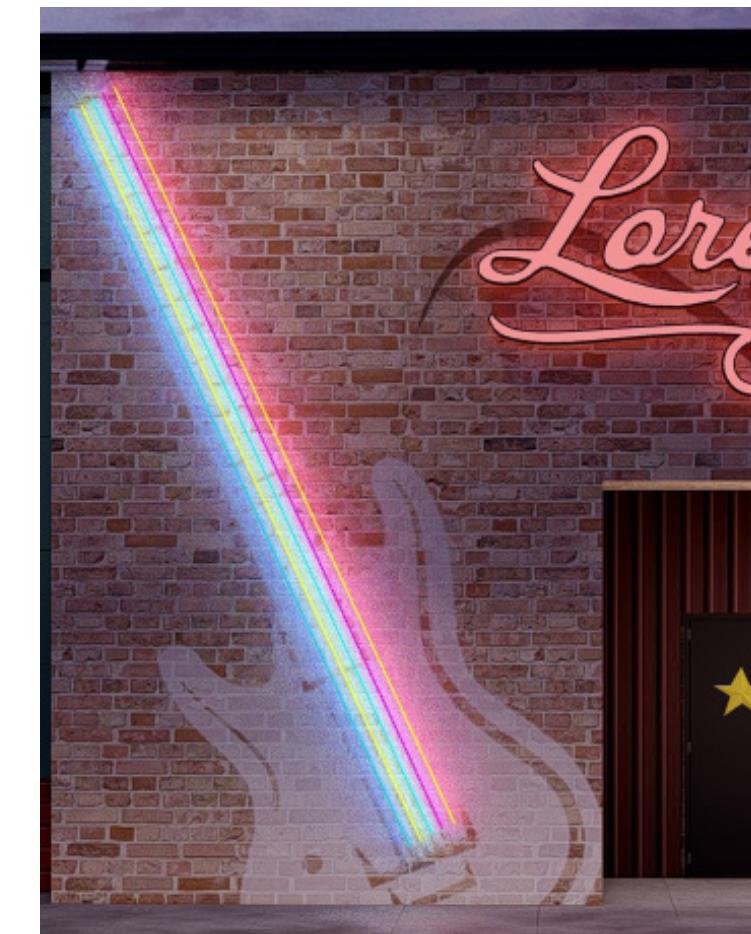
ALUMINUM STAR

APPROXIMATE

ROUGH DIMENSION:

4'-8" W X 4'-8" H

## SIGN 8:



RED: #EE1816 R: 238 G:24 B:22

YELLOW: #E3DA8D R: 227 G:218 B:141

GREEN: #84B99A R: 132 G:185 B:154

PURPLE: #C84BC9 R: 200 G:75 B:201

ORANGE: #D5673D R: 213 G:103 B:61

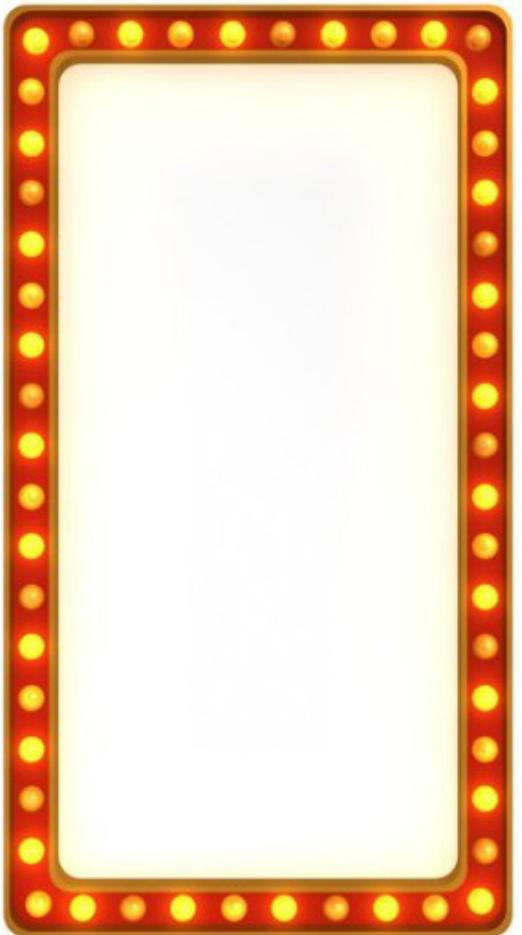
MATERIAL: CMYK LED STRIPS. COLORS TO  
MATCH DESIGN INTENT IMAGE.

APPROXIMATE ROUGH DIMENSION:

12'-11" W X 22'-3" H

# SIGNAGE INFORMATION

## SIGN 9:

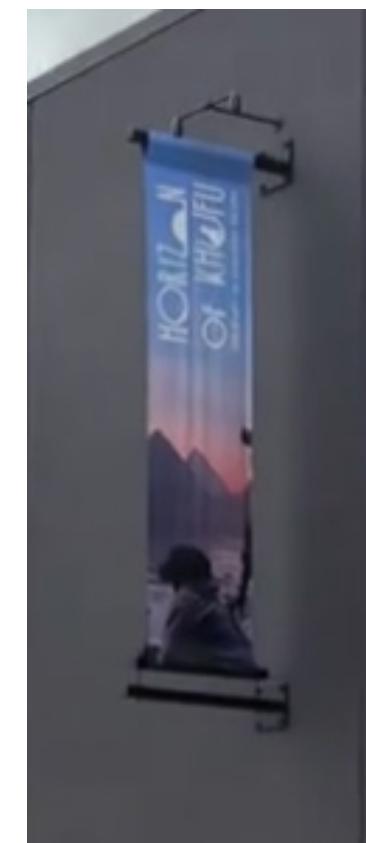


RED: #FE030F R: 254 G:3 B:15

MATERIAL: LED LIGHT BULBS MOUNTED ON ALUMINUM SIGN

APPROXIMATE ROUGH DIMENSION:  
3'-7" W X 6'-0" H

## SIGN 10:



MATERIAL: WALL MOUNTED BLADE SIGN FABRIC

APPROXIMATE ROUGH DIMENSION:  
4'-0" W X 11'-0" H

# SIGNAGE INFORMATION

## SIGN 11:



RED: #BA212A R: 186 G:33 B:42

MATERIAL: INNER LIT ACRYLIC

APPROXIMATE ROUGH DIMENSION:  
12'-0" W X 8" H



Gravity Architecture & Design, LLC

ENVISION \* ESTABLISH \* ENGAGE

Material/Color Call-outs Salem, NH:

All Colors / Materials to match Fenway Park base colors as well as existing Loretta's brand colors where applicable.

MTL-1	Loretta's Accent Corrugated Metal Panel. Paint Dark Brown and seal/match brand color
MTL-2	Aged Corrugated Metal Roof. MTL-5 base with art directed aging in field and seal
MTL-5	Raw Corrugated Metal sheeting. Clear Coat for metal show-thru and seal
BR-1	Face Brick/Thin Brick. Match Fenway Park Brick Medium Red/Brown
BR-2	Face Brick/Thin Brick. Match Loretta's Fenway Interior Brick/light brown
PT-1	Green Paint – Match Fenway Park standard. Main body of building is existing EIFS
PT-2	Paint CMU @ trash enclosure to match BR-1
PT-3	White Paint on Metal Cap
PT-4	White/Gray paint to match concrete color on metal cornice cap. Verify in Field
PT-5	Dark Brown metal trim. Match WD-1
PT-6	Green paint to match PT-1 on metal fencing and slats at trash enclosure
WD-1	Wood Stain/Rustic Oak. Match Loretta's Fenway interior trim

Northam Survey, LLC  
750 Central Ave Suite N  
Dover, NH 03820  
(603) 953-3164  
eric@northamsurvey.com



**Re: Request for Waiver – Subdivision Regulations §278-4:1.15**

**Applicant:** Jacqui L. Fuschetti

**Property:** 59 Millville Street, Salem, NH

**Tax Map:** 81 | **Lot:** 2946

**Project:** Two-Lot Subdivision (NS Project #1667)

Town of Salem Planning Board  
33 Geremonty Drive  
Salem, NH 03079

Dear Members of the Planning Board:

On behalf of the property owner, Jacqui L. Fuschetti, we respectfully request a waiver from the requirements of Section 278-4:1.15 of the Salem Subdivision Regulations, which requires preparation of a plan for minimizing soil erosion and sedimentation during construction and operation of the proposed development, unless deemed unnecessary by the Board.

The subject property is proposed to be subdivided into two residential lots, constituting a minor subdivision as defined by the Subdivision Regulations. The lots will be served by existing municipal water and sewer, and no new public or private streets, stormwater systems, or other municipal improvements are proposed. The subdivision results in minimal land disturbance beyond that associated with typical single-family residential development.

Further, a variance from the Zoning Ordinance was approved by the Salem Zoning Board of Adjustment on October 7, 2025 (Petition #08-2025-00032), with specific stipulations addressing stormwater and site impacts, including roof runoff infiltration, depiction of existing and proposed utilities, and driveway placement. These conditions ensure that drainage and site impacts are appropriately managed as part of future development.

At this stage, no construction activity is proposed as part of the subdivision approval itself. Detailed erosion control measures are more appropriately addressed during the construction and building permit phase, when final grading, driveway layout, and building footprints are known. Deferring the erosion and sedimentation plan to that phase will allow for a more accurate and effective plan consistent with actual site development.

Northam Survey, LLC  
750 Central Ave Suite N  
Dover, NH 03820  
(603) 953-3164  
eric@northamsurvey.com

Granting this waiver would be consistent with the intent of the Subdivision Regulations, would not be contrary to public health or safety, and would not result in undue impacts to abutting properties or municipal infrastructure. The requested waiver is reasonable given the limited scope of the subdivision and the safeguards already in place.

Accordingly, we respectfully request that the Planning Board grant a waiver from Section 278-4:1.15, allowing erosion and sedimentation controls to be reviewed and approved during the construction phase rather than at the time of subdivision approval.

Thank you for your consideration of this request. Please do not hesitate to contact us should you require additional information.

Sincerely,



Eric Salovitch, LLS

*President*

Lot File: P:\1667 Jay Parent\Survey\DWGS\1667 SURVEY.lot

CRD File: P:\1667 Jay Parent\Survey\CRD Files\1667 SURVEY.crd

Lot: 2946 , Block: 1, Type: LOT

PNT# Bearing Distance Northing Easting Station

9226 105676.606 1102354.439 0.000

S 11°33'01" E 81.589

9232 105596.669 1102370.776 81.589

S 83°00'45" W 137.688

9233 105579.919 1102234.110 219.277

N 05°12'08" W 85.378

9229 105664.946 1102226.369 304.655

N 84°47'52" E 128.600

9226 105676.606 1102354.439 433.255

Closure Error Distance&gt; 0.00000

Total Distance&gt; 433.255

Area: 11089, 0.2546

Lot: PROPOSED LOT, Block: 1, Type: LOT

PNT# Bearing Distance Northing Easting Station

9232 105596.669 1102370.776 0.000

S 11°33'01" E 79.398

9230 105518.879 1102386.673 79.398

S 84°47'52" W 146.400

9231 105505.605 1102240.876 225.798

N 05°12'08" W 74.622

9233 105579.919 1102234.110 300.420

N 83°00'45" E 137.688

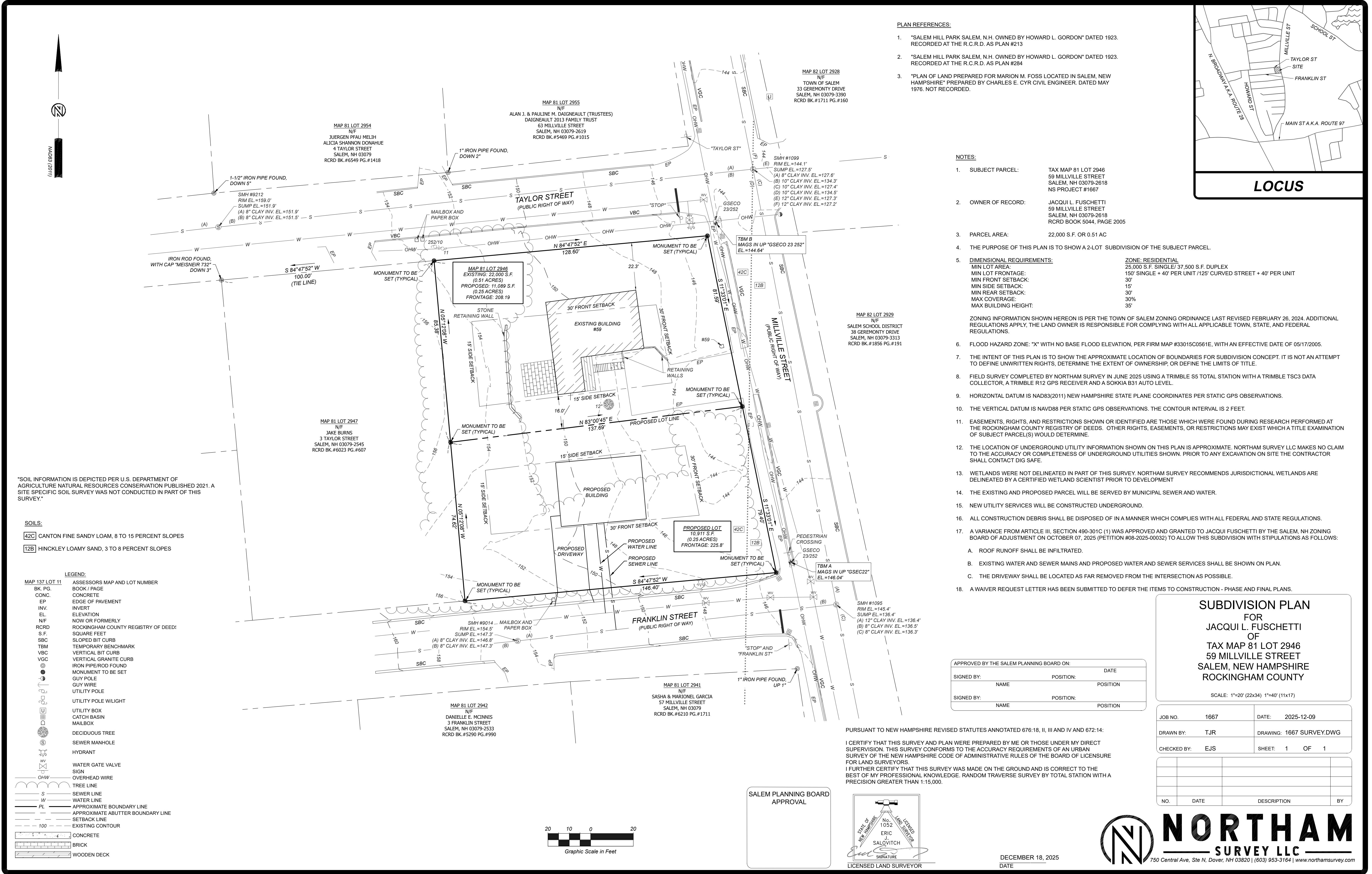
9232 105596.669 1102370.776 438.108

Closure Error Distance&gt; 0.00000

Total Distance&gt; 438.108

Area: 10911, 0.2505

Block 1 Total Area: 22000, 0.5051



Northam Survey, LLC  
750 Central Ave Suite N  
Dover, NH 03820  
(603) 953-3164  
eric@northamsurvey.com



**Re: Request for Waiver – Subdivision Regulations §278-4:1.13**

**Applicant:** Jacqui L. Fuschetti

**Property:** 59 Millville Street, Salem, NH

**Tax Map:** 81 | **Lot:** 2946

**Project:** Two-Lot Subdivision (NS Project #1667)

Town of Salem Planning Board  
33 Geremonty Drive  
Salem, NH 03079

Dear Members of the Planning Board:

On behalf of the property owner, Jacqui L. Fuschetti, we respectfully request a waiver from the requirements of Section 278-4:1.13 of the Salem Subdivision Regulations, pursuant to Article 7, Section 278-7:1 (Waivers). Section 278-4:1.13 requires that the minimum basement floor elevation for each lot be shown on the subdivision plan.

Article 7 authorizes the Planning Board to grant waivers where strict compliance with the Subdivision Regulations would result in unnecessary hardship or where such waiver would not be contrary to the intent of the Regulations or the public interest. The requested waiver meets these criteria for the reasons described below.

The proposed project consists of a two-lot residential subdivision, classified as a minor subdivision. At this time, no building or foundation design plans have been prepared for either proposed lot. As a result, establishing minimum basement floor elevations at the subdivision stage would be premature and speculative.

Final basement floor elevations are more appropriately determined during the building design and permitting phase, when detailed grading plans, foundation designs, utility connections, and drainage considerations are fully developed and reviewed by the appropriate Town departments. Deferring this requirement will allow for a more accurate and coordinated review that reflects the actual proposed construction.

Granting this waiver will not be contrary to the intent of the Subdivision Regulations and will not adversely affect public health, safety, or welfare. Compliance with flood hazard, drainage, and building code requirements will continue to be reviewed and enforced during the construction and building permit phase.

Northam Survey, LLC  
750 Central Ave Suite N  
Dover, NH 03820  
(603) 953-3164  
eric@northamsurvey.com

Accordingly, we respectfully request that the Planning Board grant a waiver from Section 278-4:1.13 in accordance with Article 7 of the Subdivision Regulations, allowing minimum basement floor elevations to be reviewed and approved during the design and construction permitting phase rather than at the time of subdivision approval.

Thank you for your consideration. Please feel free to contact us should you require additional information.

Sincerely,



Eric Salovitch, LLS

*President*

